

THE



RESOURCE

NEWS, REVIEWS, INFORMATION TO USE

*We're putting the "modeling" back
in Model Railroading! ®*

Volume 10 No. 1

September/October 2022

SCALE

**Punta Cayo Part 2
House by the Railroad
The Deep Run Railroad
Building an Unloading Trestle Diorama
Livestock Business for the Mountain Electric
New Tracks: Mentors Can Really Help Improve Your
Modeling
And So Much More...**



**Trees • Bridge Parts • Rock Molds
Lumber Loads • Engine Boxes
Structures
All Scales • Ready Made • Affordable
Handcrafted Premium Scenery Products**

**1(801)-891-7255
GrandCentralScenery.com**

March 16 - 19, 2023



... SHOW ...
Saturday 9am-5pm **Sunday 9am-2pm**

The parties whose names appear above, have agreed to hold harmless all of the organizers, the sponsors, Pioneer Limited LLC, the Westin Lombard Yorktown Center, and others, singly and collectively, for any injury, harm, loss, damage, misadventure, or inconvenience suffered or sustained as a result of participating in this March O Scale Meet 2022, or in connection with any activity related to this event, whether of negligence by agents under their employ or otherwise.

Published Bi Monthly

The Model Railroad Resource LLC
407 East Chippewa Street
Dwight, Illinois 60420
815-584-1577

September/October 2022
Volume 10 No. 1

Owner / Publisher
Amy Dawdy

Managing Editor
Daniel Dawdy

Advertising Manager
Jeb Kriigel

Welcome to the online O Scale Resource magazine. The magazine is presented in an easy to use format. The blue bar above the magazine has commands for previewing all the pages, advancing the pages forward or back, searching to go to a specific page, enlarging pages, printing pages, enlarging the view to full screen, and downloading a copy to your computer.

Front Cover Photo

Beautiful scene from Stephen Fisher's Deep Run Railroad

4 Bill Of Lading

7 From the Publisher's Desk

9 News You Can Use - New Items of Interest

16 Riding the Rails In the Land of Pleasant Living
By Stephen Fisher

38 House by the Railroad 1:48 Scale Rendition of the Edward Hopper Masterpiece
By Myles Marcovitch

51 "Punya Cayo" Part 2
By Jon Stetz

64 Livestock Business for the Mountain Electric
By George Paxon

71 New Tracks: Where Mentors Help Modelers Build
By Contributing Editor Jim Kellow MMR

96 Easy Landscaping on Your Layout
By John Frankforther

98 Unloading Trestle Diorama
By Phil Camp

103 Making Your Own Useful Tools and Jigs
By Graham Gibbs

108 What's on Your Workbench

111 Reader Classifieds

112 Show Schedule

113 Classified Ads

113 Advertiser Index



The Model Railroad Resource, LLC publishes *The O Scale Resource* and *The S Scale Resource*. Be sure to look at both of our magazines. There are many articles in our magazines that are not scale specific and will be of interest to you. Click the magazine title in this announcement to see the magazine.

STREAMLINED BACKSHOP

Sales ... Service ... Solutions ...



ZIMO MS Sound ?

WWW.SBS4DCC.COM



MODEL RAILROAD RESOURCE LLC 3D DIVISION



New Resin castings in O scale!

<https://modelrailroadresource.com/WP/>

ALL ITEMS WERE DRAWN TO SCALE FROM MANUFACTURER CATALOGS AND DATA SHEETS.

Tube is NOT included - You supply the 3/32 brass tube (K&S # 8126) or aluminum 3/32 tube (K&S # 8101) or Plastruct and others
SA1 will need tube and LED

BackdropsOnly.com

**At Checkout Enter code OSR-20
To 20% Off All Backdrops**



BackdropsOnly@gmail.com

or Call 800-320-5057

AffordableModelRailroads.com

GETTING YOUR DREAMS ON TRACK

Professional Layout Design & Construction

**We Complete Unfinished And In Progress Layouts.
Design, Build, Fix And Complete Any Size or Scale!**



We Can Build On Your Site or In Our Shop

MR@AffordableModelRailroads.com

(800) 216-9202

The Public Delivery Track

5000+ O gauge items in stock

100+ PDT special run items

2 Rail and 3 Rail

Atlas • MTH • GGD • Lionel

Weaver • Williams • Woodland Scenics

www.PublicDeliveryTrack.com

pdt212@gmail.com . 805-226-0320

P.O. Box 2637, Paso Robles, CA 93447

Details make the scene!



Check out all of our detail parts – Luggage, baggage carts, hand trucks, mail bags, trunks, train step stools, crates. See them at:

www.berkshirevalleymodels.com

ALLEGHENY SCALE MODELS LOCOMOTIVES & ROLLING STOCK

**O SCALE MODELS FROM THE COLLECTION OF
MIKE HILL**



SCRATCH BUILT AND CUSTOM MODIFIED BRASS LOCOMOTIVES
CUSTOM BUILT PASSENGER CARS BY JOE FISCHER AND DAN PANTERA
EACH MODEL EXTENSIVELY RESEARCHED AND PROFESSIONALLY FINISHED

UNIQUE - ONE OF A KIND MODELS

www.alleghenyscale.com - oscale@alleghenyscale.com

908-684-2070

SCRATCH BUILDING SUPPLIES

Barrels – Screen – Hand Tools
Poles – Blackened Cable & Chain
Plastics – Model Makers Pins
Window Glass – Eye Bolts & Rings
Wood Ties – Straight & Coiled Wire
Black Nylon Filament – Sheet Metal
PC Ties – NMRA Standards Gauges

DRY TRANSFER LETTERING

Cars & Locomotives - Stripes
Numbers - Chalk Marks
Building & Product Signs
Alphabets - Car Names

Shop Online - cloverhouse.com



Clover House

Box 215D, Veradale, WA 99037-0215

From the Publisher's Desk

Wow, another summer is about gone for those of us in the northern hemisphere. It was a toasty one, and Com-Ed was my new best friend.



Amy and I during the Maid of the Mist boat tour taking us right up to Niagara Falls.

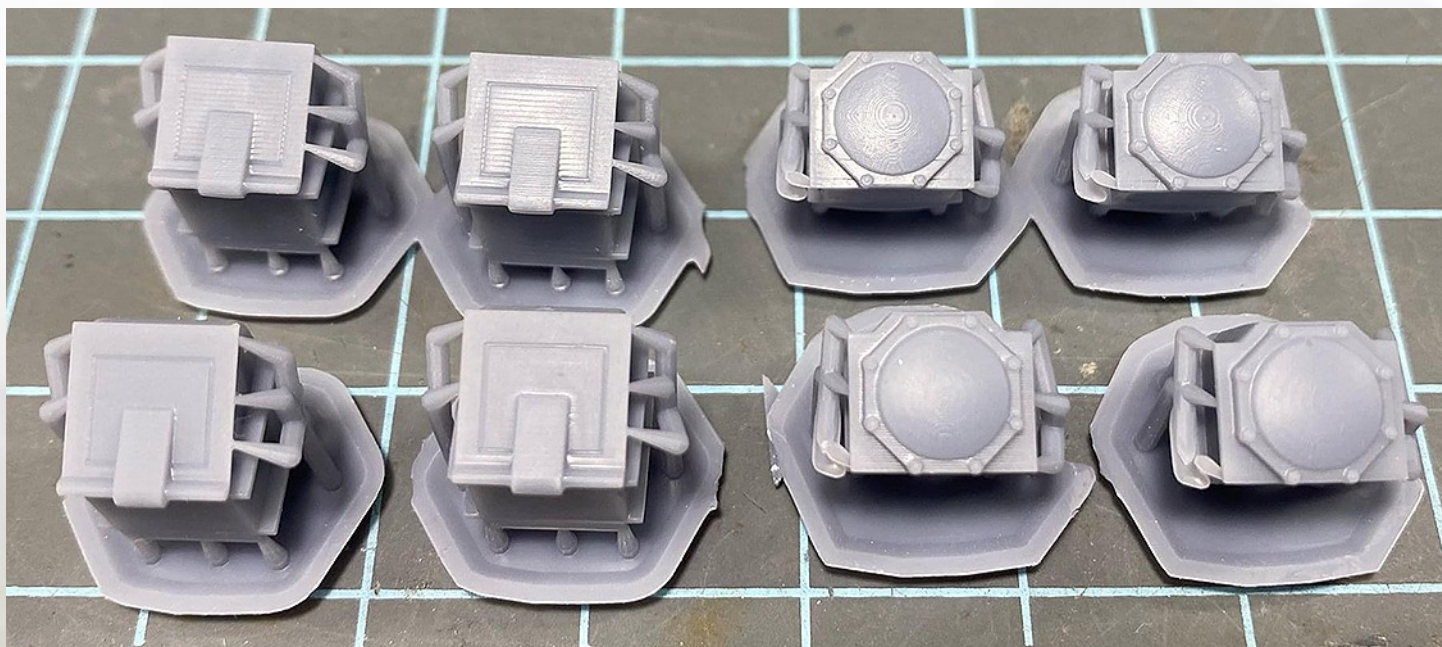
Not that I am crossing over to the darker side, but Amy and I did attend the NASG (National Association of S Gaugers) convention in Buffalo, NY over August 2nd through 6th. In case you did not know, I am now the editor of their magazine *The DISPATCH*. It was as much of a vacation as anything else. Three days of tours and site seeing, then the convention with its many clinics and a dinner and auction to end it. I remember O Scale National Conventions somewhat like this. Back in 1982 at the West Lafayette, Indiana National, there were tours set up for families and wives, as well as attendees, and a dinner the last night. It was much more of a social event back then. Somewhere along the line, we lost that part of it. The S scale community embraces it.

Anyway, I did pick up a few small building kits. I have a few places on my layout where the background going around a curve is very deep and thought I would try some forced perspective by using a smaller scale building. No doubt I'll do an article on my experiences with that.

Below is a view from one of the many wineries we stopped at on the way back from the convention. If you look closely you will see a CSX engine with train and then Lake Erie. Not a bad view with a glass of wine and a cigar!



It's been crazy busy here this summer, but things will be calming down and I can get back into the basement and modeling again. I have so many projects I need to finish as I bet many of us do. I owe you another 3D printing article, and I have received my new printer which so far is exceeding my expectations. I promise... next issue.



The top row is printed on my original Elegoo Saturn, while the bottom row printed on the new ELEGOO Saturn 2 Resin 3D Printer with 10" 8K Mono LCD. I used the same settings and same resin. When making patterns for clients, there will be almost no clean up needed.

Also, it's our 10th anniversary issue... woo-woo! Our first issue, [September/October 2013](#), was a whopping 45 pages of information and modeling. We knew what we wanted to do, but the execution was lacking in that first issue. As time went on, we grew thanks to our advertisers and you, our readers, who have supported us and made us the largest read scale O magazine out there.

Let us know what you are up to. Send an Email to daniel@modelrailroadresource.com with any projects, large or small, and let's talk. Don't worry if you don't fancy yourself a great writer, we'll work with you and help get your thoughts down.

Thank you all for your support, and remember to tell our advertisers you saw them in *The O Scale Resource Magazine*!

Happy Reading & Happy Modeling,

Amy & Dan Dawdry

NEWS YOU CAN USE



ITLA Scale Models Inc. has an O / On30 Brick Building kit. Designed as a clinic kit, this configurable, laser cut wood structure represents many of our ITLA product features. Laser engraved, weathered masonry on all four walls. Multiple Roof Top & Wall details included...Chimney, Snorkel Vent, HVAC Ducting, Loading Dock Door, Bumpers, Access Ladder, etc. Separate window headers & sills allow for mask-free painting. "Knockouts" left in place for modeling bricked up doors and windows.

construction, easy to build and paint, Colour instructions and sign sheet included.

Additional detail kits are available in O scale. Please see our website at www.itlascalemodels.com.



Bill Mosteller from Great Decals! Has a new O scale decal for the Ludington & Northern.

The Ludington & Northern chiefly hauled casting sand from a pit about 6 miles north of Ludington, down to the Pere Marquette/C&O/Chessie's yard in Ludington. It also served a few local industries in Ludington, including a company that made track maintenance machinery. Ludington & Northern #16 and Michigan Southern/Kendallville Terminal #16 SW8 diesel decals are available in six prototype colors (#205).



The configurable structure measures 5.75"L x 5.75"W x 6"H ... OR it can also be built as a Background "Flat" 22.25"L x 1.75"W x 6"H with extra 1.5" side return walls included. Robust wood



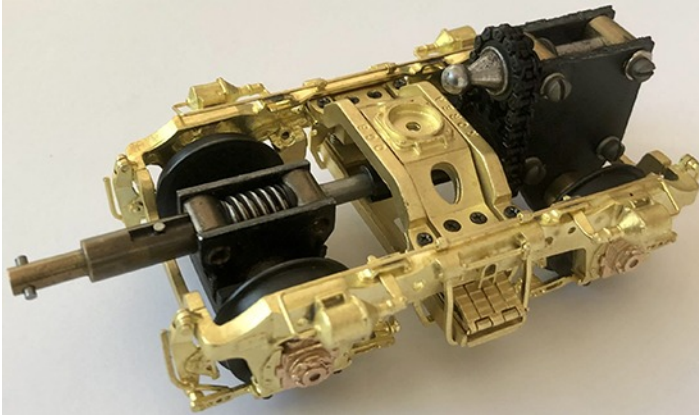
Through the years, #16 retained basically the same paint scheme despite operating on six different railroads. This six-color decal set is designed to model the six major paint and lettering variations it has worn over the last nearly 70 years.

Decal Instructions: <http://greatdecals.com/LUN-1ns.pdf>. Bill Mosteller, Great Decals!, 3306 Parkside Terrace, Fairfax, VA 22031 in O-scale for \$25.99 each.



New from John Wubbel and [The All Nation Line](#): The All-Nation Power Truck Transmissions are back! Now available, spare parts to restore old drives and newly completed unassembled kits for installation into your non-powered engines can be found in our webstore.

We even tested our drive in the Blomberg truck from P&D Hobbies. The tower chain drives are



available immediately, the helix gear configuration will be forth coming. The Part Number 4040 drive shaft spacer bushings are also available separately for repairing the older mechanisms.

https://allnationline.com/WP/?product_cat=transmissions-drivers-motors

Also released is our new basic beginners C&LE Traction Freight Box Kit 3D Printed. The details are left to the modeler and is designed to challenge the beginner and build model building skills.

<https://allnationline.com/WP/?product=all-nation-line-cle-box-freight-car-traction-kit-pn11k>

[Click here to see alll their fine O scale products!](#)



[Wit and Wisdom Models](#) announces a six-wheel heavyweight passenger trucks. Top-equalized, Pullman Type 242. In O Scale 2 Rail, traditional (5') gauge.

25% of the Pullman fleet, as well as many railroad-owned passenger cars, utilized these trucks. The models are cast in lost wax brass, sprung, metal wheels, blackened and detailed. These trucks have never been produced in 5' gauge, although Protocraft offered them in P48 at one time.



242 trucks were produced by All Nation in cast zamak, but are out of production. The only other version was by Lobaugh back in the 1940s. Both of the earlier versions were unsprung and, by comparison, crude.

These are from the final run of Korean passenger trucks. They are not cheap, at \$165/pair plus shipping. Only 30 pair available. When they are gone, they are gone. [Order from Wit and Wisdom Models](#). See ad this issue.



Joël Rassehaert from Chrezo sent us an update:

This Consolidation was delivered to the French state railways from 1913 to 1919 in more than 300 units. Some of them were ordered by the War Office for heavy railroad artillery pulling, but WW1 finished before they were used for this purpose. During WW2, most of these locomotives were used by the German railways administration for its huge transport needs on the Eastern Front. First dedicated to goods traffic, they were simple to operate and to support, so some of these locomotive stayed operating up to year 1975 on little branchlines.



Main features:

- 1/43,5 scale, Fine scale standards - Overall-all length : 440 mm
- Weight: 2.5 kg - Runs on 32mm Fine Scale tracks, 1020 mm radius curves - Die-Cast metal frame
- Injected ABS body, finely detailed,
- Full metal rods - Added-on details : hand-rails, whistle, sans-pipes... - Detailed driver's cab - 5 poles motor with flywheel,
- 8 axles electrical Pick-ups - Reversing headlights Front / rear - Screw couplers and sprung buffers - 4 liveries with details variations: smoke-door box, exhaust chimney, safety valves, tender arrangement.
- Limited edition: only 200 pieces per livery

The model is sold as analog, but is very easy to convert to DCC with ESU products (to buy separately to your dealer), after removing the dummy plug located under the tender shell:

* ESU LokPilot 5L réf. 59315 * ESU LokSound 5 L réf. 58315 - The sounds file have to be uploaded on ESU website www.esu.eu by N° 90308. * Loud-

speakers: they are factory assembled and wired, no necessary purchase ! * Smoke generator: ESU 54678

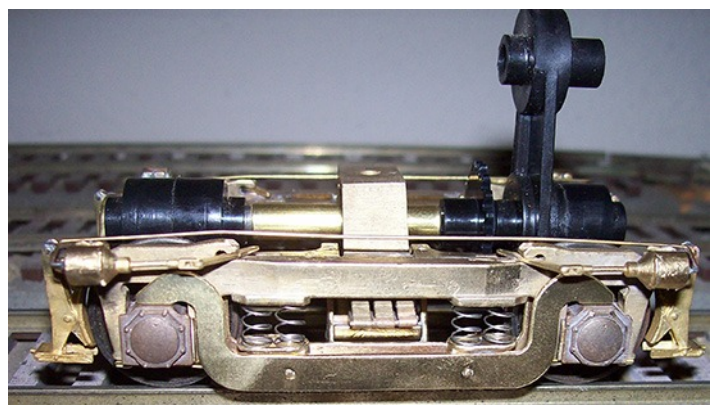
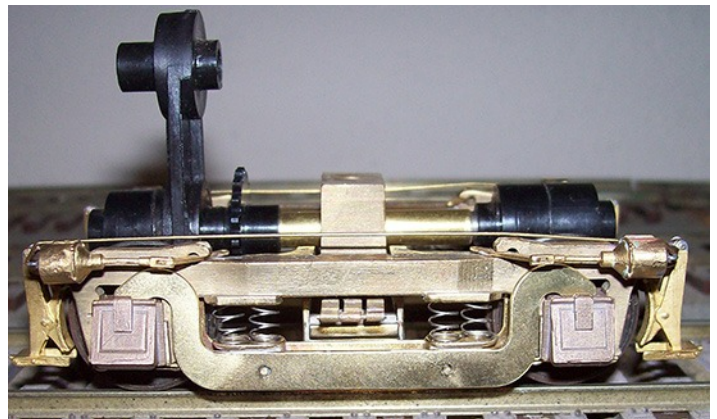


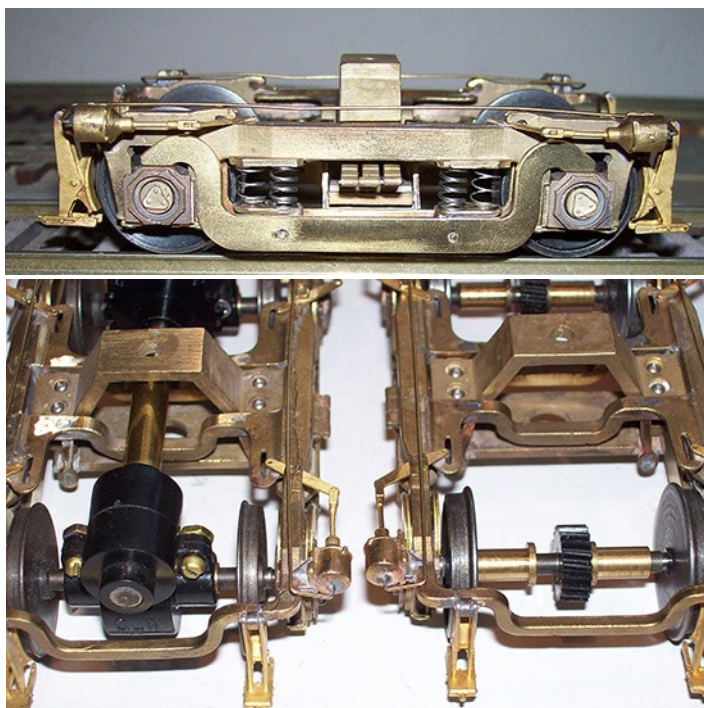
Price: 750€ without french VAT, insured and tracked shipping to USA costs 60€.



New from Central Locomotive Works! The AAR Type B truck is now available in both OW5 and P-48.

Will work with OMI, Weaver, Right O' Way, All Nation, etc. drives as well.





Price is \$200.00 / pair plus \$10.00 s/h - or - \$185.00 for 3 pair or more plus \$20.00 s/h. Choice of early friction bearings, later sealed bearings, or modern roller bearing with rotating bearing cap.

Lou says: When I purchased CLW from Bob Smith in 1998, my goals, to put mildly, were lofty with visions of managers and factory workers pounding out brass kits of diesel and steam locomotives. The model railroad manufacturing business model of overseas production quickly opened my eyes to reality and kits, steam models, and those factory workers were no longer in view. However, the diesel models from revised artwork and etchings along with the CNC machined chassis was a hit and continues today. The new GSC AAR-B truck (Alco truck) with floating bolster, working leaf spring and choice of 3 journal styles is now available. Another new model is in the works the SDL39 and it is new, from the trucks up. A redo of the U25B is scheduled and several other GE models are planned including the Dash 9's, ES and ET series brutes pounding the rails currently.

I would be remiss not to mention the several modelers who have helped me over the years in getting to this mile post. Russ Briggs and Jerry Snow have been there from 90's and more recently Jay Criswell of Right O' Way, Glenn Guerra, Dan Dawdy, Brad Kowall, and Jimmy Booth of Glacier Park Models. There are also those vendors who are gracious enough to produce the "small orders" of

parts at reasonable prices. And by-the-way, this is a 100% made in the USA effort! Hopefully this team of talents will continue to be there for new CLW items.

The aforementioned is mute however if it were not for you, the customer, who have remained loyal and patient. Many thanks you all.

Lou Houlemarde
Central Locomotive Works
clwusa@verizon.net for more information.



New from Bill Mosteller and Great Decals! Louisville and Nashville baggage, dormitory, RPO, and REA car decals, in dulux gold, are available in O-scale (set # 200) from Bill Mosteller, Great Decals!, 3306 Parkside Terrace, Fairfax, VA 22031 for \$23.99 each. Virginia residents please include sales tax.



Decal artwork: <http://greatdecals.com/WSM-200.JPG>

Lettering diagram:

http://greatdecals.com/GreatDecals/In-bagdorm_bill_lettering_r2.pdf

"The sheet includes L&N road names in two fonts, baggage, dormitory, RPO, and REA labels, and road numbers. Each sheet does at least one car. Artwork by Curt Fortenberry. Dealers' inquiries welcome.

More from Wit & Wisdom Models: Announcing John Armstrong's CEMENTIPEDE.

John Armstrong is credited with for many innovations in model railroading, particularly in track planning and train operation. However, John was also an accomplished modeler.



The model is built from John Armstrong's blueprints and project notes, as well as measurements taken from the original Armstrong-built model (a generous gift to W&W from the late Alan Fish, who wanted to allow other modelers to share this unique model). The model will be offered as track ready and a kit. Track ready models will come painted, lettered, and boxed, with two-rail trucks and Kadee couplers. Models will be a limited run, built to reservation, and will be separately numbered (e.g., No. 3 of 50). Construction is anticipated to be a combination of resin, metal, and 3D-Printed parts.

Some assembled models will be numbered 4315 to match the Armstrong original, but others will be individually numbered to allow each modeler to have multiple models (the kits will contain separate sheets with different numbers and allow road names custom lettered by the modeler). An illustrated history of the car will accompany each model. Kits will include a comprehensive instruction sheet.

Prices are not yet set. Models will not be cheap, but will be unique. No deposits are solicited or accepted, but first dibs on the limited run will go to those modelers who have signed up in advance. To sign up or receive more information, please email us at witandwisdommodels@gmail.com.

Tom Yorke has a new product for you narrow gauge modelers.

On30/On3: Southern San Louis valley Diesel Loco D-500. This was a home-built loco built by the railroad at Blanca, Colorado using an old D&RGW idler flat as a frame. It originally used traction tires, but these soon proved unusable.

The original version was powered by a Ford V8. That was the first version. The second is what is modeled here. Same idler flat, but the power was from an International Diesel with chain drive to the first truck (Bettendorf). The second truck was powered by chain from the first. A totally new body was built.

My version is for narrow gauge, but the dimensions are prototypical for a standard gauge unit with some work. This is a limited run Craftsman's kit made of resin castings plus a 3D printed cupola (assembled) by Francis Foley. Intended power is two Black Beetles with 26" wheels and a 30mm wheelbase, not included nor are the couplers. Some styrene parts along with a complete interior provided. Stainless steel screen is supplied for the grates on the sides. A half engine casting is also supplied as is one engine compartment door for the open doorway. The only other material needed the modeler must supply except glue and paint is some hole-punch 1/4" rounds in styrene and some 5-Minute Epoxy for the lenses. This is a medium range assembly kit. Some experience assembling craftsman-type kits is suggested. Price is \$125 plus shipping.

Thomas A. Yorke, 210 Pitch Lake Lane, Roswell, GA 30076 or ebonydog17@gmail.com. Direct only.





Norm's O Scale Trains Norm Pullen

P.O. Box 147
South Casco, ME 04077

207-655-2550

norm@normscale.com

Email us for our
latest sales lists!



Millhouse River Studio

Made in
USA

Turntables • Transfer Tables & Coal Industry Items

Products That Are Built to Last Generations!

Manual or
Auto Indexed

Lost Wax
Brass
Castings

TMCC
Control

(716) 830-5267



Turntables from 24" - 34"
Optional Turntable Railroad Specific Arches
& Extra Bridge Details Available



Scale Transfer Table
28" Weathered Version Shown

2 or 3 Rail
Aluminum
Construction

Rotary
Coal
Dumper



O Scale
Operating
Coal Tippie
Flood Loader

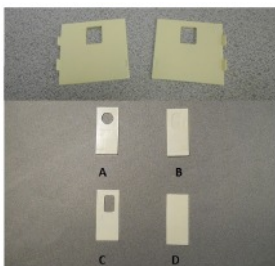
Check Website
for Details

www.millhouseriverstudio.com

Delta Models

Cast Resin Parts for
O Scale Passenger Cars

Baggage Doors
and
Vestibule Doors



MADE IN AMERICA

Visit Us at www.deltamodelsusa.com

O Scale Decals plus limited HO decals



for steam-diesel era freight cars for
"as-delivered and repaint schemes"
from 1930 to 1960, with over 650
decal sets representing 200 roads.

Available at protocraft.com

PROTOCOL™ DECAL LINE



'O' Scale First with Accurately Scaled Rail Since 1963

Now Producing Red Cliff Miniatures
& American Switch & Signal

Rail, Turnout & Crossing Parts, Switch & Track Parts
Wood Ties, Track Spikes, P48, Motors & Drives

2286 Hampton Way
Clovis, CA 93611
559.297.0505

Jay@right-o-way.us
<http://right-o-way.us>

ALLEGHENY SCALE MODELS

O SCALE LOCOMOTIVES & ROLLING STOCK

Sales, Collections, Consignments, Appraisals
Brass, Custom Built Models, RTR, Kits, Decals

470 Schooley's Mountain Road, Suite 8-117
Hackettstown, New Jersey 07840

www.alleghenyscale.com

PH: 908-684-2070 FAX: 908-684-8911
oscale@alleghenyscale.com

YOUR LOGO AND LINK TO YOUR WEBSITE COULD
BE HERE!

OVER 6,600 READERS AS MEASURED BY IP ADDRESS
FOR THE JANUARY/FEBRUARY 2021 ISSUE FROM
PUBLICATION DATE THROUGH MARCH 4TH, 2021

DON'T MISS OUT ON THESE WORLD WIDE READERS.

OSCALE TURNOUTS™, INC.

"Ready to Lay on your Right of Way™"

Features:

Designed to the NMRA, A.R.E.A. and 1921 Maintenance of Way Standards.
Cast frog, guard rails and points for a prototypical appearance.
PC ties resistance soldered to the rail to insure precise track gauge.
Solid (non-hinged) Points.
Prototypical tie spacing.
Staggered rail ends.
DCC compatible.
Insulated Frog.
Made in the U.S.A.



CONTACT:
P.O. Box 818
Hamburg, MI 48139
810.231.4461
wcm@oscaleturnouts.com

Estuary Trestle



Laser-cut wood parts and lots of nut-bolt-washer castings. Rail and spikes not included. Approximate size is a scale 14' x 90'. #17144 O \$129.95

Ballasted Deck Trestle



Laser-cut wood parts. Track and ballast are not included. Approximate size is a scale 16' x 50' #17103 O \$39.95

Cheat Run Trestle



Laser-cut wood. Rail and spikes are not included. Approximate size is a scale 13' x 27' #17142 O \$37.95 #17143 On3/30 \$37.95

B.T.S.
Better Than Scratch!

1782 Trinity Rd
Belington, WV 26250
304-823-3729

www.btsrr.com
\$5.00 S&H in the US

**DES PLAINES
HOBBIES**

**Kits or RTR
Brass
Books
Supplies
Structures
Detail Parts**

3-½ Miles north of O'Hare airport on Lee St.

1524 Lee St., Des Plaines, IL. 60018

Ph. 847-297-2118 Open Week Ends

www.desplainshobbies.com

Check our on line store. We ship daily.

International sales welcome. Thousands of items listed.

Call our friendly staff



Wasatch Model Company

The California Zephyr Passenger Train is in Production

Stevenson Preservation Lines

O Scale

Kits and Parts From Past Masters

Baldwin Model Locomotive Works

Lobaugh

Adams & Sons

Lenoir

Kansas City Kit

Hines Lines

Alexander

Pearce Tool Co.



We are moving. Check our web site for details

www.stevensonpreservationlines.com

YOUR AD HERE!
...OR HERE!
...OR HERE!
...OR THERE!!!
FOR AS LITTLE AS \$56
CLICK HERE TO EMAIL JEB KRIEGL
ADVERTISE WITH US

ALL-ABOARD-TRAINS.COM



First time ever in O, On3 scale. The New Century manual switch stand. \$28.95

**Model Trolley Specialist
For Over 40 years**

St. Petersburg Tram Collection
Car Works * MTS * Q Car * Bronze Key
Ken Kidder * LaBelle * Clouser * Ashland
Parts * Trolley Poles * Power Trucks
Kits * Overhead Wire & Fittings

NEW TORONTO CLRV KITS ARE NOW SHIPPING!
KIT INCLUDES INTERIOR, UNDERBODY AND NEW DECAL SHEET
NEW PCC KITS, ILLINOIS TERMINAL INTERURBANS
& MORE COMING!

The Berkshire Car Shop
505 Morning Dove Road, Audubon, PA 19403-1807
610.631.9751 Evenings and Weekends
Email berkshirecarshop@comcast.net

RIDING THE RAILS IN THE LAND OF PLEASANT LIVING

By **Stephen Fisher**

It is early morning and the Hogger has been up all night keeping the steaming beasts contented. It won't be too long before they will be called upon to serve their masters and do their duties hauling freight and the occasional passenger car over the lands of the Eastern Shore. Ah, the Eastern Shore of Maryland, known as "The Land of Pleasant Living," a place that is unhurried, a slower pace of life if you will, no rush, no problems. And so, starts the day, like any other day on the Deep Run Railroad.

The Deep Run Railroad is an anomaly in today's post Korean war days, a time when America is re-establishing itself. This little 30" railroad with all of its outdated and worn-out equipment must be seen, no experienced, to truly appreciate all of its nuances and charms from diminutive, little steam engines to late in life diesels and gas switchers. So, come along and enjoy a short ride over this little gem, and hopefully, you too, will find the charm that has made this such a beloved railroad. In my imagination, I have combined facts and fantasy.



The story and a tour

It is now 7 a.m. and the engineers and all of the crews are assembled down at the engine house, receiving their orders for the day. Some will be assigned local switching tours, while others are in for a longer day, pulling trains back and forth over the entire 25-mile line. (Photo 1) The super has called the various stations to see if any passenger tickets have been sold. If so, then a car or two will have to be added to the different trains, so that the passengers can ride in some comfort. Well, now that the orders have been handed out, the crews know what they have to do, and are walking over to their assigned engine for today, so why don't you come along with me for a ride, your fare has been paid.



The motive power is being made ready for another day of work.

Before we head out, let me share a few items with you. The engineer on today's run is Barney Williams and his fireman is Cletus Johnson, both are good, experienced family men and I am your brakemen and tour guide. Maybe you can help me with the switching duties. The locomotive that we are using today is Old Maude, a little 2-4-0; (Photo 3) the railroad prefers this wheel arrangement over all the others, because it is easy on the track. Today we are operating a main line train, running west bound from Pomona (Photo 4) almost to the end of the line at Deep Run. We are scheduled on train number 3 and we run inferior to the east bound trains. So, if we (and I am sure we will) encounter a couple of east bound trains, we will have to take the siding and wait. But then who cares, we have all day. So, climb aboard and let's go.

Barney lets loose the brakes and applies just a touch of sand, then starts to open the throttle just a bit to take the slack out of the five cars and the crummy. Since the railroad is fairly flat except for two small grades one



Old Maude is one of the favorite locomotives on the railroad.



down country between Deep Run and Marydale and the other just ahead of us, we should have no trouble with traction and slippage. We will quickly go through Midway and head into Evans, Old Maude can certainly handle things. The slack is pulled out and the drivers start to dig in and we finally start our journey.

Right: The interchange shed and platform between the standard gauge and the narrow gauge.



The cattle pens in Midway.

We have to move fairly slowly through the Midway Yard past the cattle pens, Cobb's Fuel Company, and Dabney's Dairy, because of all the activity in the yard between the standard and narrow gauge.

Since our train won't be going too fast, it will allow you some time to stand on the back deck and take in this beautiful country. As you can see, we do a lot of farming around here, in fact so much so, that even though we are the smallest county in the State, we produce the most amount of certain crops in the State.



Coming into the ghost town of Evans.

We are going through the ghost town of Evans and the collapsed bridge; it came down in the hurricane of '36. We are going to slow down some, since we are coming past the station and town of Tolchester. If you look over there to your left, you can see the Chester River; it is one of the two major rivers in the County.

As we take the big curve just outside of Tolchester, I can see the signal is dark ahead, so we will have to stop and wait for the local out of Chestertown that is coming our way.



Above: Downtown Tolchester.

Below: Looking down the track on the swing bridge, it looks like some kind of construction is about to get started.





The swing bridge at the narrows with Betterton in the background.

In a bit, once we get going again, we will come through the Narrows and pass by Betterton. The Narrows are akin to an Interchange point between Pomona and Chesterton yards and the rest of the railroad. Sometimes, to save time, instead of hauling a car all the way to Midway/ Pomona Yard and then send it back out over the line, it will get placed on the spur for future pick up. We can't leave livestock or items that would spoil if left too long though.

If you look to the left, you will see Betterton; now that is a busy place! There aren't a lot of houses there and most of the workers come in and out on the railbus that serves the town and upper county. Betterton also has McCallister's Wharf. This is another shipping point over to Baltimore with lots of rail traffic. There used to be a canning house until it burnt down about 10 years ago, not too sure as to what is going to happen with the building. Oh, those two buildings, one is Wisenheimers Brewery and the other is Coastal Machine Company. They really keep the town hopping.

While we sit here, let me tell you a little bit more about the railroad and the County. As I told you earlier, we are in fact the smallest county in Maryland and played a predominant role in the War of 1812. It was our state and the local militias that stopped a force of British Regulars, and that in conjunction with the stopping of the British fleet at Fort McHenry in Baltimore's harbor, pretty much helped to end the war. As you have seen so far, we have a fair amount of industry to go along with the farming. We have a number of water to rail connections to keep products and produce flowing out of the County.



Above: The Betterton Station.

Below: The cattle pens in Chestertown are an important water stop for the livestock.





Above: The Chestertown Station.

Below: There is always time for a conversation with friends and neighbors.





The Sewell Farm just outside of Still Pond.

We are also known for our fruit and vegetables and our sea food; it really can't be beat. Oh, there goes the whistle; I guess we got the all clear, so here we go again.

It might surprise to you that our ride is as smooth as it is, since this is a 30" gauge line. When it became clear so many years ago, long before me, that the County needed a better transportation system other than boats and horse drawn wagons, trying to get a railroad seemed the only solution. The County floated a bond and most of the County populace contributed; it didn't take a lot of persuasion since road conditions could be so bad. I see we are coming into Chestertown, we will take on water and then head out again, so let's just stay close by, stretch our legs a bit, and talk some more.

As I was saying, we have a lot going for us. In fact, we have a college over behind those trees, the only one that George Washington ever allowed to be named after him while he was alive. He was a regular traveler through these parts, going to Philadelphia and other places while we fought for our independence. OK, I guess we have taken on all the water we need, and Barney has signaled us so get on board. We don't want to be left behind.

Well, it looks like Barney has gotten in a hurry; by Deep Run RR standards we are really hauling the mail. It's got to be 10 mph. Let's see, we are coming up to the Still Pond cut off and soon after that the cut off down to Rock Hall will be coming up. Still Pond, now there is a sleepy little town. They have Cerrone's boat yard and a pier for shipping. Not much more though, not like Rock Hall. You wouldn't think a sleepy little community

like Still Pond could have any real importance, but it does. The first is its role in the Underground Railroad. It was primarily a community of Quakers, who gave shelter to the runaway slaves. The second, the village men offered and encouraged their women to participate in the vote for County and State politicians.



The Randall Stove Company off loading stone blocks.

You can see Rock Hall is fairly large for down here. Since it sits on the edge of the Chesapeake Bay, there is lots of commerce. They have an ice company where ice is brought all the way from Maine. They also have a canning house, boat yard, machine shop, and a company that imports stone from up on the Susquehanna River. The town is a jumping place on the weekends with Cliff's Juke Joint dance hall. They dance and drink and once in a while a fight will break out, but in the end, everyone makes up and stays friends. That's just how it is.

Now, even though it is small, even by our standards, Kennedyville still can boast a fairly large grocery store, a church, and a station, and oh ya, Mr. Knapp's junk yard. Well, he doesn't call it that, but most other people do. That man can take a pile of junk and make something useful out of it, never saw anything like it. Nothing exciting has ever happened there, probably never will. They moved the railroad station a number of years ago from Marydale and stuck it beside the railroad tracks but never changed the sign, guess everyone in these parts knows where Kennedyville is.

Well, I see the Marydale signal is dark so we will go in the hole again and wait for the morning local out of Deep Run. You need to keep an eye on the cars and make sure everything is OK. It won't be too much longer, and we will be in Deep Run.



Above: Tucker Supply Company, The Cannery, Bangor Ice Company and many more businesses are located in W. Rock Hall.

Below: The Rock Hall waterfront in E. Rock Hall.





Above: Rounding the curve going into E. Price.

Below: Crossing the low trestle between Marydale and E. Price with the MML high line in the background.



As I told you earlier, we are the smallest county in the State and the land is fairly flat, but we are coming up to a section of the county that makes a liar out of me. We are getting ready to cross a pretty big gorge by our standards on a pretty big trestle, get yourself out on the rear deck so you can see the view: "That's pretty impressive, ain't it? If you quick look back, you can see the logging grade going up the hill. Yea, it ain't much of a hill by your standards, but it does require a helper to push a train up it, should the cut of cars be too much."



*Above: Old Maud approaches the trestle on the low line.
Below: The train and track crew stop for a chat, I wonder what that is all about?*





*Above: The station and water tank at E. Price.
Below: Richardson's Pickle Company.*





Above: Chewing the fat and solving the problems of the day.

Below: Stones Cattle Company and Processing.



As we come around the curve, you can see the logging grade again coming downhill on the right, in front of Perry's Turpentine Mill. They have a signal that has to be dark for them to enter the main line.

Well, that was fast. This here is Price and the last town before we reach Deep Run. Sometimes we get a break, and it looks like today this trip will be a quick one. Do you see that building over there with the wood tanks? Well, that's an interesting story to tell you about that business and the family that owns it. It seems many years ago this fellow moved here with his wife and a couple of boys. Their last name is Richardson. Would you believe they come all the way from Australia? Yea that's right from down under. Now this guy is pretty talented, and they are really nice folks, everyone likes them. Anyway, he decides he is going to grow grapes and make wine to sell. Have you ever heard anything so crazy? Well with all of his talent, making good wine wasn't one of them, so he decided to try his hand at pickles and vinegar; and don't you know people can't get enough of his products. Now who would ever believe something like that could happen here. Life is funny that away!

When you see Richardson's Pickles you know the town of Price is just around the bend and there's the town. The town of Price is a nice place if you don't mind the smell. As old man Stone tells it, that's the smell of money. Mr. Stone has the slaughterhouse, butchering and the meat

processing company, the hides go back to the Harper Brothers Leather Company up in Pomona. All of the meat is cut to size, frozen, and shipped out to the rest of the world. He's the biggest company in Price and has made it what it is today. We have to run a train just about every day, either bringing in cows and hogs, and once in awhile, chickens. We usually haul three carloads of meat back to Midway to send out to the rest of the country. His company keeps us real busy.

Price is also the beginning of the grade for the Meadow Mountain Lumber Company trackage. This railroad runs up the hill to Big Hill Lumber camp and to the town of Fair Hill, and the reload area, just about 5 miles out of town. No, you can't call them mountains, just big hills. No mountains on the shore here. Fair Hill has some industry, and the railroad runs a train or two a day, either hauling loaded cars downhill or empty log cars uphill for the lumber company or a mixed train with products for the towns folk or the occasional cattle car for livestock. Big Hill has a spur that goes even further down country to the pine forests. There is a railbus or jitney, as we call it, to run people up and down the hill, but it only runs on an as needed basis and not a regular schedule.

I guess our ride for the moment is finishing up; I see the Crinkle Brothers paper mill up a head in Deep Run. Yea, that set of buildings over there. You have some time to stretch your legs, Barney is going to have to drop his train and run the loco light over to the tank and turntable. In the meantime, the local switcher will take this



Above: Coming around the big curve into Deep Run . That's Crinkle Brothers Paper Company in the background.

Below: The end of the line, the Deep Rock yard and Crinkle Brothers Paper Company.



cut of cars and swap them for a new set that we will have to take back to Midway and Pomona. How about you go over to the station and get a bite to eat. Make sure to ask for Flossie!

I can join you in a bit; I need to square away the paperwork with Eustis, the yard boss. Then after the crew eats, we will head back to Pomona and that will be our day.

And so, the story goes, just another day on the Deep Run Railroad.



The actual railroad

The actual Deep Run Railroad has been in existence for more than 45 years in some form or another and in different places. I have lived many places in all those many years, and I have always had an On30 railroad, be it modules or full size. I was attracted to this scale and gauge because, at the time, few modelers were trying their hands at it and that was the allure for me.

Like most modelers my age, I started out with Lionel and American Flyer then graduated to HO, the trains were put away during high school, and not brought out again until my son was born. After a few years of building a HO railroad, we relocated and I decided why not add a narrow-gauge branch line. I didn't really know too much about narrow gauge and had to do a lot of reading, but I got it and I was bitten. Then one day I saw an article in one of the American model railroad magazines all about On2 ½ and thought why not give this a go. Because there wasn't an Internet, trying to find information about this gauge and scale and how to go about building an On2 ½ empire was difficult at best. In the meantime, I met a fellow modeler, who lived fairly



close to where I lived. He and I decided to build a couple of modules, and so we became the first On2 ½ modular railroad in the Eastern United States.

As time went on, we exhibited our not so good efforts, but we attracted a couple more like-minded modelers and the module group known as The Manchester Mini Bunch was born. As a group, our skills really improved, we exhibited for many years, and even showed up in a few model railroad magazines, including *Narrow Gauge Downunder*. We decided to retire the modules and focus on our individual railroads; we have stayed together as friends for more than 45 years. Since those early days and, with the help of those many friends, I have the railroad today which is probably my swan song, so to speak.

Today's railroad is by any definition of the word a very large point to point railroad, occupying a space of 1900 square feet. The railroad is totally battery powered except for the signal system, which could be converted, but for now is electrically powered. The railroad operates with a dispatcher, train orders, car cards and waybills. We usually operate with 10 guys and can incorporate an additional 5 more.

The locomotives are a combination of Bachman On30 pieces, scratch built, and kit bashed HO pieces, and a few heavily kit bashed Bachman pieces. At present, I have 38 locos under battery power and will probably leave it at that.

The freight and passenger cars are also Bachman products as well as scratch-built pieces. I have used the Bachman lowering frames as the basis for my scratch-built cars and have modified them by removing ½" from the center and gluing the halves back together. I glue a wood deck on and proceed to scratch build my various freight cars. These run the gambit from box cars and flats, to gons, bulkhead flats, and even water cars. The



advantage to using these frames is the fact that coupler pockets and bolsters are cast in place; this to me is the hardest thing to get correct when having to scratch build the frames.

Most of the structures are scratch built specifically for a given space, but there are also a few kits and ready-made buildings as well. As you can see from the photos, detail is very important to me. Without it, the scenes that I am trying to portray would be flat and lifeless. I have used everything from wood and paper to plaster and styrene to fabricate my structures.

The railroad is traditional framing with a top layer of plywood and Homasote. Scenery is a combination of dirt and various commercial products, as are the different trees. The track work is both standard gauge and narrow gauge, some dual gauge hand laid and flex. When looking at the pictures, take particular notice to the fancy standard gauge track work. This was done by Cliff Bailey. He is a true artist, and has built track work that is truly amazing. My back drops were done by two friends, one being the late wife of one of our group.

As I mentioned earlier, the railroad is operated with the Del-Tang r/c battery system. Using battery gives me a stable operating platform that doesn't require expensive add-on equipment. I don't need gaps, nor do I worry about keeping all of the rail clean. Simply turn on and run your railroad. I was introduced to r/c battery by my friends Steve Sherrill and Geren Mortenson, something for which I will always be grateful.

There are so many friends to thank, for their continuing friendships and talents as well as items for the railroad, but the two who have been there from the beginning are Garry Cerrone and the late Jim Barcus. These two were the spirit of the module group and the Deep Run Railroad. Without them, the railroad would have never become what it is, and this article never written. Garry also produces a number of On30 and O scale items under the banner of [GC Design Group](#), which can be found on the web. I also want to thank the guys in my



operating group; they bring the fun to model railroading.

I guess this pretty much covers the history of my involvement in On30, and hopefully it will give you some insight as to where I started and what the long journey has produced. In the future, I would like to write about the rest of the railroad that wasn't covered in this article, including the mill town of Barcusville and the lumbering operation and the town of Fair Hill. So, may all your

trains be on time and your tracks parallel, high ball for as long as you can.

Since I wrote this article back in 2018, many things have happened, sadly, not all happy. We lost many of our members, corona hit, which caused us to go online to hold Zoom sessions and therefore no physical operating sessions. In March of 2021, we lost one of our group to Covid, it has been devastating. On a positive note, we have added three new members, none of which live in Maryland. Also, two major sections of the railroad were totally rebuilt, track work reconfigured, new scenery, and industries were moved around. The group still meets once a month by way of Zoom, so for now we keep on being a group. Who knows, one day we will actually be having trains run again over the railroad, just like in the old days.

There are two videos, the first showing a trip from the west to the east, we have posted a return trip, but as of yet, I have not seen it. The video was shot by one of our group members Martin Lampner. As a side note everything shot from around the 10:15 mark, has been replaced. I spent most of the fall in tearing down and totally rebuilding the “front” of the railroad.



Click the images above to view these beautiful videos.

SCALE CITY DESIGNS
Formerly Kell-Line & Walther's O Scale

FREE CATALOG DOWNLOAD!

DETAILS FOR
 *FREIGHT *SCENERY
 *BUILDINGS *FIGURES
 *LINESIDE *DIESEL
 *SIGNALS *TRACTION
 *PASSENGER *VENTS
 *PASSENGER INTERIOR
 *TRUCKS & ++MORE
SCALECITYDESIGNS.COM

*****NOW OFFERING***
 CUSTOM PAINTING & WEATHERING**
 See our examples at
<https://www.facebook.com/justweatherit>
 @justweatherit



YOUR SOURCE FOR OVER 80 RAILROAD LETTERING, RAILROAD ROMANS, ALPHABETS, SILHOUETTES, DINGBATS, TRAINS, PLANES AND AUTOMOBILES AND EVEN SOME SURPRISES. ALL FONT SETS ARE AVAILABLE IN WINDOWS, TT AND ATM, OR MAC VERSIONS.

RAILFONTS.COM

**Love Modeling?
 Want to improve your skills?
 Join. Learn. Explore. Enjoy!**

NEW TRACKS
 with host Jim Kellow, MMR
"Where Mentors help Modelers Build"

NEW TRACKS is an exciting, **NEW, FREE**, communication concept.
 Our shows are live and interactive on Youtube every Wednesday at 7PM ET.

Find a mentor - Meet and talk with talented modelers and manufacturers from around the world.

Get discounts - Buy kits used in our BUILD ALONG segments.

Explore a wide variety of modeling topics including new technologies. Scratchbuilding. Kit Building. Kit bashing.

Share your own modeling in our MY BUILD sessions.

Give us a try!
 Subscribe to **NEW TRACKS** at our website: newtracksmodeling.com for details and Zoom links or subscribe on our YouTube channel: **New Tracks Modeling** for show information.

NEW TRACKS

THE *A*LL-NATION LINE

***Presenting the All-Nation Line Power Truck
Transmissions, small gear units and parts now
available!***

<https://AllNationLine.com>

PHONE 570-580-7406

AMERICAN SCALE
MODELS



Purchases and Sales of Scale Model Trains

Estate and collection liquidations

Consignment Sales

Purchases of new, used, and unwanted equipment

References gladly furnished.

Bill Davis, P.O. Box 1011, Oconomowoc, WI 53066 • (262) 560-1619 • bdavis148@aol.com
Visit my website at americanscalemodels.com for models, detail parts, trucks, and more!

TRAINZ

Trainz wants your O scale collection! With over 45 years of experience, we make it easy to sell your collection.

**sellmytrains.com/osr
(866) 234-1900**



Looking for a great selection of
new and used O scale trains?

Visit trainz.com

HOUSE BY THE RAILROAD

1:48 Scale Rendition of the Edward Hopper Masterpiece

By **Myles Marcovitch**

Several years ago I scratch-built two Victorian-era buildings with turrets and Mansard roofs. In one interior, I replicated Edward Hopper's "Nighthawks" painting. For the other, I scratch-built a 2nd Empire town house based on a picture of one in The Bronx, NY.



(Left) "Nighthawks Cafe" with Hopper's eponymous interior. (Right) Woodbourne Gallery based on the extant Bronx Documentary Center in Bronx NYC.



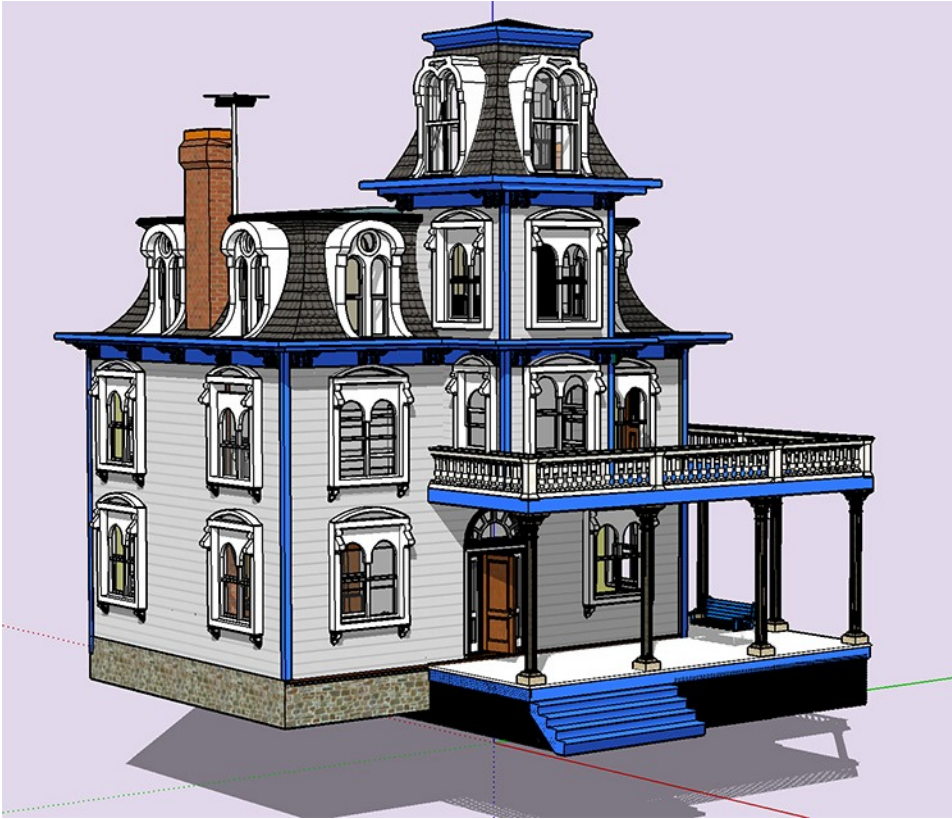
These two projects built my confidence in creating period buildings and I was encouraged to delve deeper into Victorian structures for my 39' X 15' O gauge model railroad. For my next challenge, I chose to build Edward Hopper's painting, "House by the Railroad" painted in 1925. The real house on which the painting was based is possibly in Haverstraw, NY. Hopper took artistic license with his painting, and I took more in creating my model.

Edward Hopper's 1925 painting: "House by the Railroad" residing in the Museum of Modern Art, NYC.

This project had three phases:

1. Drawing - 3D master drawing along with a many detail parts in order to 3D print and build the structure.
2. Printing - 3D printing is a steep learning curve unto itself.
3. Assembly - Getting it all together had its own challenges.

Drawing.



House by the RR Final SketchUp drawing.

I do all of my initial design work in SketchUp Make ver. 2017. When looking at Hopper's painting, you can see lots of fancy window frames, reverse-curve Mansard, balcony with balustrade, large porch; signposts of a high-Victorian structure. Not seen is: what's happening on the right side and in those shadow areas? For instance, where's the front door and what does it look like? For a famous American artist, these ambiguities work, but to make a respectable model, I needed to fill in the blanks. I originally traced the building's shape using SketchUp's "Match Photo" facility.

As an experienced SketchUp user, I still had much to learn. I had to produce many drawings for the project. Some of the drawings took longer to complete than the model's assembly.



The window frames caused the most difficulty having intersecting curves. Some details such as the balustrade were downloaded from SketchUp's 3D Warehouse and used as-is. Other SketchUp models served as the design basis which I then greatly modified, like the 3-story staircase.

Drawing changes continued well into construction. As building began, I kept finding errors and omissions needing correction. I have an ever increasing appreciation for commercial model manufacturers putting out error-free kits.

Left: A sampling of just some of the details that needed to be fleshed out.

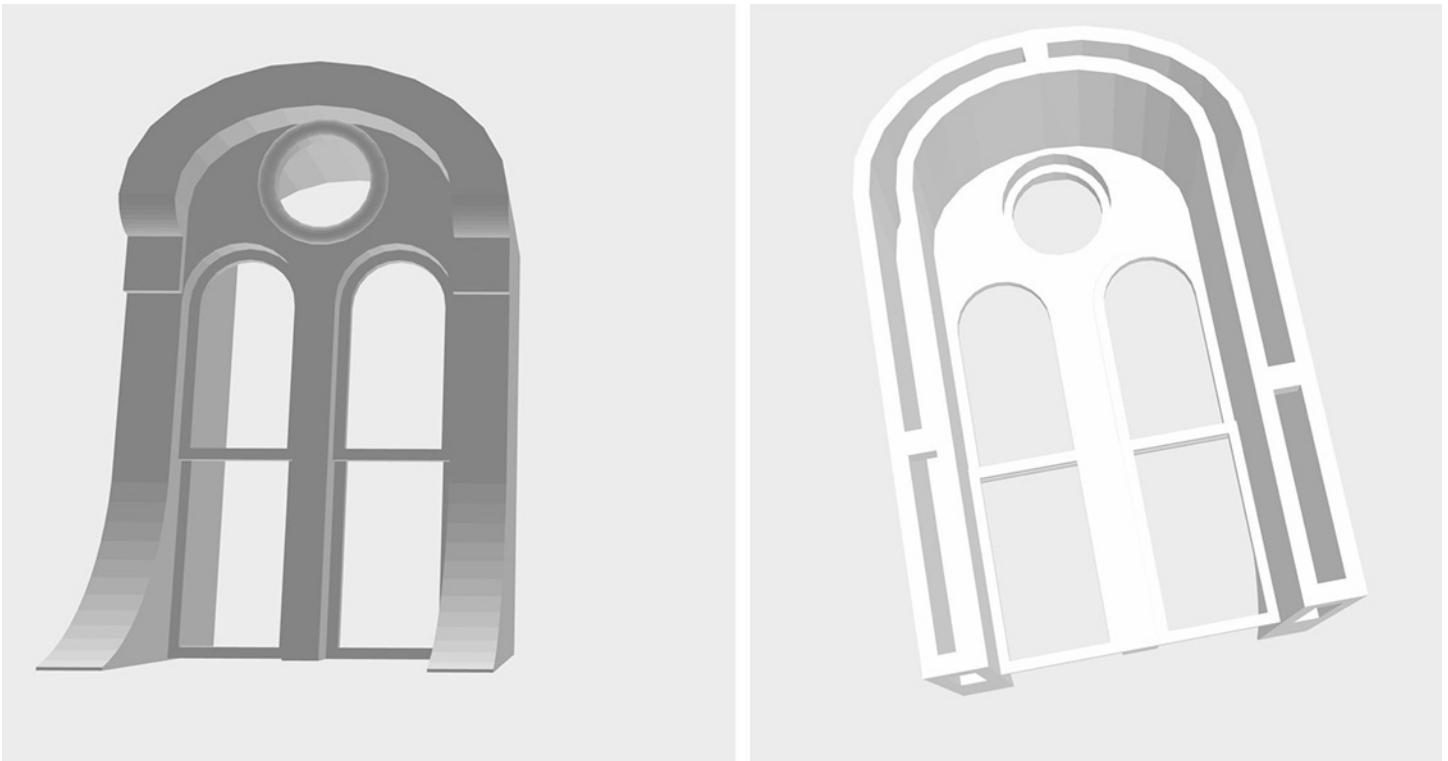
Some of the changes I made to Hopper's original are:

- Centered the turret and changed left-side wall
- Changed the porch/balcony shape
- Repositioned the chimneys to correspond to my fireplace locations
- Substituted double Mansard windows for singles
- Created front door design
- Added a porch swing
- Created a full interior

Each of these changes started out as drawing changes.

3D Printing:

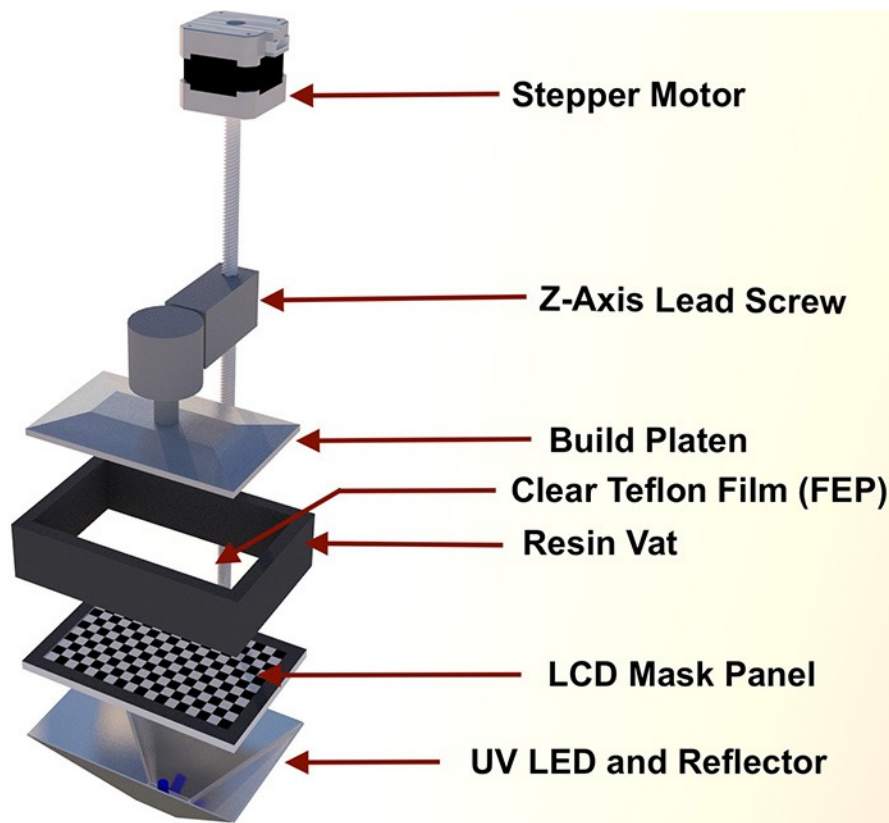
After finalizing the Mansard window design, I hollowed out the massive portions to reduce resin use. This produced some thin, warp-prone surfaces needing strategic ribs. I was able to print the window mullions with the windows thereby reducing parts count and providing gluing surfaces for the acetate glazing.



Mansard window STL file image showing hollowed out thick parts to reduce resin use.

While it's not my intent, nor is it possible to give a complete set of instructions for resin printing, 3D printing was essential to create this structure. Therefore, some background can be helpful.

There are two prominent 3D printing technologies available for hobbyists today. They are Filament Additive Machines (FAM) which I call "string printers" and LCD Matrix Machines. FAMs are analogous to a hot glue gun where the "glue" is a thermoplastic polymer. While the plastic is oozing out of the nozzle, the nozzle and/or build table move back on forth (X and Y Axes) to "draw" the profile of the first layer.



The mechanical arrangement of an LCD Matrix 3D Resin Printer. In practice, the Z-Axis stepper motor lies below the lead screw.

Depending on machine design, the nozzle (or table) then moves up (or down) in the Z direction one layer and the nozzle draws the next layer. This repeats until the object is formed. FAMS lack the resolution to create the small parts we modelers need.

Resin printers are optical in nature and are capable of much higher resolutions. The earliest 3D resin (stereo lithographic) printers used a laser to draw each layer in a liquid resin photopolymer or powdered polymer reservoir, line by line much like the string printers do. While able to produce very finely detailed parts, they were/are expensive with prices of \$3000 and up. The 3D printers in use now to produce parts out of metal, still use this technique. DLP video projection system has been substituted for the laser increasing process speed, but are also very

expensive. Two years ago a third scheme, LCD Matrix machines, were introduced dropping the price tenfold. I purchased my Elegoo Mars machine in June of 2019 for \$350. They're now available for much less. (As of this writing my machine has dropped in price to \$150.00 and new machines of higher performance are now available at the old price point.)



Loads of 3D printed parts needed to complete the House.

The breakthrough was adopting a 2k smart phone screen as a photo mask. There is one moving axis, Z, that raises an aluminum build platform holding the object being formed. A UV LED light source lies below the LCD and shines through it to cure the photopolymer. Above the screen is a resin vat with a clear Teflon bottom. The Teflon provides a transparent, non-stick surface that seals the vat. The to-be-printed object is presented as a series of pixel images on the LCD. Each image represents one layer of the object. When the LCD is clear, the U-V light passes through and cures the photopolymer. When the LCD is black, it blocks the UV light. The plate starts out at the vat's bottom raising one layer height for each

layer to be cured. At the end of each exposure the Z-axis raises about 5mm to pull the just-cured layer off the Teflon. It then returns one layer higher and the process continues. Of all the difficulties incurred in this printing method, it is the pulling of the just-partially-cured resin off the Teflon that causes the most trouble. I will tell you more about this further on.

The object I created in SketchUp, must be converted to an STL (Stereo Lithography) file to be read by the slicer. The slicer is the software that literally slices the object into a series of individual pixel layer images that the printer reproduces. It's analogous to a CAT scan.

You must master many variables for 3D printing success and it is a new skill set for us modelers. In addition to the printer, you need a means to remove the partially cured resin. After printing, the parts are not fully cured so each layer can fuse properly to the last one. After cleaning in isopropyl alcohol, I use an ultrasonic cleaner to finish the job. You then remove any supports that were needed in the print and put the part into a post-cure UV chamber to finish curing the resin. My low-tech chamber is an aluminum foiled-lined box with a UV LED array stuck through the side.

Supports

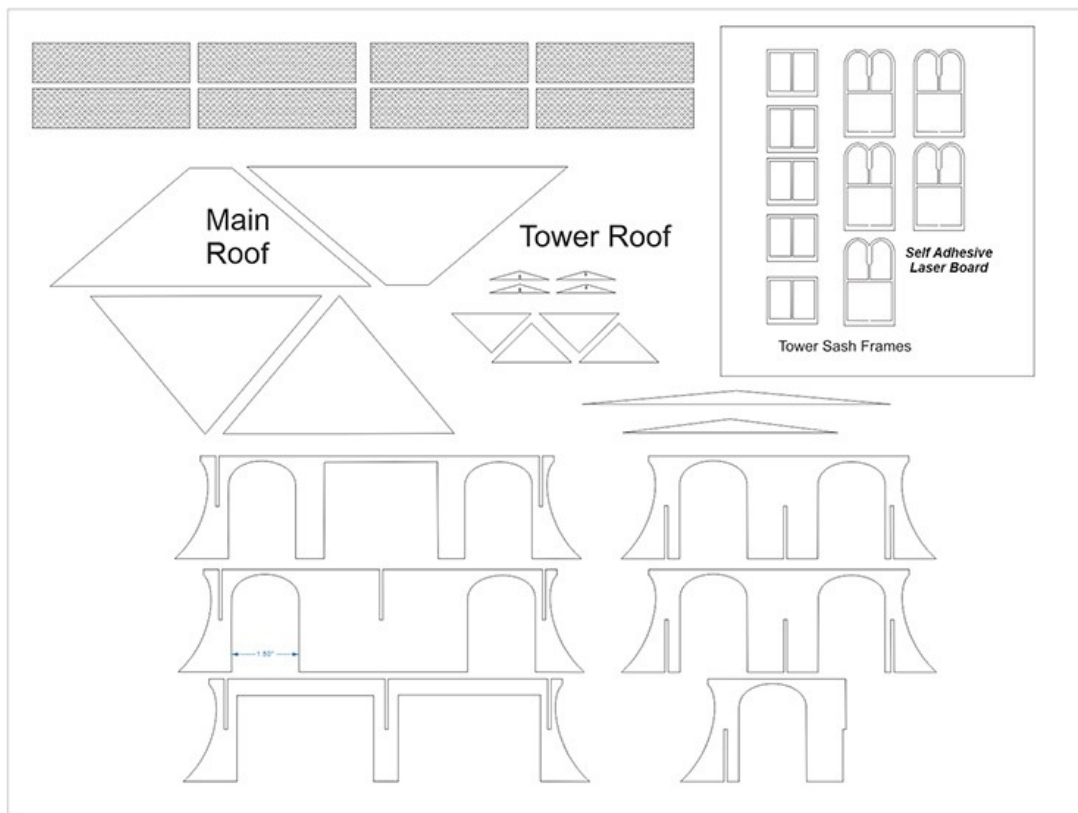
In order to pull the object off the Teflon, it must be firmly attached to the build plate. Depending on the part's shape and its orientation in the machine, it may need an extra lattice of resin supports to hold all of it in place. The supports are "grown" along with the object. Support placement and design is the most arcane aspect of 3D printing. It is also the primary failure mode. There are many videos on YouTube to teach you how. That's how I learned.

Laser-cutting

All of the flat parts were laser cut for me by Rail Scale Models. I imported flat projection screen prints from the SketchUp master into CorelDraw and drew scale drawings directly over them. Laser cutters cut any lines

.3pts or smaller and engrave any that are .5pts and larger. I also had Rail Scale laser cut the window frames for the cupola out of self-stick laser board and the acetate glazing for all the windows.

The only detail part I didn't produce was the cupola. I wanted this to be a single printed part, but my Elegoo Mars was too small. A fellow-member of the Military Modelers Club of Louisville, and head of the University of Louisville's Advanced Manufacturing Institute, produced the part on an FAM for me. It had those pesky filament lines. I thought I'd have



Sample CorelDraw page with laser cut instructions for the roof parts and attic interior partitions.



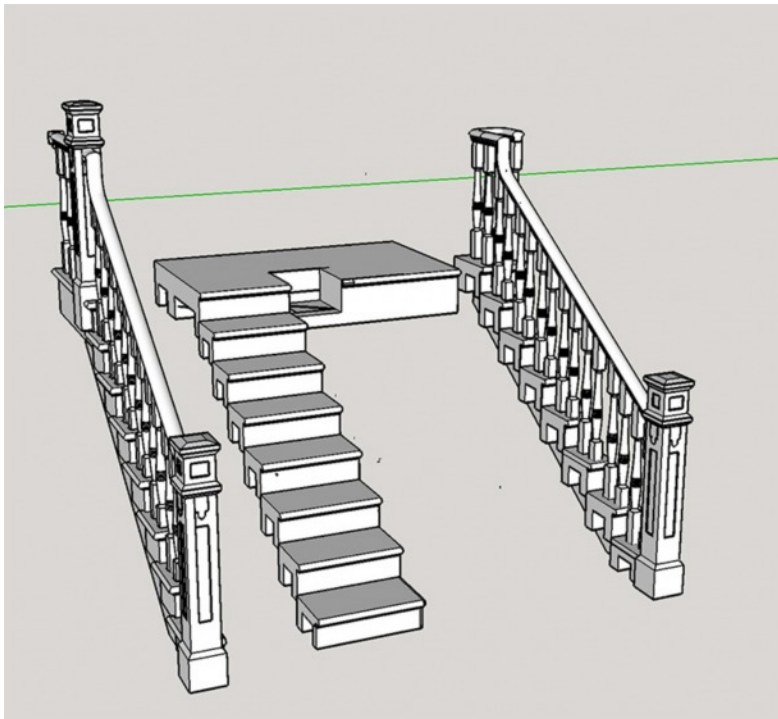
Single-print cupola assemble to upper turret room. Also show is my 3D printed serial staircase to the cupola room.

to fill them, but several coats of Rust-oleum white primer/top coat obscured them well enough.

I was able to work on other projects while printing the parts. They often take hours to print and most of them were printed overnight. When the settings are right, resin printers just hum along. I would be greeted by newly created parts every morning. My printer runs at 10 seconds/layer. The new Elegoo Mars 3 produces layers in 2.5 to 3.0 seconds/layer; a four-fold increase in speed.

Assembly

Because I'm a deranged modeler with AMS (Advanced Modelers Syndrome), I built a complete Victorian-themed interior including baseboards and crown moldings, Victorian wallpaper, detailed furnishings and paintings, plus an entire 3-story staircase. The staircase was challenging. Supports couldn't attach to the railing spindles as their removal would destroy the spindles. Instead, each flight was two or three printed parts. One part was the railing and a chunk of the steps. The other was the center treads. CA cement joined the parts after cleanup. CA holds resin parts like crazy, and something in the resin causes the CA to kick even faster than normal giving very little working time. If you position the part incorrectly, you may have to break the joint to fix it.



Above Left: Printing considerations required a multi-part staircase design.

Above Right: Completed stair comprised 11 separate parts not including the additional walls and closet door.

I tried three designs before I found a simple square spindle was the most successful. The finished staircase consisted of 11 parts (not including the walls, trim or closet door). It was an experiment that worked!

Initially, I was going to glue the staircase onto the 1st floor plate, but after test-fitting the walls, I couldn't get the 2nd floor to drop over the steps. I ended up gluing the staircase to the 2nd floor and dropping it over the 1st without gluing it there.



The full-interior really complicated the project. My original partition walls didn't fit like they should and I had to remake several. I had to learn how to install 1:48 crown molding! Floor coverings are a black and white tile pattern that I had drawn for other buildings and some parquet floor images that I also used in other projects. I found samples of Victorian wallpaper designs searching on Google. Using swatches with Corel PhotoPaint, I was able to create entire walls of each pattern. Each room is different. Also on Google, I found oriental rug designs that I sized and printed. A grand house needs moldings. Using a SketchUp plugin, Profile Builder 3, I drew baseboard and crown molding parts. The fireplace hearths also came from SketchUp. The House has four of them. I settled on a formal interior door design with bullseyes in the top frame corners. Thin prints warp pretty badly. To counteract this, I printed the doors in back-to-back pairs. When glued together it canceled out their warp. Furthermore, I printed them directly on the build plate producing beautiful surface finishes without supports to remove.

Above: Living room ensemble including 1:48 replica of our own Henkel-Harris oval coffee table.

Below: The dining table, chairs and server on the left wall are all pieces that exist in our home. The china cabinet was my free-lance design.



I kept experimenting to see how far I could push the envelope. In addition to finding some Queen Anne, STL files (Pretty Small Things) on line, I photographed furniture in our house, scaled and drew it in SketchUp and then printed it. I even was able to reproduce the table tops photographically by using images of our actual furniture. Unfortunately, without removable walls, you can't

see much from the outside. It's especially disappointing not seeing that fabulous staircase. But I know it's there. Gluing the crown molding to the walls was a very time consuming and at times, frustrating experience. I had to trim the wall paper so the various glues I was attempting to use worked. The molding, like many flat and thin 3D printed objects, warped putting strain on the glues to hold it in place. All of this was an exercise in futility since the ability to see the ceilings is greatly limited in the finished structure. I took a lot of pictures during construction and blogged all of it on O Gauge Railroading's forum so others could see it as it went in, but once buttoned up, only the plastic characters inhabiting the mansion can see if, if they're able to look up that is.



Above: Mansard roof skinning require additional fillers behind the skin flanking the window cuts. The balsa was very fragile.

Below Right: Testing the fit of all the Mansard window frames. These were all 1-piece SD printed parts including the delicate mullions.

Creating a reverse curve Mansard roof was right up there with 3D printing. Using laser cut formers, I skinned the curves model-airplane style with 1/16" balsa sheeting soaked in vinegar making it more pliable. My material specs to Rail Scale were wrong with formers being cut from 1/64" ply instead of 1/8" MDF. The resulting narrow gluing surfaces were weak and needed reinforcement all of which took lots of time and fussing.

The Mansard curve made it difficult to hold up the window component and trace its shape for cutting the openings. I had to carefully hold the pencil parallel to the window to project the window's edges to the skin, start cutting at the curve's bottom to make a gap, move the component into the gap and make additional tracings until I got the cut completed. It was much easier to cut these openings in SketchUp, but hey... that's just a drawing.

The exposed balsa edges kept breaking. I eventually reinforced these with thick pieces of balsa or aircraft ply. The skins were covered with Rail Scale Models laser-cut Victorian shingle mix (straight and fish scale). For flashing, I used adhesive-backed real copper tape.

This tape is normally used to install flat surface wiring in doll houses, and besides making very convincing flashing, I use it to do all of my surface-mount LED lighting circuits. I pre-treated the copper with JAX metal browning and patina solutions. Time consuming, but it adds another level of authenticity.





Above: The completed model showing what it would look like if Hopper's painting was real.

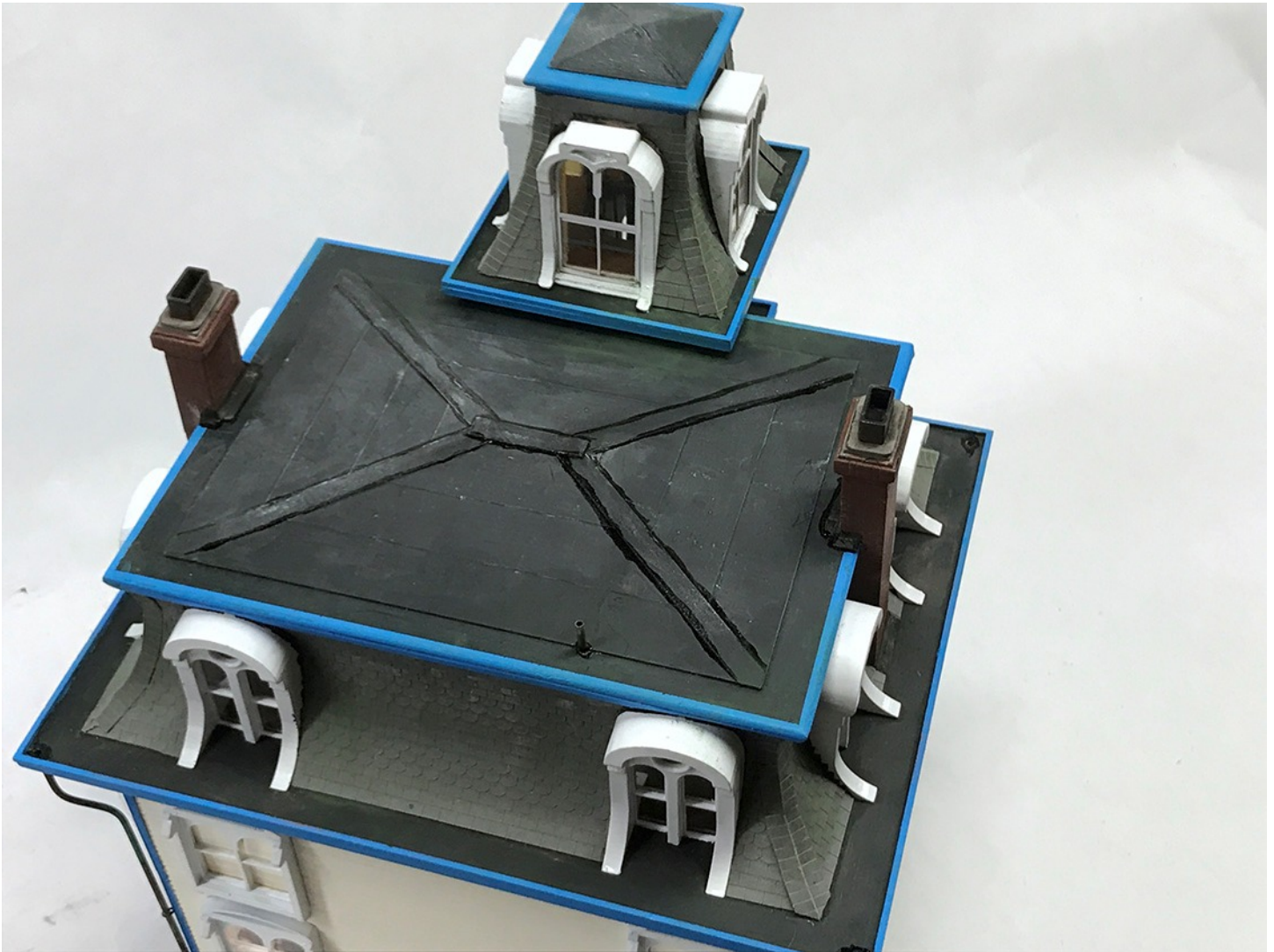


Left: Real chemically-aged copper foil is used for all the flashing. Shingling between the windows and chimneys was tedious.

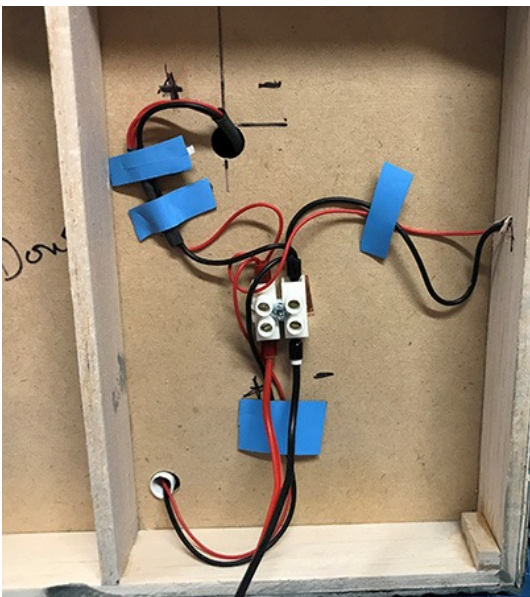
The top roof was created using triangular laser cut pieces that were produced from projections created in SketchUp. There's a slight pitch up from the center which creates a group of four triangles that are no longer exact right triangles. They were covered with copy paper strips with NATO black (a very dark, dark gray) with tar strips highlighted with gloss black.

Lighting

All the lighting is LED except for one grain-of-wheat bulb cupola light. I use CL2N3 LED driver chips to eliminate current-limiting resistors. My 12 vdc can supply can power 3 LEDs in series with full brightness. The House has four parallel circuits tied together in the Chooch foam foundation



Above: Drone's eye view... the roof is copy paper strips painted NATO Black with gloss black "tar" sealant.



I built a chandelier from fiber-optic filament, a 3D printed spindle and ring, and an embedded 2mm LED. Bondic UV curing resin formed the "bulbs". It's a bit over-scale due to the limitation of the relatively large curve radius before the fiber optic plastic strand broke. I built it just to see if I could. To simulate the actual bulbs, I made spherical droplets of Bondic UV curing resin. The central column was another 3D printed part with a hollow center whose diameter would accept a 3mm warm-white LED. I had to add a lots of gold paint to everything but the bulbs to get it them to glow without light leaking through all the other surfaces.

Left: Four circuits are paralleled into one connector and then lead to the below-the-layout DC bus. I joined the circuits in the ferrules that go into the Euro-style junction block.

Final Assembly

I glued two adjacent walls together early in construction and then joined these two subassemblies later to form the building's envelope. I glued in the 1st floor, but didn't glue the partitions to the outer walls. It wasn't necessary. I spent an inordinate amount of time wallpapering and trimming the back sides of the exterior walls.

The result of all this effort is a convincing rendition of the Hopper painting. It has turrets, corbels, Mansard roof, fancy windows, and a great big porch; all the trappings of a great house.



A view into the House showing the second floor bedrooms and the central stairwell. The windowless back center served as the wiring trunk bringing the ceiling and attic lighting leads down to the basement.

Site Prep

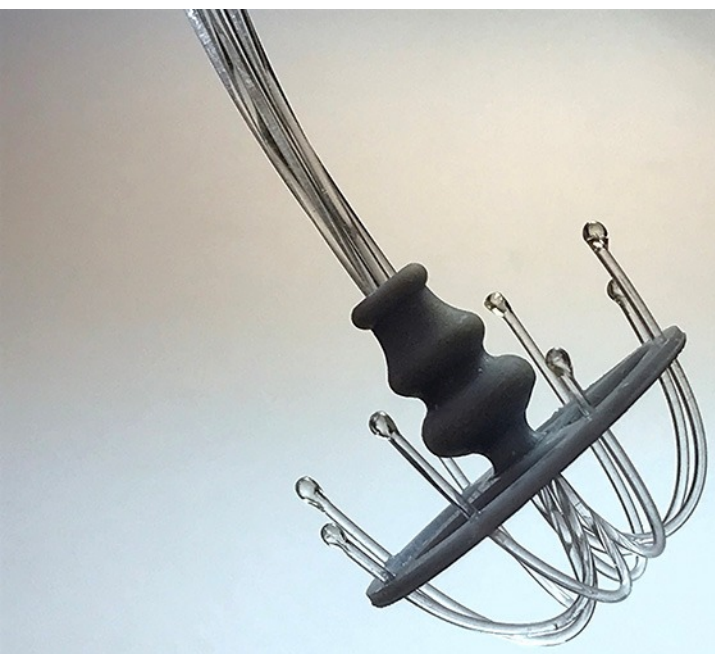
The site was the previous location of my Bar Mills Idaho Hotel. It was a lovely model, but didn't compare to this piece. I relocated Idaho to the front main street with its back facing the viewer. I added a parking lot and a custom-made, LED lit, parking lot sign. I later sold Idaho and will sell a couple more of the earlier craftsman kit structures to make room for more custom-designed and built structures, which are more gratifying to me.

The Idaho site was not deep enough to contain the House... I had to add more material to the back slope to create level ground closer to the RR tracks that ran behind. I then added a retaining wall made of Ross Custom Switches pre-cut railroad ties. I've used these before for the same purpose behind the Bernheim Distillery.

After the Scultamold cured (a couple of days) I painted, ground covered and did the final landscaping. I had purchased a fancy wrought iron fence also in Rail Scale's laser cut catalog and also pre-made garden flowers from Scenic Express. I had populated the interior with figures from various sources. I have a man and his older son sitting at the dining room table and four people conversing in the parlor. With the addition of the fencing and flowers, the building was complete.



House Final Rear View: To make a level back yard, I had to added a lot of filler including Sculptamold supported by a Ross Custom Switches railroad tie retaining wall.



Detail close ups.



With site placement and improvements, the House by the Railroad forms a visual centerpiece in the middle of the layout. It gives the viewer an unobstructed view of my version of Hopper's Masterpiece.

“Punta Cayo”

A Tropical Railroad of a whimsical nature

Installment Two, It's about the Details

By **Jon Stetz**

Editors Note: If you missed Part one [please click here](#).

A considerable amount of progress has been made on “Punta Cayo” these last few months, and I expect I may be finishing up sooner than later, but as we all know “NO” Model Railroad are ever really done. Even this railroad has the means for extending the track and railroad, out behind the boat engine repair building.

Figure 17. In the first installment we left off with all the track work complete, gaps between the rail sections filled, road and footpath crossings done, and switch stand support bases were added. Once all the ballasting was completed, the tracks were cleaned and tested and the actual switch stands were installed.

Figure 17



Figure 18

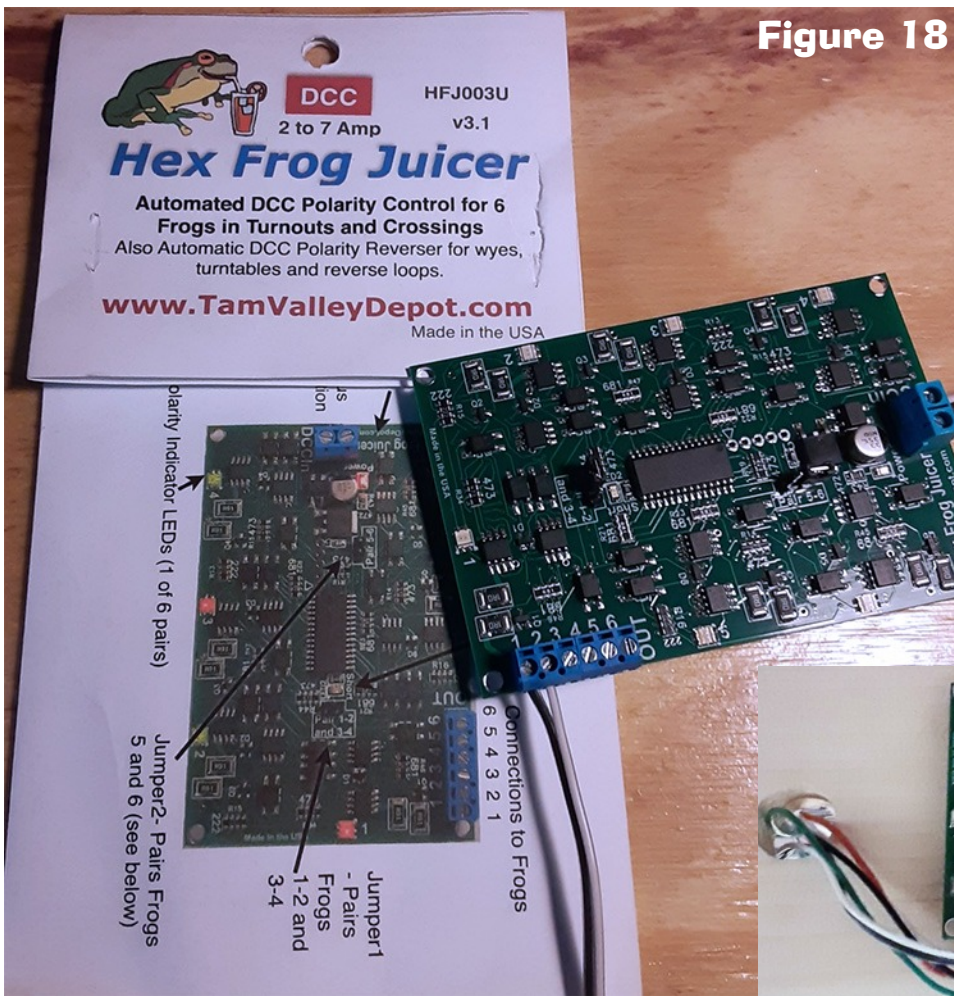


Figure 18. As much as I wanted to avoid going “high tech” with this railroad in electronics for operations, I decided to add a “HEX Frog Juicer” from “Tam Valley Depot”. I had already pre-wired the switch frogs before adding them to the layout, so adding this was not difficult. Figure 18A.

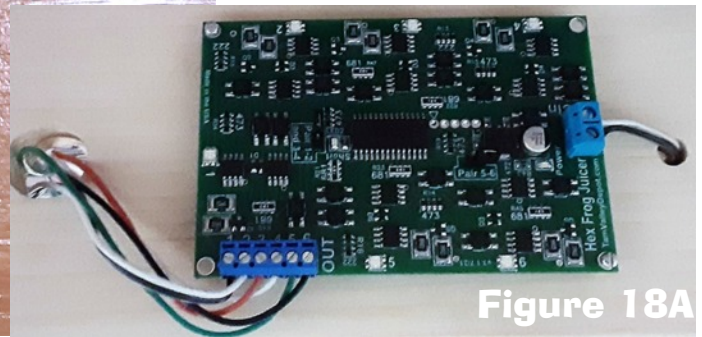


Figure 18A



Figure 19

Figure 19. With a fully functioning, tested, and running railroad, I was now able to move forward with the scenery and detailing of all the foreground areas, making sure to leave gaps in the scenery to access all the switch ground throws and any possible derailments. Figures 19A-C.

Figure 19A



Figure 19B



Figure 19C



Figure 20



Figure 20A

Figure 20.
Every once a while I stopped and took a grounds eye view of the railroad to see if I was capturing the feel of the tropics. Figures 20A-B.



Figure 20B



Figure 21

Figure 21. No matter what hobby or scale someone works in, details make all the difference in creating “Realism-In-Miniature”. Figures 21A-C.

Before, a barren scene with the acceptance of the Norfolk Island Pines, handmade by the friends at “McKenzie Brothers Timber Company”.



Figure 21A



Figure 21B

Figure 21A, after, with that lived in look. All that's missing are some workers.

Figure 21B, before, just rocks, a boat launch, and a few buildings.



Figure 21C, after, and it's all about the details.



Figure 22. Scenery and detail work not only progressed on the Harbor side, but also on the plantation side of the railroad. Palms were starting to be added to the last hill side, an oil and coaling station was added by the water tank and the ground cover was completed while waiting for the planting of the banana trees. Figures 22A-C

Figure 22



Figure 22A

Figure 22A, site of the future oil and coal bin.

Right: Figure 22B. Built from the scrap box the oil and coal bin shed. An operating light pole will added later.

Below: Figure 22C, Future Banana field to the right side of the tracks



Figure 22B



Figure 22C

23. Unfortunately, I was a bit complacent about taking photos of every project on the railroad. The two smaller piers in the harbor are a prime example of that, but the larger pier fared a bit better in its construction, well almost. (23A-D) (Next Page)



Figure 23



Figure 23A

Figure 23A, just like building regular railroad trestle bents, I made a jig and prefabricated seven pier bents before doing the final assembling on site.

Figure 2B, cross bracings, NBW castings and rock rubble where added.



Figure 23B



Figure 23C

Figure 23C, digging into my plastic scrap box, I built up the concrete section of the pier with Evergreen Styrene's, more Carmel Apple sticks and heavy threads along with NBW castings.

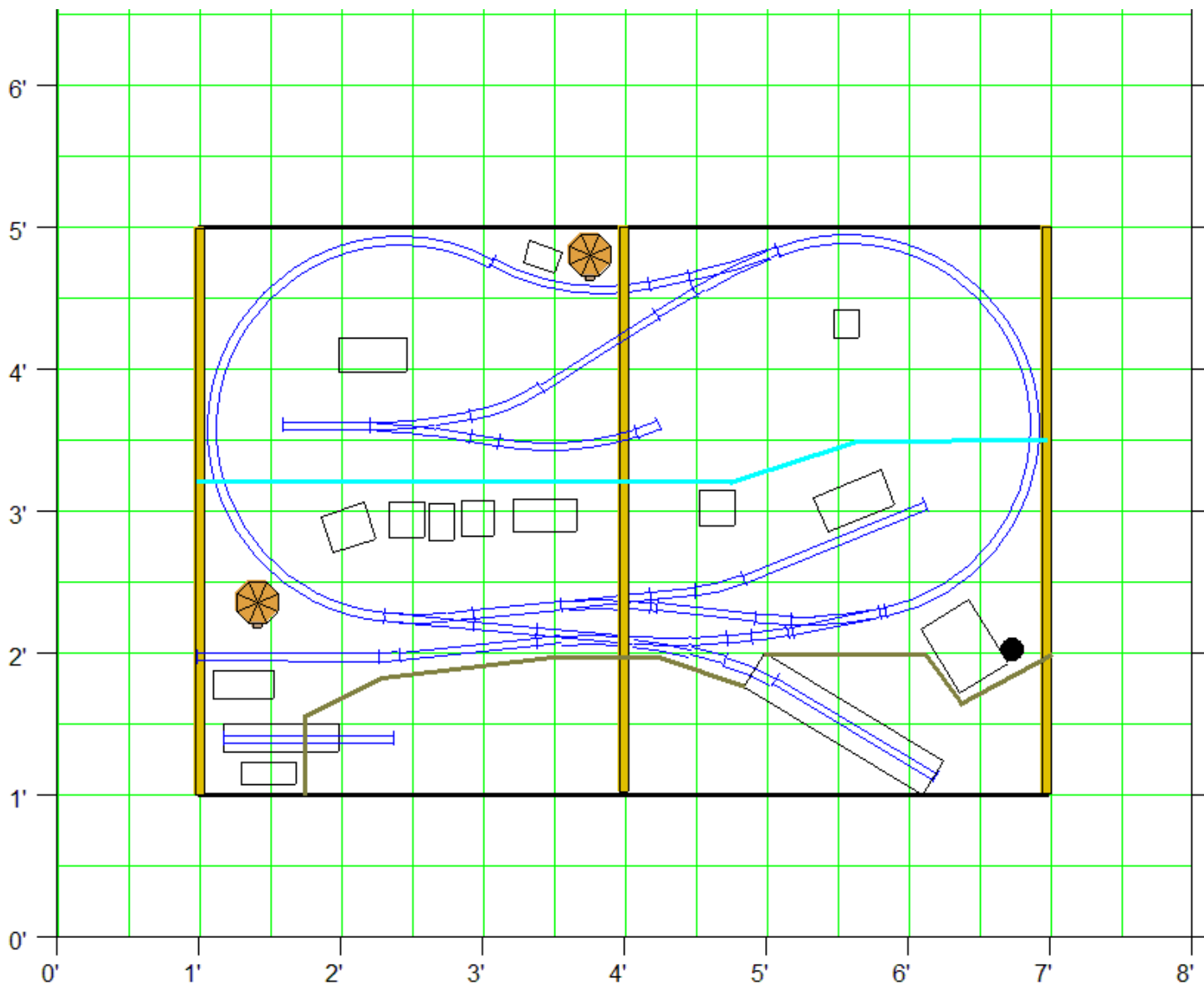
Figure 23D, there is still a bit of work to be done here on the pier. From adding the track, to a walkway, to water access, details, and a loading crane.



Figure 23D



Well, that is about it for this second installment of the building of “Punta Cayo”. I’m going to close out with a few more close-ups photos of the railroad.



Drawn with *XTrackCAD*

See Ya all, down the road.

TO BE CONTINUED.....

Excele Lubricants distributed by JT's Mega-Steam

ExceleTM Lubricants

- Designed specifically for model railroads
- Oils are plastic compatible and conductive
- Convenient 4-pack; or purchase individually
- Different viscosities for specific applications
- Usage guide provided
- Featuring precision needle point applicators
- Developed, tested, and packaged in the U.S.A.

MEGA-STEAM • 11 Out of Bounds Road
Palmyra, VA 22963 • email: jkmegasteam@aol.com
For Visa / MC orders and info:

www.megasteam.com (434) 589-2660 • 10am to 6pm EST

Paint Sets

Currently Available

10403: Weathering, Set #3

TCP-009 Grimy Black
TCP-093 Oxide Brown
TCP-173 Weathered Iron Oxide
TCP-348 Weathered Gray Wood
TCP-357 Burnt Iron
TCP-359 Weathered White

October, 2022

10404: SCENERY & DIORAMA, SET #1

TCP-400 Matte Rail Brown
TCP-401 Matte Dirt
TCP-409 Matte Railroad-tie Brown
TCP-411 Matte Signal Red
TCP-432 Matte Concrete
TCP-434 Matte Aged Tar

Come Visit Us at Our New **YouTube Channel** at:
Tru-Color Paint. We are also on **Facebook** &
Instagram. Watch For Our Facebook Group Soon.



**Tru-Color
Paint**

Paints & Accessories

When Only the

TRUEST

Color Will Do!

① Highly Saturated

Precise & Accurate ②

③ Larger Content Cans

What We Have Available ④

P.O. Box 74524

Sales Phone: (714) 488-9779

Phoenix, AZ 85087-4524

Info Phone: (623) 551-2548

website: trucolorpaint.com

e-mail: tru.colorpaint1@yahoo.com

Along the Railroad

A North American Rail Journey
1958 - 1978



Howard Serig

with
Kent Loudon

Along the Railroad features the exciting and unique railroad images of photographer Howard Serig who has captured the special time "when trains were really trains!" Many have claimed it to be "The best railroad book out there!" To see a video preview of the contents of the book please click on the image above. This is a limited edition hard cover book containing 222 pages of content. Copies are available for \$49.95 plus actual shipping. Orders may be placed by contacting the seller below:

kjkriigel@aol.com (mention book title) or by phone: 434-589-2660.



Click and subscribe to
our YouTube Channel.
We have show videos,
layout tours, how to
videos based on
articles and lot's more.

Please check it out and
hit that subscribe
button so you never
miss a new release.

LIVESTOCK BUSINESS FOR THE MOUNTAIN ELECTRIC AND YOUR LAYOUT

By **George Paxon**

When the subject of stock cars comes up, our minds usually conjure up visions of cowboys and western railroads. But eastern roads had reasonable livestock handling business as well at one time. There was a big stockyard in Pittsburgh on an island in the river that received many carloads of livestock. Next door was a large meat processing plant that slaughtered and packed the meat for local and area consumption. Packed meat was trucked all over the city. Meat was dispatched from this packing house by private reefers to other places nearby as well. Look at Photo 1 and you can see quite a sea of Pennsy stock cars that certainly indicated a big livestock business on this eastern road.

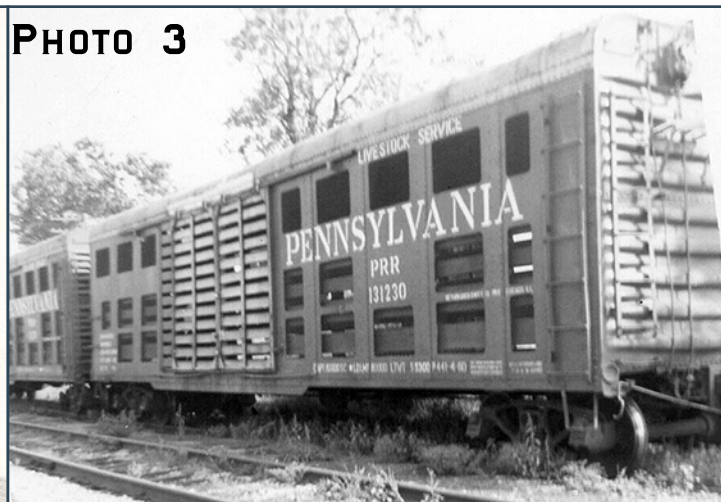
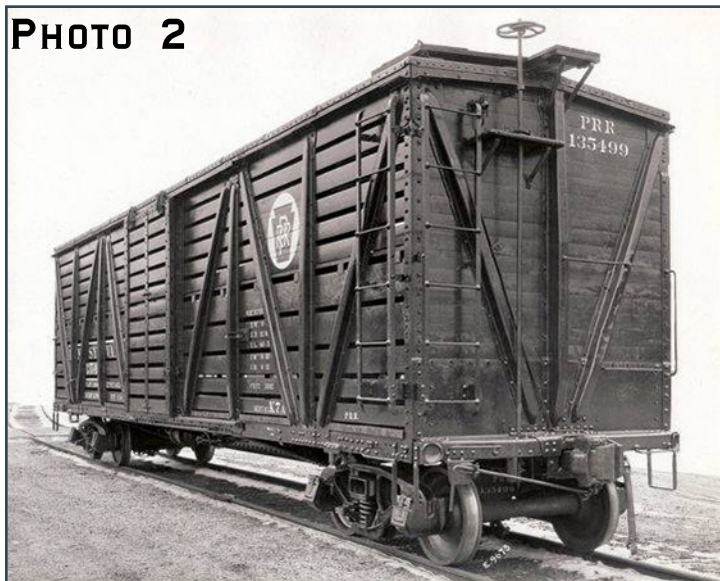
Livestock was big business on almost all railroads once. Moving livestock required a bit more work, and created more operating challenges than moving coal. Time was of the essence to avoid livestock losing weight and, therefore, value. Stock trains received priority treatment as a result. And by federal rules, livestock had to be periodically de-trained, watered, exercised and fed. This also provided motivation to move stock trains quickly to get them onto another railroad before the mandated time for de-training: if you passed on the stock cars, you also passed on the responsibility and cost of de-training the stock. The service would have cost more to provide, but I believe the rate for such freight was better than average as well. I suspect changes in the locations of stock raising and loading areas, the locations of slaughterhouses over the years, the move to better roads and truck transport, and the introduction of refrigerated trucks, all contributed to the loss of the railroad livestock business after the 1950s.

I recall we did have other small slaughterhouses in the southwestern Pennsylvania region I model. I remember a tornado hit one night during a bad storm when I was young. It went through a timber slaughterhouse up toward Washington, Pennsylvania. In the morning, there were bits of wood and cows everywhere in the surrounding area. It looked like steaks were growing on trees.

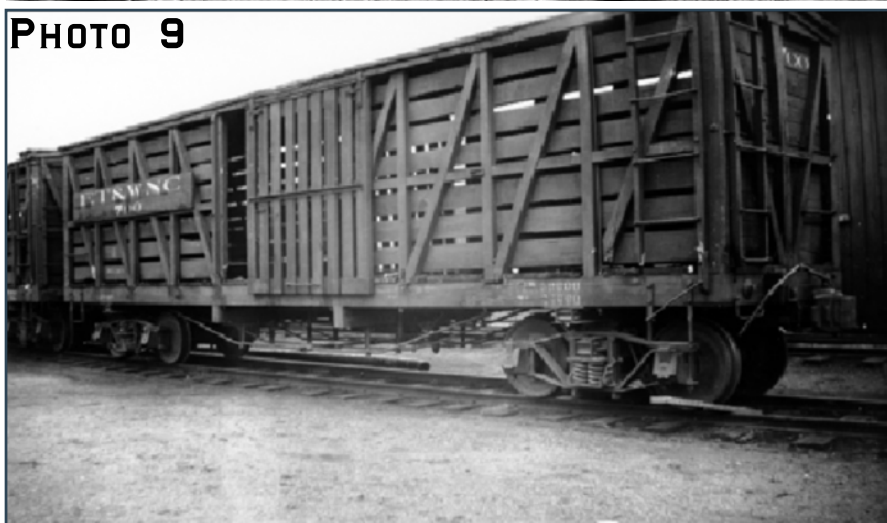


To have a livestock business on your layout, you need three things: (1) a stock car, or two, or three...; (2) a stockyard and/or a meat processing plant; and (3) an operating plan to move stock cars realistically.

Both the Pennsy and B&O in my area of interest had several series of stock cars as can be seen in Photos 1 through 5. Both these roads built or bought stock cars into quite modern times, and both provided livestock transport for many years. Photo 6 is of a nice model of a rather modern B&O car. The Pennsy made some modern stock cars by cutting holes in the sides of some automobile box cars that were obsolete for automobile parts service. This rebuild converted X32B boxes to K9B stock cars in the 1950s. I believe Photo 3 is a K9B, but I can't read the car classification to confirm that.



Stock cars did operate on traction lines, too. Photo 7 is a builder's photo, from the AC&F archives in the John Berringer collection, showing a car built in 1923 for the Interstate. The Indiana Railroad and other Midwest traction roads also had a substantial livestock hauling business and provided such cars for this service. And several Midwest traction lines had powered stock motors. These were just self-powered stock cars.



The train crews operating them must have loved the extra smell that came with those runs. I would imagine they dreaded stopping for a red signal as the smell would really catch up with the motorman's compartment then. These motors could pull additional stock trailers, and operated as trains into a large stockyard in downtown Indianapolis at one time from several rural locations in Indiana. Photo 8 shows a stock car from the Pacific Electric, a large California traction line. This car was a full-sized steam road car and probably provided to the PE by Southern Pacific from its car fleet since the SP was the parent road.

Traction cars were often narrower and of lighter construction than steam road cars of the same era. Larger narrow gauge cars would then make reasonable prototypes to model for standard gauge traction application. Photo 9 shows such a 3-foot gauge car on the East Tennessee & Western North Carolina Railroad. It was 34 feet long inside, so about 37 feet

over the strikers. A model of it could make a reasonable and interesting traction stock car. Unfortunately, my “to build” list can’t handle anymore projects at this time. Hopefully, one of these days... The Washington and Waynesburg, a narrow gauge Pennsy subsidiary that ran near where I came from, had stock cars; but, they were quite short and really too small for realistic standard gauge traction models.

I had a half-built San Juan kit of a 34-foot D&RGW stock car left over from my narrow gauge days. I reworked this kit and finished it for use on the ME Ry. The radial ends were added to the car to increase its apparent length and make it more suitable as a standard gauge traction car. Photo 10 shows this car when completed. I have another untouched San Juan kit in my hoard, and should be able to build it up for use on the Mountain Electric as well one of these days to help out with livestock movements on the layout. I had quite a few D&RGW, RGS and C&S stock cars on the old layout, and sold them with most of my narrow gauge stuff when making the switch to traction. In hindsight, I probably could have re-purposed a few of these although they were short cars at 30 feet. [In a previous article in OSR](#), I provided a description and sketches for fabricating radial ends for a square ended narrow gauge car to both add length and make it better able to accommodate radial couplers. This same technique was used for my ex-D&RGW stockcar conversion and would be an appropriate addition to any narrow gauge car to make it better look the part for standard gauge traction use.

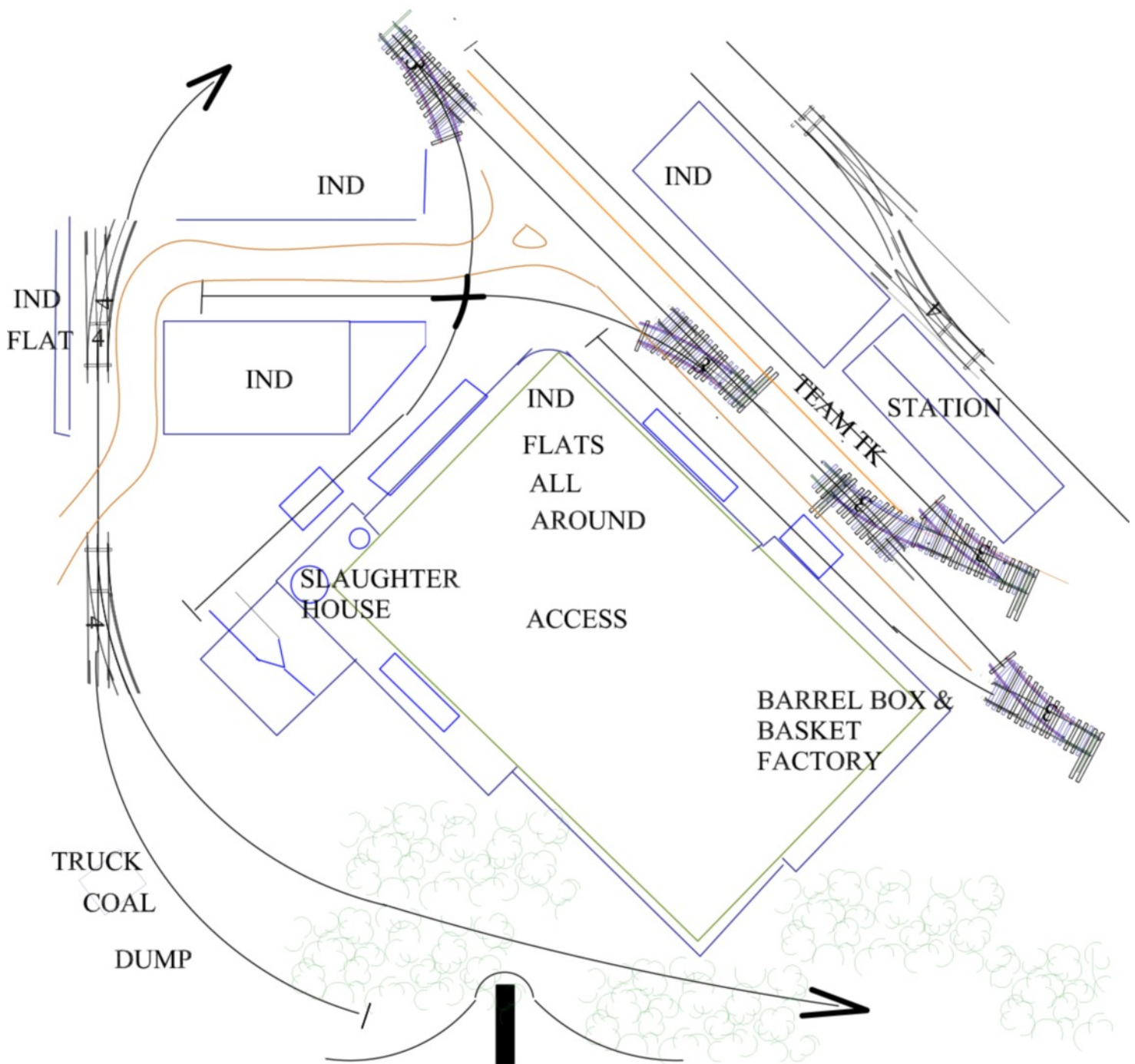


You can easily build a meat processing plant as a lineside industry. This need be nothing more than a rectangular building with a small stock pen for holding the livestock when they are unloaded and awaiting their trip up the chute and into the slaughterhouse. Such a basic building can be made to better look the part by adding a water tank, smoke stack and a cooling tower to the roof. Stock cars can then be moved from the interchange with another railroad to the meat processing plant. Photo 11 is a nice shot of someone’s model that certainly would make a nice meat

PHOTO 11

processing industry with only the addition of a small stock pen for unloading livestock. You don't even need to model the structure as a rectangle. Due to limited space, I won't. I have access holes planned for the towns of Scottdale and Somerset. A meat processing industry will be located in one of these towns around the access hole. I am thinking that I could model two walls of the building at a corner of the access hole with the access hole behind those walls. I need just enough building depth to make room for the roof details needed to make the building look like a slaughter house. This will give me a bit more of a 3D effect, but will use little layout real estate. I will need to find space for a small stock yard next to the building to accommodate the arriving livestock though. Figure 1 shows the tentative plan of Scottdale and the slaughterhouse layout.

Figure 1.
PLAN FOR SCOTTTDALE



An alternative is to have at least one stockyard on your layout to load the stock cars. This will allow you to move loads of livestock from the stockyard to an interchange for forwarding to an off line meat processing facility.

Ideally, you could have both a stockyard and a meat processing facility on the layout. This multiplies potential car moves substantially as stock can come via interchanges from off line to the on line stock yard on their way for summer pasturing, from off line stockyards to the on line meat processing facility, from the on line stock yard to both the on line meat processing facility and such off line facilities. And of course, the meat processing facility will load reefers with dressed meat for delivery on and off line. A meat processing facility can generate other traffic such as periodic car loads of hides for movement to tanneries, too. A carload of offal could be dispatched regularly. Similarly, a periodic tank car of meat by-products might be shipped for production of soap, glue and other products. Another outbound load would be lard, as almost all meat processing facilities produced this as well. In addition to receiving livestock, a meat processing facility would receive a regular car load of coal for the boilers needed to produce steam that operates meat processing machinery and the refrigeration plant. Packaging materials and an occasional carload of machinery might come in box cars as well.

Another move can be livestock in transit that is offloaded at the stockyard for mandatory water, rest and feed, too. After at least 5 hours of rest, the livestock can be then reloaded to continue its journey. Between offloading and reloading, the stock cars can be cleaned as described below.

A small stockyard will prototypically mostly be a seasonal industry. A meat processing facility was not as seasonal and could be serviced all year. Small country stock yards often dispatched livestock during the fall rush to larger stock yards that functioned as feed lots where livestock remained for quite a while, were well fed, and fattened. Livestock at these big feed lots could then be moved regularly in smaller shipments to drip feed the meat processing facilities.

Often during livestock moving season, it was quite a rush. Cars, or even trains, one after another, were cycled between the stockyards and destinations to clear the backlog of livestock needing transport. In western areas, livestock could be moved twice a year. In the spring, the livestock was often moved from ranches to high pastures that were not usable for grazing in winter due to snow. In the fall, before the snow became a problem, the livestock in the high pastures was loaded into stock cars and either moved back to lower pastures for the winter or toward meat processing facilities.

Photos 12 and 13 are of two stockyards on my old narrow-gauge layout. In one photo is a model of a 30-foot D&RGW stock car that probably could have been reworked for use on the traction if I had not sold it.



We have yet to make it to the town of Scottdale on the new traction layout where we will eventually encounter our first stockyard site. We will get to Scottdale shortly. And, we probably will have another in the mountains at or near Somerset as well. A stock yard is simple to build as it basically can consist of only a bit of fencing with a crush and chute to trackside. It can fit into most any odd shaped area along the track. You can make it more elaborate with feeding shed, etc., too, if you have the space. We plan to have two stock yards and a meat processing plant on the Mountain Electric. And since we have active interchanges with five other lines, we can move livestock, packed meat and other products between all these on a regular basis.

The need for cleaning and bedding the stock cars can justify a stop at a service track while moving stock cars from an interchange or a car storage track to a stockyard for loading. Cleaning can also apply between mandatory unloading and reloading for water, feed and exercise. This cleaning is, operationally, much like routing a reefer via the ice house. It just smells heaps worse. Cars might be steam cleaned and then disinfected with a lime solution, but usually contract workers just shoveled and broomed the cars out. Usually clean bedding of sand, cinders or sawdust, about 2 inches thick, was put back into stock cars after cleaning with the same workers doing the shoveling needed to spread the new bedding.

Stockyards could function as mini feed lots, so a car of hay or grain could provide food for the livestock fattening process. Bedding might also be delivered in a gon in anticipation of arriving cars that would need cleaning and bedding. In dry areas, a car of water might be spotted at the stock pen to ensure sufficient water for livestock during their layover. Water shortage is not just a western desert problem. The East Broad Top Railroad in south central Pennsylvania converted quite a few steel box cars to water cars to carry water for the boilers of the Robertsdale coal mines during the heat of summer when the local water supply was insufficient. This could yield additional traffic for your layout from the livestock industry.

Another car movement I thought of was the residue from car cleaning. It could be loaded into an old gon and forwarded to either an on line or off line fertilizer factory.

Once loaded, stock cars should get priority treatment and be moved quickly toward their destination. Often a locomotive spotted the cars during loading and then immediately headed off with the train when loading was complete. Maybe the car or cars were just passed on to a connecting railroad at an interchange.

Your operating scenario can have a periodic "stock season" where the livestock movements are heavy and seriously stuff the normal flow of routine traffic for a few operating sessions. This is much as it was on many prototype roads. You will need a few stock cars to make this work. And, I guess you could also have a drover's car to accommodate stockmen who often traveled with stock trains to handle the livestock. I think this was mostly a western thing, but you do have a modeler's license. Some roads constructed special cars for drover service, but others just used an old caboose or passenger car. Usually the drover's car was placed just behind the locomotive and ahead of the smell.

Adding livestock and related traffic and the essential allied services and facilities can result in a substantial increase in layout operations and is worth considering.

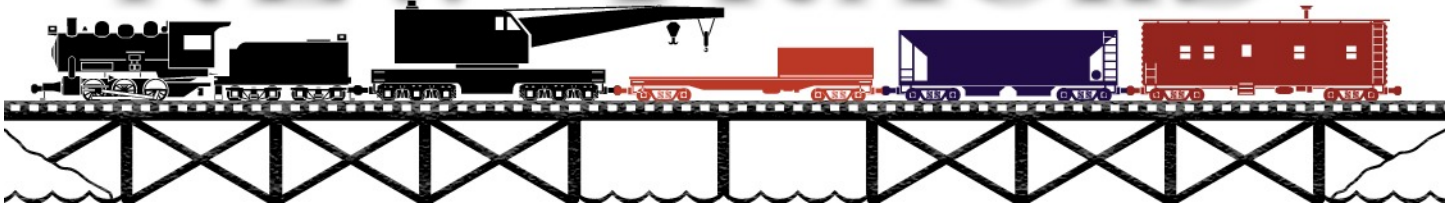


Did you miss an issue of The O Scale Resource? Forgot to look? Well, sign up here and never miss another exciting issue. We'll send you an Email when the latest issue is loaded to our site. Don't

worry, we won't sell or post your Email address to anyone!

[CLICK HERE TO SIGN UP TODAY!](#)

NEW TRACKS



Mentor Definition: A Trusted Counselor or Guide

By Contributing Editor Jim Kellow MMR



MODELERS WHO MAY BE ABLE TO HELP YOUR MODELING!

Where Mentors Help Modelers Build



**Association of Professional Model Makers
Ambassador: Jim Kellow MMR**

I am honored to have been appointed, two years ago, to be the Ambassador for the Association of Professional Model Makers (APMM) to the model railroad community. I believe the skills and knowledge acquired by model railroaders make us

well suited to consider exciting careers as professional model makers. I encourage all modelers to visit the Association's Website: <https://www.modelmakers.org/> and find out about how the Association can be of benefit to you and maybe help you find a career as a professional model maker.

If you are a young modeler who wants to investigate a possible career, or a modeler who just wants to meet professional modelers who could help you learn new modeling skills and acquire knowledge of new technology, membership in the Association is worth your consideration.

I have met some outstanding people and been able to discuss modeling subjects with some of the best model makers in the world. If you love creating and building models, I know the APMM can be of benefit to you as it has been for me and my modeling.

If you have any questions or just want to talk about my experiences and get more information about the APMM, I will be glad to talk with you. My email is jimkellow@newtracksmodeling.com. You never know where a little conversation may lead.

"New Tracks" Modeling Announcements.

Thanks go to all the volunteers who helped "New Tracks" become successful.



On July 18, 2022 we reached a milestone by having the 1,000th modeler subscribe to our [YouTube Channel, New Tracks Modeling](#).

Thank to everyone for your interest and support of our live Zoom shows. We have some exciting ideas for future shows and a lot of outstanding companies and modelers for you to meet and learn about their modeling philosophies, techniques, and skills.

“New Tracks” currently has two different shows which introduce talented experienced modelers who could become your mentor, or at least give you ideas to improve your modeling and the confidence to try new techniques.

We have something new every show, and try to make all our shows a unique, enjoyable learning experience one small step at a time. Make sure you don’t miss any by subscribing to our YouTube channel, New Tracks Modeling, and clicking on the bell. I think you will be glad you did.

Our Website: newtracksmodeling.com provides the latest information about our upcoming events and provides Zoom links to website subscribers by email.

If you are not already a subscriber, I hope you will become one and help us by sharing your ideas and thoughts about what content we should offer on our shows and guests you would like to meet. Everyone is most welcome.

Also, please tell your friends about us so they can also join in the mentoring and fun of our shows. Thanks in advance for your help. Word of mouth is our best way to advertise our shows. Please help us.

Door Prizes

Periodically we have a door prize offered by a manufacturer on our shows. Alan Rogers owner of [New Creations Victorian Structures](#) offered one of his kits, a Southern Pacific Handcar Shed that once stood at Woodford, California, in the winner’s choice of either HO, S, or O scale on our July 20, 2022 show. The winner was John Garaty in Queensland Australia. Congratulations John! We look forward to seeing your completed model on one of our future MY BUILD segments. Thank you Alan for offering the door prize.

We hope other manufacturers offer a door prize to help promote our shows and your company. Please contact me at jimkellow@newtracksmodeling.com to schedule a date for offering your door prize. Thanks in advance for your help.

Our new show: Modeling Lifestyles

Sponsored by Narrow Gauge Modeling Company (NGMC).



June 2, 2022 “New Tracks” started a new bi-weekly live series called: Modeling Lifestyles. It will be available to watch live on our YouTube Channel, New Tracks Modeling.

Each show will introduce you to a different featured guest modeler along with some of their friends. They will discuss their modeling, share stories of their modeling experiences, and opinions about the hobby.

[Click image to view Narrow Gauge Modeling Company’s video introduction.](#)

On July 14, our quest was Lou Sassi with his three friends, Jim Murphy, Kip Grant, and John Burchnell. On July 28 Cheryl Sassi, aka “The Arborist”, was our guest with her three friends, Nancy DeGhetto-Murphy, Phillis Baker and Jack Dziadul. August 11th was Tom Farrell, with his three friends, Al Judy, Dan Rousseau, and Dan Wolschon. You can see all of these interviews on our YouTube channel New Tracks Modeling.

Some of these modelers you may have heard about and always wanted to meet. Others you may not have heard about, but I know you will be glad to have an opportunity to meet and chat with them.

Please subscribe to our YouTube channel: New Tracks Modeling, and ring the bell to get prior notification of the shows and be able to join us live.

We have many more great modelers scheduled who are making real differences to our great hobby in these early years of the 21st Century. For a complete list of the modelers and their friends who will be on our shows, please visit our website at newtracksmodeling.com.

Don’t forget to subscribe to our [YouTube channel, New Tracks Modeling](#), and ring the bell to not miss any of these interviews.

HELP WANTED



The image shows two signs side-by-side. The left sign is red with white text that says "HELP WANTED". The right sign is yellow with a black border and a black and yellow striped top and bottom. It features a red circle with a black silhouette of a person digging, and the text "UNDER CONSTRUCTION" in red. Below the signs, on a black background, is the text "Volunteer and help the New Tracks Team" in white. Underneath this text is a list of volunteer roles in two columns.

HELP WANTED

UNDER CONSTRUCTION

Volunteer and help the New Tracks Team

- Show Producers
- Web site management
- Web Site Content
- YouTube Operations
- Promotion
- Video Editing
- Build Along Modelers
- Segment guests

If you watch our “New Tracks” shows, we need you to be part of the team that makes them happen.

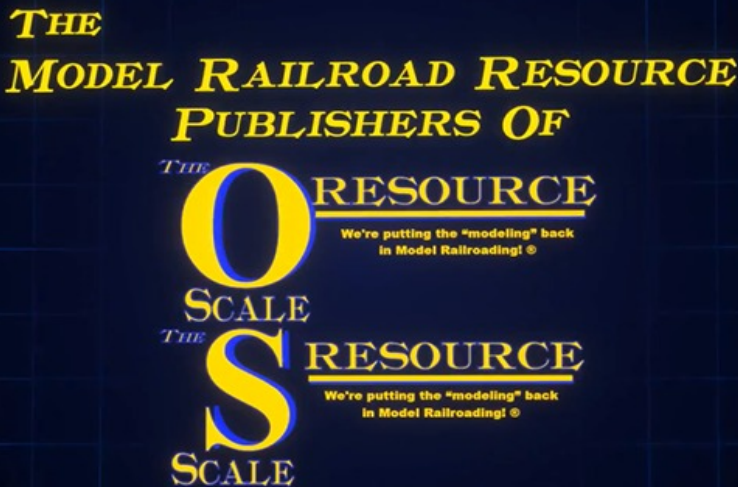
We are fortunate to have as many volunteers as we do who make our programs possible. But as our show’s audience continues to grow and the types of segments viewers ask for increase, we are always in need of more volunteers. Please help us keep the shows going.

Everyone who watches our shows has a contribution to make. Offer your help in participating as a mentor, or help making and producing our Zoom and YouTube shows, or help with our website and digital marketing, etc. Any amount of time and help you can provide is greatly appreciated and definitely needed. Contact me at jimkellow@newtracksmodeling.com and let’s discuss.

“NewTracks” MY BUILD

Sponsored by Model Railroad Resource LLC.

These are the photos modelers shared on our recent July 20, 2022 My Build. Each of the participants has an email address included and would welcome your comments.



The logo for The Model Railroad Resource is displayed on a dark blue background with a grid pattern. It features the text "THE MODEL RAILROAD RESOURCE" in yellow, followed by "PUBLISHERS OF" in white. Below this is a large yellow "O" with "SCALE" in white to its left. To the right of the "O" is the word "RESOURCE" in white. Below the "O" and "RESOURCE" is a smaller yellow "S" with "SCALE" in white to its left. To the right of the "S" is the word "RESOURCE" in white. At the bottom of the logo, the text "We're putting the 'modeling' back in Model Railroadin!" is written in white.

THE MODEL RAILROAD RESOURCE
PUBLISHERS OF
O SCALE RESOURCE
S SCALE RESOURCE
We're putting the "modeling" back in Model Railroadin! ®

[Click image to view The Model Railroad Resource LLC video introduction.](#)



Alan Rogers – O scale –
Scratchbuilding a barn using
common household products.



Greg Cassidy – HO scale –
Microscale Models Pine Creek
General Store. Work in
progress.



Gary Shurgold – HO –
Unknown manufacturers
kitbash in styrene. Pickle car
converted for use on the
road. Used parts given to him
from the cartoonist Milt
Moore.



Bob Farquhar – HO Scale – Bar Mills Clark's Wood Products. Work in progress.



Jeff Jordan – On30 – Altoona Modelworks Water Tower. Converted to narrow gauge.

Our monthly “New Tracks” MY BUILD segments provide opportunities for viewers to have their work featured. They can show their latest project, discuss their modeling technique, or a new tool they found, or give a tip they learned that helped their modeling.

Modelers can use this opportunity to engage in discussions with the “New Tracks” modeling community. No judging is done, only helpful discussions. Join us to see and discuss your modeling with other model builders.

The show segments are sponsored by Amy and Dan Dawdy, owners of [The Model Railroad Resource LLC](#), the publishers of this magazine. The MY BUILD is moderated by Chris Course, an excellent modeler and owner of [Conowingo Models](#).

If you want to participate in our next monthly My Build send your



Chris Coarse

model photos with a brief description and your name to Chris Coarse at railrunner130@hotmail.com. The next MY BUILDS are scheduled for, September 21, 2022 and October 19, 2022. This is the time to plan a projects you want to share at these upcoming events.

Chris will incorporate your photos into the show. When your photos come up, you will have time to share your experiences from building the model with the “New Tracks” community. I believe each of us has unique modeling talents and skills, and showing your modeling can be a great teacher and motivator for other modelers. Sharing your modeling is a significant part of mentoring. Please participate in these programs which are designed to help modelers improve their skills.

New weekly Wednesday Show Segments

Women in Modeling



I looked for and found a talented modeler and hobby business owner to lead this program. Her name is Kristen Kemick. She and her husband David own a 3D Printing and CAD design development company called 3dp trains. Their website is 3dptrain.com. We are calling the new segment “Petticoat Junction News” and initially it will be a bi-weekly segment starting on October 19, 2022.

This “New Tracks” segment will introduce the women who are talented model builders who could become your mentor and who are helping to make our hobby great. Kristin needs a lot of

help developing this project. If you are interested in helping, email KristinKemick@newtracksmodeling.com.

Local NMRA CLUBS and DIVISIONS Sponsored by the NMRA



This is a bi-weekly segment which started April 27, 2022. It is based on one of the ways I got, along with many others, modeling help and met some really outstanding modelers. I have been honored to have served on two NMRA Regional BOD and as Superintendent for one NMRA Division, AP chairman for three Regions, and Contest Chairman for two Regions.

I believe, at its core, mentoring is best done one on one at a local level with a talented model builder, who can guide a person in learning the skills, techniques, and gain confidence in their modeling efforts. These are some of the greatest memories I have.

A mentor does not have to be an MMR or a famous model railroader, he or she just needs to be able to build models you admire or have some skills you want to learn. Ultimately, a mentor wants to help you improve your modeling. Those were the people and friends who helped me become a modeler.

Since most of my “New Tracks” efforts are focused on mentoring, I have been trying to figure out how to communicate with you, the “New Tracks” audience, the value of seeking local mentoring and how to find it.

This new segment is based on my personal experiences and is my answer to how you can find a local mentor and meet some great people in the process. Join us in this special segment of our show.



Phil Edholm

I have asked Phil Edholm, a NMRA Division Superintendent of the NMRA PCR/Coast Division, which includes the San Francisco Bay Area down to Monterey in California, to lead this segment. We kicked off the series with Gordon Robinson, the President of the NMRA, who discussed the value of NMRA Clubs and divisions to improving the modeling abilities of their members.

We will be inviting NMRA Divisions and clubs from all over the world to talk about what activities and mentoring opportunities they offer. If you hear something that sounds good, see if your local club or division offers it as well. If they do, take advantage of it!

Let's Go To Our Local Hobby Store Sponsored by the National Retail Hobby Stores Association (NRHSA).

Starting October 19th, [Mark Poggendorf](#), a NMRA member working on his MMR, hobby manufacturer, owner of Poggies Trains and a member of the National Retail Hobby Stores Association (NRHSA), will moderate a monthly

segment concerning common issues facing hobby shops and modelers. The focus of these segments will be on how both groups can work together for their mutual benefit and hopefully introduce the value of your local hobby store to help you become a more knowledgeable and more skilled model builder.

Mark's involvement in our hobby as both a modeler, manufacturer and Hobby Shop owner, who not only has a brick and mortar store in California, but also attends, as a vendor, many of the Train shows throughout the country, makes him well suited to lead these discussions and provide

his insights into the problems and issues facing both the hobby shop industry and modelers. While I live in Florida, I use Poggies as my local hobby shop. Great service, knowledgeable personnel and competitive prices. I can not go down the street to the store, but for me, Poggies is next best thing. Who knows, you also may find a new source for your modeling products from this series.

This segment will provide a forum where modelers and hobby shop owners can better understand each other's positions on hobby issues, and hopefully help each other solve the issues for both of their benefits. Please join us in these discussions and make your voice heard. I guarantee the industry will be listening. After all, they are the sponsor of this segment. When local hobby shops are successful and meet modelers' needs, everyone benefits.

If viewers have a hobby shop to recommend to be on our show, or you are a hobby shop owner who is interested in being on our show, please let Mark and me know at either

Mark.Poggendorf@newtracksmodeling.com or contact me jimkellow@newtracksmodeling.com

Remote Switching Layout Operation

On July 13th, Heath Hurwitz showed us his small, 48" x 10" switching puzzle he calls Human[c]ity Junction, a variation on John Allen's Timesaver. What makes this layout special is that it can be operated 100% remotely. One "New Tracks" viewer will have the opportunity to operate the layout, including coupling and uncoupling the boxcars from the comfort of their own home, controlling the locomotive and turnouts while seeing it all in real time over the Internet.



One day, maybe not too far off, we may all be able to operate layouts of all different scales remotely from all over the world. Having dedicated space for a layout will no longer be an obstacle to engage in operating a model railroad layout.

View our YouTube video on our channel New Tracks Modeling of the July 13th show to see how it works and the technology that makes it possible. Then starting on August 10th, and every two weeks thereafter, we will be offering our viewers the chance to operate on the layout

with different puzzles at various difficulty levels. Operators of all skill levels are welcome. If you want to be a remote operator, please email me to get on the schedule at: jimkellow@newtracksmodeling.com.

“New Tracks” Build Alongs

Our "Build Along" modeling experiences give you a personal mentor. Join in for a great learning experience.

CAD Design 3D Printing

Starting July 13, we had a Two Step Modeling BUILD ALONG with Earl Hackett. Look at our June 8, 2022 show on our YouTube channel New Tracks Modeling to find out details of what this CAD learning experience is all about.

Step 1. A CAD produced model BUILD ALONG using a free downloadable CAD program to produce the parts needed to build the model.

Step 2. Get the CAD parts printed. Print them on your own printer or have a friend print them or have a company do it. Or have David and Kristin Kempic owners of 3dptrain.com print your parts as they are offering a 10% discount to all “New Tracks” viewers who need their parts printed. Thank you David and Kristin. You can contact them at KristinKemick@newtracksmodeling.com.



Step 3: Building the model. Earl wanted to do something that was complex enough to show all the problems that can be encountered during a CAD design. He felt a 50' plate girder bridge would be a good topic. Earl searched his digital copies of

MR and found drawings and dimensions. It looks complicated, but there are only 7 or 8 parts that you have to design that are used over and over. Rather than building it in one big print, it will be printed as a kit with a bunch of parts to be assembled. The biggest challenge is putting in all the rivets.

Once you get the parts needed to build the bridge, everyone will be able to Build Along with him. To give everyone time to get all the parts made, we anticipate this will start on the September 21 Zoom show for four shows.

Card Model with Paul Egri, designer, David Rarig and Fr. Ron Walthers

On July 23, 2022, a Build Along of a Paul Egri created a card model of a service station was started by both David Rarig and Fr Ron Walthers using different techniques. The [FREE model can be downloaded](#) as of June 15, 2022, in any scale from our website, newtracksmodeling.com. Don't miss this, as you will find out how to do the scaling and downloading of the model, plus get some tips to help you get started.



Special Card Design Along with Earl Hackett and Paul Egri

These segments will teach how to use MS paint to do your own card designs like Paul does. Earl will create the outlines in a CAD package to insure accuracy and import them into a paint program for detailing. This program will start after Earl has completed his current 3D CAD Build Along program. The exact date will be announced in the near future.



Sea Port Model Works

Starting August 17, 2022, Bruce Nickerson, owner of Sea Port Model Works started building his Kit #H136W A Waterline kit of a 53' Coastal Steam Passenger Ferry in HO scale. Bruce is offering a 25% discount off the price of the kit to New Track modelers who want to build the kit along with him. To obtain this discount for 30 days from the August 17th start date, either call Bruce at 693-498-3849 and order kit H136W and use the code word "Steamer" and order from his website at

<https://seaportmodelworks.com/product/h136w-ho-53-passenger-ferry-laser-cut-wood-craftsman-kit/>

This specific vessel would have been designed, built, owned, and operated by local families, captains, railroads, or investors, etc. The steamer that we present through this kit is similar to the one we are familiar with called the Sabino or originally called the Tourist. The Sabino is up and running in Mystic Connecticut at the Maritime Museum. This model is not a model of the Sabino, but similar, and would have been used for very similar purposes. It will carry a lot of passengers, mail, and heavy cargo and attempt to keep schedules. It will make multiple stops at various islands and mainland harbors to deliver and pick up mail, produce, dry goods, livestock, groceries, etc. This kit, with some minor modifications, could be converted to a double decker which would increase passenger capacity.

This kit represents a small steam powered vessel working on the east or west coast, lakes, rivers, or oceans carrying on commerce that was vital to the growth of our nation, from the mid 1800's to mid 1900's.

There is so much to say about these steam powered vessels that have a distinct parallel with our nation's history. I strongly recommend that you read up on the subject. Here are a few great books that you might have a hard time putting down.

- STEAMBOAT DAYS by Fred Irving Dayton
- STEAMBOATS YESTERDAYS (ON CASCO BAY) by Capt. William J Frappier
- STEAMBOATS of GLOUCESTER and the NORTH SHORE by John Lester Sutherland
- STEAMBOATS COME TRUE by James Thomas Flexner

Bruce was recently on our Zoom show, and this "BUILD ALONG" is a result of modelers' interest in his models during his presentation. If you have not built a ship before, Bruce says don't worry, it is not much different than building a craftsman structure kit since the hull is a resin one piece part and there is very little rigging to do. I am looking forward to this as it is our first ship building project for the show. Thanks Bruce for your help.



Apogee Locomotive Works

Starting August 24, 2022, Arron Piotraschke, owner of Apogee Locomotive Works, will build one of his (ALW) locomotive kits in a Build Along. The kit is the ALW 28-Ton Boxcab: ALW 28-Ton Freelance Boxcab for Northwest Shortline Stanton Drive with 6'6" Wheelbase & 33" drivers. Accepts standard Kadee coupler boxes & couplers (preferably #58 couplers and #242 coupler boxes) Tichy Train Group phosphor bronze wire (.020) recommended, plus any additional details (up to modeler).

Recommended tools: needle files of various shapes, pliers, CA glue, Tamiya masking tape, paint of choice. There is a 10% discount starting on May 26, 2022 for "New Tracks" viewers on both the Boxcab and the B unit. Use this discount code when ordering:

NEWTRACKSALW28. For more information about the company, visit: Apogeelocoworks.com.



Berkshire Valley Models

Starting August 31, 2022, Richard Rands, owner of Berkshire Valley Models, will join with Martin Brechbiel, MMR to build his On30/On3 Ore car kit. There is a 25% discount on the purchase if the kit is for "New Tracks" members who order between July 27 and September 7, 2022 using the order code NEW TRACKS.

Kit #351 a 16' On30/On3 Ore Car. The kits are precision laser cut wood that assemble easily. The detail parts are made of white metal. Trucks and couplers are not included. The ore car we have designed does not follow a specific prototype, but is patterned after the many types found around the turn of the century.

A good place to see examples are the "Car Builders Dictionary". The shorter cars were made to carry the heavy precise metal copper, and iron ores.



Motrak Models

Starting September 7, 2022, Jeff Adan owner of Motrak Models and Greg Cassidy will team up to build the Alton Fire Station in S scale. It will be available in all 4 scales, N, HO, S, and O. There will be a 15% discount off of the price of the kits for "New Tracks" modelers between August 15, 2022 and September 30, 2022 when the code word ALTON is used. Additional information is available on the Motrak Website.

Sylvan Models

Starting September 7, 2022, Claire Gilbert, the owner of Sylvan Models, will have his CNE 1937 Boxcar kit built by Bill Davis.

The model is available in HO and O Scale and Bill will be building the kit in both scales on the show. There will be a 10% discount off the price of the kits for "New Tracks" modelers when the code word New Tracks is used during ordering. More information is on the Sylvan website.



Poggies Trains

Starting September 21, 2022, Mark Poggendorf, owner of Poggies Trains, will use several of his molds to build a rock wall and bridge. More information on this Build Along, and the dates the 25% discount on the molds for “New Tracks” viewers is available, will be posted soon on our website.

Narrow Gauge Modeling Company (NGMC)

Starting October 5, 2022, Kevin Macomber owner of the Narrow Gauge Modeling Company (NGMC) will begin a Build Along with Martin Brechbiel MMR of one of his Wagon kits. More information will be available soon. Wagons start with V201, but there are many more not shown.

<https://www.narrowgaugemodeling.com/wagons-carts-cars/>

Hangman Creek Dioramas



Starting October 26, 2022, Jess Dozier owner of Hangman Creek Dioramas will build one of his kits. The Diorama Kit is called “Log Boat at Lane Deer Mill”

It's an easy kit to build and has a lot of different materials, resin, wood, Hydrocal®, and scenery to complete the 12” x 12” diorama. Additional information will be available soon on our website.

New Creations Victorian Railroad Buildings

Starting November 16, 2022, Alan Rogers, owner of New Creations Victorian Railroad Buildings will begin a Build Along of his kit, the St. Mary Star of the Sea Catholic Church of Ocean City, Maryland. He is offering a 25% discount off the price of the kit in both O and HO from November 1st through December 31, 2022. To get the discount, use the code "NewTracks."

<https://newcreationsvictorian.com/shop/ols/products/st-mary-star-of-the-sea-catholic-church-o-scale>. More details can be found on his website and by contacting him directly at Eauchiche@gmail.com.



Dwarvin Lighting Without Wires

Starting November 23, 2022, Michael Groves will begin a Built Along project using his Lighting Without Wiring system. He will explore what this paradigm shift in lighting will do for you. Michael is offering any of the Lamplighter Starter kits, a month before the first Build Along event in which you will need to use it.

The kits will “go on sale” using the code “NewTracks” for 20% off the regular price from October 23rd through November 23rd, 2022. Two examples of what can be achieved using the product are shown from Michael Morley - winner of Fiber Guild for Model Railroaders competition.





Berkshire Valley Models

Starting December 7, 2022, Richard Rands, owner of Berkshire Valley Models, will join with Bill Davis to build his kit of the Walsh/Duncan Bay Window House in Animas Forks, Colorado. The kit is available in both HO and O Scale. A 20% discount on the kit is available from November 2 through December 15, 2022 by using the Code "New Tracks I" when ordering.

On the left is a photo of the prototype the structure in Colorado, with a photo of the model on the left below.

Happy New Year 2023 - BUILD ALONG Models

Mudd Creek Models

Starting January 4, 2023, Frank and Natalie Saladino, owners of Mudd Creek Models, will build one of their kits. More details will be available soon on our website.

Atlantis Models



Starting February 22, 2023, Atlantis Models owner Peter Petri will build an O scale gasoline Truck. There is a 15% discount for "New Tracks" modelers building this model for orders for one year beginning September 1, 2022 to Aug 31, 2023. This is the first truck model we have built on the show, and I believe it will appeal to many viewers. Please visit the Atlantis website and see this and other O scale models they offer.

<https://atlantis-models.com/white-fruehauf-gas-truck-sinclair-us-army-plastic-model-kit-1-48/>.

More Build Alongs to come

I hope you want to participate in all the "Build Alongs". The modelers and manufacturers, who are making these events possible, are doing them to try to help you improve your skills and have more enjoyment and confidence in your modeling. They are a true learning experience that have helped many modelers. Join us.

This program is providing modelers, their own personal mentor on our shows. So if you have been sitting on the sidelines for awhile, give model building a try. I believe you will have some fun. It is really great for me to hear the enthusiasm and excitement from first time or previous armchair builders about their experiences by participating.

Please show your support for these events by your active participation. Thank you.

I am looking for more modelers and manufacturers to be involved in future "Build Alongs" in 2023. I have several scheduled so far, and if you are interested, please let me know. Remember a model builder can select the dates, manufacturer, and specific kit, you want to build. A manufacturer can build his own kit, find a modeler, provide a model builder, or I will find someone to build their kit. Contact me at:

jimkellow@newtracksmodeling.com.

We have several Other Regular Modeling Segments on our Zoom shows.

"Watch Me Build"

These segments are meant for modelers who want to share their modeling efforts and discuss their modeling techniques so others can benefit. These segments can be for one or more shows depending on the details included for the model building presentation.

On July 6, 2022, Pat Rivard and Paul Hurly did a segment "Making Coniferous Trees". They demonstrated how to use a variety of tree branch materials to recreate species that are seen throughout North America.

You may never have shown your modeling before in public for a variety of reasons. I assure you I think you will enjoy this experience knowing many others will benefit from your techniques. My email is jimkellow@newtracksmodeling.com or, if you would like to discuss your idea by telephone, I will provide my cell number to you.

Ask Your Modeling Question

These are segments where viewers can ask modeling questions and get answers from other modelers on the show. It is a forum where viewers can help each other solve specific modeling problems or offer advice on modeling techniques. We have a form on our website you can use to ask your questions. This allows us to schedule the appropriate time for this segment on each show. Don't hesitate to ask questions, after all that is how we learn new things.

"Remembering Old Kits"

Modelers build kits from our distant past that are either no longer manufactured or hardly available. Kits whose names we may have forgotten, but when we hear their name again, brings back great memories from our youth. These builds will definitely remind all of us what past modeling used to be like.

The first two segments had Martin Brechbiel, MMR building a Van's Car Shop and a Train Craft kit. Martin has plans to build more old kits shortly. If you have an old kit and want to build it let me know at: jimkellow@newtracksmodeling.com

A question I often hear is: How do we attract young modelers to become model railroaders?

After someone tells me our model railroading hobby is dying, the discussion normally turns to this question.

I recently talked with Stephen Elliott, who is a [National Retail Hobby Store Association](#) (NRHSA) member and serves on their Board of Directors. He also is the owner of [Fundemonium Hobby Store](#) in California. In our discussion on my Zoom show, he told me his store's main market is the young beginning modeler with varied interests, but not necessarily model railroading.

If his customer decides to become a serious model railroader at some point, Stephen recommends a specific model railroading hobby shop nearby that can better serve their needs. This was very interesting to me as these young modelers are what I constantly hear is the age group the model railroading community seeks to attract.

Where are these young modelers and how can we attract them to model railroading?

They are at the local Hobby Store or with a group of like minded friends, which may not be where experienced model railroaders normally shop or attend. As Stephen told me: "To grow interest in a hobby, you have to meet potential hobbyists where they are. Find the kids who are interested in miniature games and snap-fit fantasy models and introduce them to the broad range of hobbies available including model railroading.

Build interest by being visible at places beyond the big traditional train shows. Participate in hobby store shows for model contests and in-store train shows.

Hold seminars for model railroading skills at non-model railroading events. Examples would be teaching weathering and detailing techniques and a scale model building club; demonstrating scenery techniques for school dioramas and table-top gaming terrain.”

Stephen continued: “Modelers love modeling and will pick up different hobbies where they can exercise their modeling skills. If model railroaders could break out of the roundhouse, they would find a whole world of modelers out there who would appreciate what they have to offer.”

Who are these young beginning modelers?

Stephen said many model in the current craze they see on the Internet such as fantasy models, Star Wars, gamers etc. He indicated, in his experience, after building models for awhile, they start looking around and then pick a hobby to become serious about.

That is when they may decide to become serious about model railroading or RC airplanes or whatever other hobby they find of interest.

A possible idea!

If the model railroad local clubs, NMRA Divisions, can help mentor these young modelers in modeling skills early on, in whatever hobby they start with, we could have a real chance to introduce model railroading as their future hobby of choice!

How to do this?

One way might be to discuss with the owner of the local hobby store about offering basic model building clinics, or attend existing clinics, on the kind of models young modelers are currently interested in building and offer to help teach them the modeling skills and techniques which will serve them well in all their future modeling efforts.

Mentoring these young modelers could be one of the best ways to encourage them to consider model railroading as their future serious hobby of choice.

Just an idea, but it seems worth trying to me. What do you think? I Look forward to hearing from you.
jimkellow@newtracksmodeling.com

Now, let’s meet some modelers who I know can can teach us all some modeling techniques.



Sarah Griessenböck

Hi, I’m an Austrian modeller who fell in love with big American steam locomotives. My first contact to the hobby started at a book store where I found an issue of *Model Railroader* featuring Ed Rappe’s spectacular Horse Shoe Curve layout. I was totally blown away by the sheer mass of O scale and decided that it’s what I want. I quickly chose the Union Pacific as my modeling subject due to availability of models and information, paired with a great variety of rolling stock.

Next stop was the O gauge Railroading On-Line Forum, where I had the chance of getting feedback and learn more about the hobby. I immediately felt the bond of comradery within the scale, and there might be some tensions from time to time between the gauges, O scalers are great people throughout. In March 2020, I had the great opportunity to meet many of my by then friends in person at the Chicago March Meet. I had the best time in my life! And the greatest thing was the week I had the honour to spend with master modeller, Matthew Forsyth, who became a very close friend and taught me how to work with a lathe and mill.

Since then, I switched over to proto:48 and have successfully converted my first locomotive, the Union Pacific 9000, a mighty 4-12-2. She is capable of 72' radius curves, has a ton of smoke and sound going on. Since then I tried to develop some skill in weathering and scenery. I am currently building a river scene module, that I can drop into the layout once the room is prepared. One of my deeper topics is to convert equipment to proto:48, that's a skill I really need when I want to successfully build this big layout of my dreams.

I'd say I have a lot to learn and learning new techniques is hard work, but also so rewarding. Currently I'm designing 3D models of detail parts that can be printed in resin with today's home printers.

I love the idea of sharing and helping each other out and that's what is going to keep our scale alive. So I'm uploading my designs on my website for free download at www.trainkitchen.com.

I plan to attend the 2023 O-scale National Convention in Denver and meet many good friends. Railroad modelling is my life, and I love that you can keep what you create and enjoy it every day. In my former work as a hotel manager, we created great holiday experiences for our clients, but at the end of the day, I had the feeling that my work somehow faded away. When I weather a boxcar, it's like parenting a puppy. These trains are my babes and I have a history with every piece of equipment. That's so joyful and gives me a lot of meaning. What better could you ask for from a hobby!



My first project back in 2018 when I started modeling.



Ted Schnepf's SM-16 resin kit, fully detailed.



Chipped paint effect on that same SM-16 resin car.



Machining tires for the UP 9000.



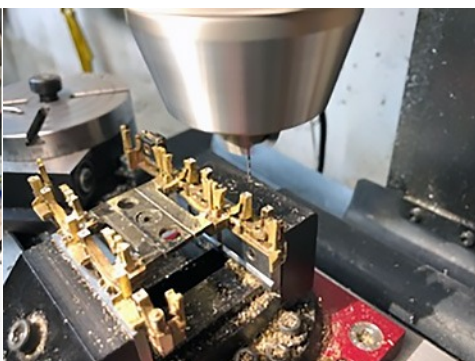
My first conversion to proto:48.



All wheels converted!



Engine almost finished.



A PSC Commonwealth tender truck conversion.



All journals spring loaded.



Getting more precise with wheel cutting.



More detail on the Boxpok driver of a UP 4-6-6-4.



Weathering on an InterMountain AAR boxcar.



An InterMountain 55 ton hopper, faded paint and heavy use.



Building my first module, a river scene with trestle.



Sculptamold.



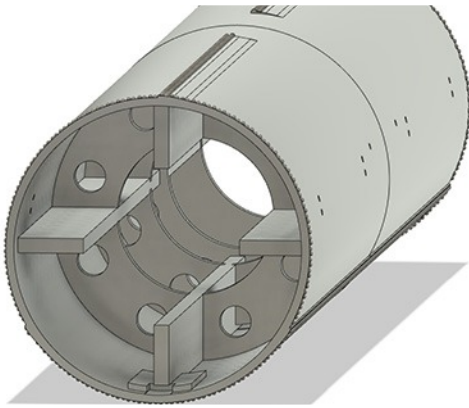
Developing white water effects.



Progress on the module.



Tree modeling, wire core, latex and love.



Starting 3D designing and printing.



Printed tank car dome.



Union Pacific O-50-6 tank car, home made.



Door hardware.

Youngstown door with Camel hardware.



InterMountain boxcar with improved door detail.

Thank you Sarah for your help and interest. She can be reached at sarah.griessenboeck@newtracksmodeling.com

Now meet a modeler who has quite a trip to On30.



Glen Gollrad

Who I am as a modeler: A life long "maker" of things, essentially - and an incessant tinkerer. Like a lot of model railroaders, it all started with a train

set - an HO scale Santa Fe set, a sheet of plywood, and saw horses. I built models of all sorts starting at age 8-9 and that evolved into pretty advanced work by my teens. As a teenager I had two primary interests: riding my BMX bike doing tricks - on a number of ramps my friends and I designed and constructed - and model railroading with a bit of RC car building and racing. Whether a rear coaster brake hub, an RC car suspension linkage, or a model railroad turntable indexing system - I loved tearing into things to see how they worked, fixing them, and sometimes improving upon the original with some personal modifications.

In 8th grade I learned quickly that my shop teacher was also a model railroader - and would facilitate a number of orders through Walthers back in the day, when it was too hard to get into Boston to Eric Fuchs Hobby Shop in downtown Boston, an amazing hobby shop that was primarily model railroading focused, and some RC as well. They carried a good range of items, including very high end stuff (the glass cases filled with all that brass were the things of dreams!). I bought my RC stuff there, as well as my early years scratch building and kitbashing supplies. They had stuff to hand lay track, tons of structural details, bass wood, castings, styrene, the tools, etc... I'm not sure when they closed their doors, but it was a long time ago.

By then I had expanded the layout - it was now the original 4X8 plywood, with a scenic backdrop dividing it lengthwise, as well as a long peninsula that was primarily a locomotive servicing area. That fancy Athearn Blue Box GP-9 with Southern Pacific grey & red graphics I got through my shop teacher is still one of my favorites! By the time I started driving, I was pretty consumed with pursuing BMX Freestyle competitions and drove all over the eastern USA doing shows and competitions. Throughout high school I stayed in the hobby - the winters were impossible to ride in half the time so I'd be in the basement working away. Sometime in high school I saw an article in *MR* about a Sandy River & Rangely Lakes layout that captivated me. Something about the diminutive, backwoods nature and the epic achievements in the early days of grading an ROW through amazing wilderness. So I started a small HOn2 module using an N-Scale loco chassis and scratchbuilding the cabs and such. Pretty crude stuff, but I was hooked on Narrow Gauge at that point.

Then... college. I continued to design, build, make, create endeavor things - but now while pursuing a degree in Architecture. Dorms and then small apartments were not conducive to a layout, per se - but I sure built a lot of models for school projects! When in school in the late 1980's we still drew everything by hand, but learned CAD on the earliest iterations of AUTOCad. Pretty crude stuff - but once I learned how to get line weights to look right - I realized it was going to be 'the future'. Now it's truly astonishing.

To the question of mentor or trial and error, I'd say that I was on my own - but my folks were always supportive. As a little kid, my dad did have his original 1950's era Lionel 'trainset' set up in the attic in a pretty elaborate layout. No real scenery, but a significant amount of track, including an over under figure 8 and other nifty features. He was a chemical engineer - so the technical background & influence was there for me from the start. I was maybe 13 or 14 when we went on a weekend camping trip in search of SR&RL remnants - related to the period of early narrow gauge interest. We found the little ROW, a mile of track, and a generally pretty tiny and sad site near Strong, Maine. What became the highlight was the pitstop at the huge Bangor & Aroostook yard we went to see (former B&M) with an engine house, turntable, dozens of locomotives and tons of evidence of a former glory. We even found a blue and orange B&A paint scheme BL-2 which was the oddest, and coolest diesel I ever saw in real life. That trip galvanized my love of American industrial design, transportation engineering, and history.

How I learned to build models stems from those early years before college, then the architectural models - some which were pretty elaborate, others more 'massing study' type things of chipboard or other basic materials.

There I learned to realize my vision for anything - how to draw, plan, design - pretty much anything. This was the key. After college, every place I lived got an extensive work over too, as the houses or apartments weren't quite... right. I had worked in the trades throughout college so learned some real world, real life construction skills to build on my early years of ramp building to jump bikes. Which led to buying fixer upper houses pretty much my entire life. The next (hopefully last) house will be one I design from the ground up - but will let someone else build most of it.

There was a long gap in model railroading for me - from age 18 to about 40. I still had a few pieces of HO scale rolling stock, and in 2008/2009 finally had a little room to build something. It was then that I stumbled onto On30 - and boy was I hooked on the spot! I bought the Bachmann Shay locomotive, and a few other pieces - as well as some Microengineering flex track and turnouts. Everything looked so amazing! I built a small lumber/logging operation, but never got too far with that - then we bought the next house and THAT stuff got put away in a bin in 2011. But the fire had been rekindled. I had scratch built a wooden truss bridge and a simple steam powered stiff leg derrick model to unload log cars, and there wasn't a week that went by I wasn't plotting the next step... I really liked O Scale as I had worked and lived in 1/4"=1'-0" scale for decades now as an architect.

Three things happened in quick succession in 2019; we bought a house with an enormous, dry, clean basement with high ceilings, my wife got me a birthday gift card for the local hobby shop, and I had started dabbling with 3D printing. I started planning an On30 layout at that time (wanting to do On3, or Sn3 - but too intimidated by the costs for either) based on the Chili Line in New Mexico, ca 1920's. Having spent 10 years in Santa Fe (2000-2009) and knowing we'll retire back to the region in coming years, I fell in love with the place, the history, the views, and the remoteness in some of the regions in CO and NM. That the D&RGW guys even conceived of what became an astonishing empire, carving ROW's in impossible area and just 'making it happen' was more than enough inspiration. With the affordable On30 locomotives & rolling stock and the familiar, older person eyesight friendly 1:48 scale - I had found my "place" to really dig in and re-launch model railroading 'for real'. I can see in coming years two things: for a large layout in S Scale will be the path as it hits that sweet spot in so many ways. For more elaborate, module/diorama endeavors, On3 will be the way. What I can do, and



know - is the trackwork is fun and easy at last. I hand lay all my new stuff and have built about 20 turnouts so far.

What I will say about the scale 'issue' is they all have so many merits - really just space to build, budget, and desire I guess! So many variables and factors. Then add in 3D printing, and the options to explore multiple scales becomes a reality without breaking the budget. For me, the first few models I started (design, print, assemble, etc..) from scratch - were the Espanola Depot, and the Taos Junction Depot, and as an added challenge I wanted to see what the 3D printer could do so I designed and printed a typical D&RGW water tank as well. These all came out better than expected; mixing the 3D printing for the walls, windows, doors, roofs, etc... plus some real wood and other more traditional materials. I then tried some simple archbar trucks, a new tender shell for an old C-19, a horse drawn water wagon, sawmill 'Frick' style mill hardware, etc. Truly a limitless potential so that if you can draw it in 3D and adjust for printer technologies, the sky (and time) is the limit. I'm fortunate to have the drawing part from years as a professional - and have worked out a lot of the beginner 3D printer issues (both with the designs, and with the printer settings). I think I'm an obsessive DIY type and end up tearing stuff apart when it's done, or almost done - as by the time I finish it I see all the ways to do it better.

Anyhow - I could go on and on! I'll stop and give you some time to 'process' and let me know what to add - or perhaps, remove :)

Photo album of a few projects: <https://photos.app.goo.gl/ZsopRcbFyj27D3wr5>

Glen thanks for sharing a little about yourself. If you think Glen can help you, please email him at Glen.Gollrad@newtracksmodeling.com

Next, meet one of the founding members (along with Steven Tuff) of Yelton Models, a professional chef de cuisine, a 3D designer, an artist and sculptor and a very talented model railroader...

Dan Notley



I started in the hobby when I was a young person. I built my first layout in my early teens, and read as many books as I could get hold of. I experimented with many building and scenery techniques along the way, built numerous kits and did a bit of scratch building as well.

I have spent a fair amount of time learning how to control and operate my home layout through computer software (TrainController).

I was fortunate enough to have a mentor in the hobby. His name was Jim Ellis, a modeler of great skill and talent. He was blessed with gift of gab as well, and knew many people in hobby and always included me in his visits to many

wonderful layouts. Jim and I worked on his home layout always trying to improve our own skills as we worked on everything from rebuilding brass engines, to every type of scenery construction imaginable, as well as, developing some of our own techniques as well.

I had the unique opportunity to work on the Little Canada Project for 6 years leading the scenery and land forms team in producing a model of Canada. If you go to the attraction and see the terrain, I probably had a hand in it; and when you see the Niagara Falls display, I not only carved the falls rock work from some giant pieces of foam, but included in the display is a scratch built version of my own home.

Here's an early construction photo.



I started 3D modeling in 2017 first for myself and home layout. I realized that some of the things I had made out of necessity other people might also enjoy.

I approached Steven Tuff in the fall of 2019 with the idea of producing small run items 3D printed to fill a void in our hobby. Given we both have many friends in the hobby, when asked for ideas of what people wanted, we took to producing these missing items. We have also produced 2 structure kits with more in the works, as well as, unique railroad based items for many eras.

Lately we have found many needs in O scale that we have been trying to fill from rail speeders to many small detail parts that either are no longer available or have never been made.

We have also found a market in custom railroad signs to be used on fascias of layouts to show town or station locations. Below is one from my home layout



I model HO scale Pennsylvania Railroad from the 40s to the late 50s. HO was the scale I grew up with and enjoy the ability to model a large area in a relatively small space, although the amount of detail that can be attained in O scale or even S scale is not lost on me.

I continue to help friends build their layouts whenever I can, and recently a good friend decided to dismantle his HO empire to start over with a new O scale project. I am having a great time helping him any chance I get.

This also helps feed the idea mill for our printing company as I find out just how many things are not available in O scale that are common place in HO.



Everything in the photo on the far left is a product we manufacture.

In the future... well, here's a glimpse of what's coming it our TH&B 40 series 0-6-0 tender.



Here's our TH&B caboose, above, and we are starting with #58 and working through the numbers after this one is finished.

On the left is our AC tower kit we brought out at Central OHIO RPM meet.

Dan, thanks so much for sharing your modeling with us. You can reach Dan at Dan.Notley@newtracksmodeling.com

This next modeler is an inspiration to us all and an exceptional modeler. I think you will agree.

Fr. Ron Walters, O.F.M.: Provincial Minister, Province of Our Lady of Guadalupe, Order of Friars Minor (O.F.M.)

We are fortunate in having Fr. Ron lead us in prayer at the start of our Zoom Shows. ([Click here to view an example.](#)) Please meet Fr. Ron.

Where do I start... My brother and I had trains as a child. In fact before I turned one, at my first Christmas, my dad gave me trains. Of course I remember nothing of that event. As told to me by my mother, my dad was scolded by his





The photo of Julie's apartment is a kit bash of two kits from Woodland Scenics, and was photographed by me. It is based on the real location of the filming of the movie "Julie & Julia". The interior of the second floor is all scratch built from scrap styrene, paper, and the mirror, which isn't very visible, is a piece cut from a Lays potato chip bag and pasted to the wall.

mother that I was far too young to know what it was or what to do with it. I had O scale (O gauge as they called it then), and when my younger brother was old enough, he received HO scale. Of course, I played with both.

I got back into model railroading after teaching high school for four years. I was burning the candle at both ends, and was exhausted at the end of every day. Sometimes, for a priest and a member of a religious community, the tasks that we performed in the process of teaching were far more time consuming than what normal teachers had. We religious priests and brothers had no families, so we did not have those responsibilities, and therefore, were freer to do what the other teachers could not. In the midst of that, I remembered spending some time with my dad when I was younger putting together a plastic (static) model of a steam engine during one of the times my mother was in the hospital. I remembered how fun that was, so I subscribed to *Model Railroader Magazine* and found a kit to put together. A year later, I bought my first wooden craftsman kit of a "general store". It was a box of wooden sticks. It took me several months to put together at the small desk in my bedroom, in between teaching and working as an assistant pastor of a parish in mission territory. I no longer have the finished kit, but it was great and rewarding. I was hooked on craftsman kits.

I joined a modular group in Albuquerque at that time and attended meetings. I belong to the group today, though most of the members were not members when I first joined. Eventually, I moved to Santa Fe and began building a module. I move often; at that time, it seemed like I was being moved every year. I ended up in Denver for ten years, and was part of a modular group there, which no longer exists, and was also part of a club which was

building a layout in the basement of a former traction powerhouse. Right outside was a full scale 4-8-8-4 locomotive. I built a couple kits for that club before the building was sold to REI, the engine moved, and both clubs there had to move out.

After ten years in Denver, I was moved to Northwest Indiana (the other end of the State where I taught high school). I was familiar with the area, and found a club in Valparaiso, IN, the Porter County Model Railroad Club. They had built, and mostly completed, a layout in an apartment above a hobby shop in downtown Valpo. I kit-bashed two or three buildings for them, but the first scratch built structure was a New York Central passenger station which used to exist East of Valpo. I started the build while my dad was dying of emphysema and white lung from asbestos. I did not finish the build until a couple years ago. The build was a challenge because the operators "bump out" was not in the traditional location in the middle of the station, but on the corner. Further complicating the build was the fact that the club wanted me to switch that bump out to the other end of the station. The station was located across from a crossing tower and basically monitored traffic through the crossing. Neither the tower, nor the station, exist today. All I had were photos in a book put out concerning the New York Central. My article on the build was published in the *NYC Modeler* magazine, but I cannot find my copy of the magazine. You know how it is — you save a copy of the magazine on your computer and then can't find where you saved it.

I had to suspend modeling for a few years. I am currently the CEO of the Franciscans in New Mexico and Northeastern Arizona. I have been an administrator in the Franciscans now for nearly 30 years. That consumes a lot of energy and time. Modeling is my way of getting away from the challenges and tensions of administration.



Judson's Candies was photographed by a member of the SAMRA club. I do not know who. This is a building that housed the Judson's Candies Company in West downtown San Antonio for many years. It is now condominiums.



St. Joseph was photographed by Ralph Alexander of the SAMRA club. This is a photo of a Roman Catholic Church above the River Walk in San Antonio, Texas which was entirely surrounded by Joske's Department Store. (HO scale model)

your methods and techniques. I am not a master builder, I am far from it. I enjoy learning new things and finding how to apply what I have learned, regardless of the source of the technique.

I model primarily in HO scale, but I have built in N scale occasionally, particularly when working with other modelers who model in different scales.

I don't have a layout. Unlike other modelers who may have some space here or there in their homes for a layout, the room I live in will probably be lived in by someone else in a year or two, so trying to build a layout and then moving it as frequently as I get moved, is beyond my patience. I am a professional. I have three master's degrees, and therefore have a professional library. So moving is a chore.

Therefore, I stick to modeling buildings. I usually end up building them for specific layouts. But sometimes I just build and then give the building away to someone. My biggest complaint about modeling is the cost. For

When I lived in San Antonio, TX, I joined the San Antonio Model Railroad Association in Live Oak, TX. I became, for three years, the secretary of the club. The club wanted to add some buildings to the layout and I was asked to build two of them. Both had to be built from scratch. One was Judson's Candy, which is now a condominium, and the other was a Church about two blocks from the Alamo. The candy company only needed to have two sides visible to the viewer. What better person to build the model of the Church than a priest? The church was entirely surrounded by a department store: Joske's. The building still exists, but the department store is long gone. So I only needed to model the facade and the roof of the Church. I still belong to the club although I now live in Albuquerque. The club will soon be redesigning the San Antonio downtown area on the layout because of damage to the wall of the building behind that area. So I may be doing some more scratch building for the club.

Most of my learning has been by trial and error. I learned a long time ago that the best way to learn something is by making mistakes. I learned this particularly from computer programming and macro building. Finding and fixing a problem, even after repeated mistakes, teaches you to analyze



The MOW shed at North Fort Worth Tower 60 was photographed by me. It is a scratch build in wood based on photos of Tower 60 in North Fort Worth. The buildings there no longer exist. Note the unusual “man” door to the right. (HO scale model)



The two photos of Carolina Craftsman Kits Shotgun House Kitbash were taken by me. I bashed the kit into a house converted into a storage shed which is not well maintained. (HO scale model)

someone like me, cost is significant, since all of us Franciscans live together and share our resources and expenses in common. So having money available for a hobby is allowed, but funds are limited. That is not to say that I don’t understand the cost of owning and operating a model manufacturing business. I do understand and am not trying to be critical. It is just a challenge for me. A kit, any kit, that costs over \$100.00, is usually out of my price range. I saved up and bought Bar Mills’ 2021 Christmas kit, but it is three buildings, instead of one, so the cost was closer to reasonable.

So the build I am doing right now for the Saturday Ice House scratch build is in my wheel house, as they say. The pieces are low cost or no cost. I buy some Evergreen Styrene every once in while or perhaps some Northeastern Scale lumber so I am building up some scratch building supplies. Details are often lacking on my models because I just don’t have the resources to buy barrels or pallets or figures, etc. I once found some very cheap figures on Amazon which were a scale 1:100, close enough for HO

scale, for a very reasonable price, but they are horribly painted. So I repainted them. I buy “for sale” signs for plastic sheets, I look for “trash” that may be able to be adapted to things I need for building (I have a large collection of clear plastic from packaging that I use for windows). In the long run, it is part of the fun of modeling.

Thanks Fr. Ron for sharing your modeling and for participating in our Zoom shows. You can reach Fr. Ron at: Ron.Walters@newtracksmodeling.com.

Well, it’s time for me to return to my workbench and start working on something that I fell in love with and just have to model. Happens all the time. Next model structure for me will be construction of new Playboy Cars Sales and Repair business for “New Tracks”. The original company was established 1947 producing a 2 seat convertible sports car. It will be scratchbuilt in card designed by Paul Egri: See the prototype Playboy 2 seat sports car at <https://www.apexautomag.com/2016/06/history-playboy-car/>

Please subscribe to my YouTube Channel, New Tracks Modeling, and ring the bell. Our website, NewTracksModeling.com, provides log in links to our Zoom events and has information about what “New Tracks” you can travel. I love getting your comments, suggestions, and modeling ideas. I so enjoy hearing from you and having a conversation. My email is: jimkellow@newtracksmodeling.com



Till next time on “New Tracks”. Happy Modeling

Easy Landscaping on Your Layout

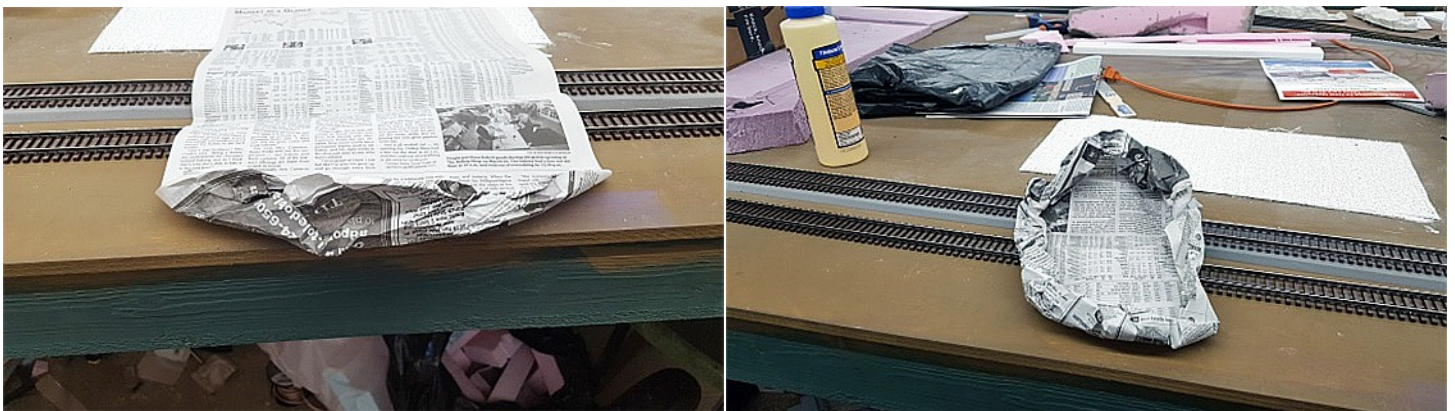
By **John Frankforther**

When you get your track work up and running on your layout, and the trains are running around your flat piece of plywood or foam board, you remember seeing the highly detailed scenes in train magazines. You wonder how they got such great scenery. Having viewed many, many articles on different ways to create scenery from carved foam board, spray foam, paper and glue, and many more, you wonder where to start.

I had also viewed many videos on creating the most realistic scenery for my layout. Somehow, it just didn't click on how to get what I wanted. I had chunks of foam carved to "look" like rocks but they were just stuck on other pieces of larger foam. Not very realistic!

Then, one of the members of our local train group, Randy, explained that I needed to look closer at pictures of mountainous landscapes, rock outcrops, etc. to see what the real world looks like. After a few sessions working with Randy, I could finally grasp how I should be sculpting my landscape. The next step was picking a medium to work in. Since most of my mountainous scenery is done in 2" foam board and I had lots to work with, I tried that. I'm more electro-mechanical/computer savvy with very little artistic abilities. Randy suggested I try plaster casting rock molds for a little more realism and lent me a few of his to try out. After creating 8-10 rock castings, I was amazed how realistic they looked. With a little more instruction and placement on my mountain tunnel from Randy, things were looking up.

The next step was to fill in the barren foam board landscape with some hills and valleys and extend the sides of the tunnel to the base of the layout. Here is where one of the YouTube videos, from a ten year old boy, really caught my eye. His dad was using the wadded newspaper and plaster cloth method of creating landscapes. The problem they were having was, the round wads of newspaper didn't look like a hill side when covered with the plaster cloth. It just looked a bunch of round bumps covered in the plaster cloth.



So what the boy suggested was to crumple the paper around the outside of the sheet in a circular motion, working towards the center in a pancake shape. This created a flat surface with rounded edges.

This created a shape, when turned over, to look more like a large boulder. You can use different sizes of newspaper to make different sizes of "rocks" and stack them to give you a more staggered look. As you can see from the picture, not all the rolled paper is the same size. Some are tucked tighter together than others to give outcrops and valleys. You can roll the paper in longer, flatter sections to cover more area.



Now that you have your hillside created, it is time to apply your plaster cloth and create the terrain. Cut the dry cloth into sections that will cover all the sections of the crumpled newspaper. As you apply the cloth, lightly work it down into the voids between the crumpled papers. Make sure you secure and smooth the edges to the surrounding material. Overlap the next piece of cloth by $\frac{1}{2}$ ", secure and smooth the edges together to get a smooth transition between sheets. Let the plaster cloth dry for a while to allow it to harden before proceeding. Then you can cover the cloth with your choice of topcoats, paints and other scenery material.

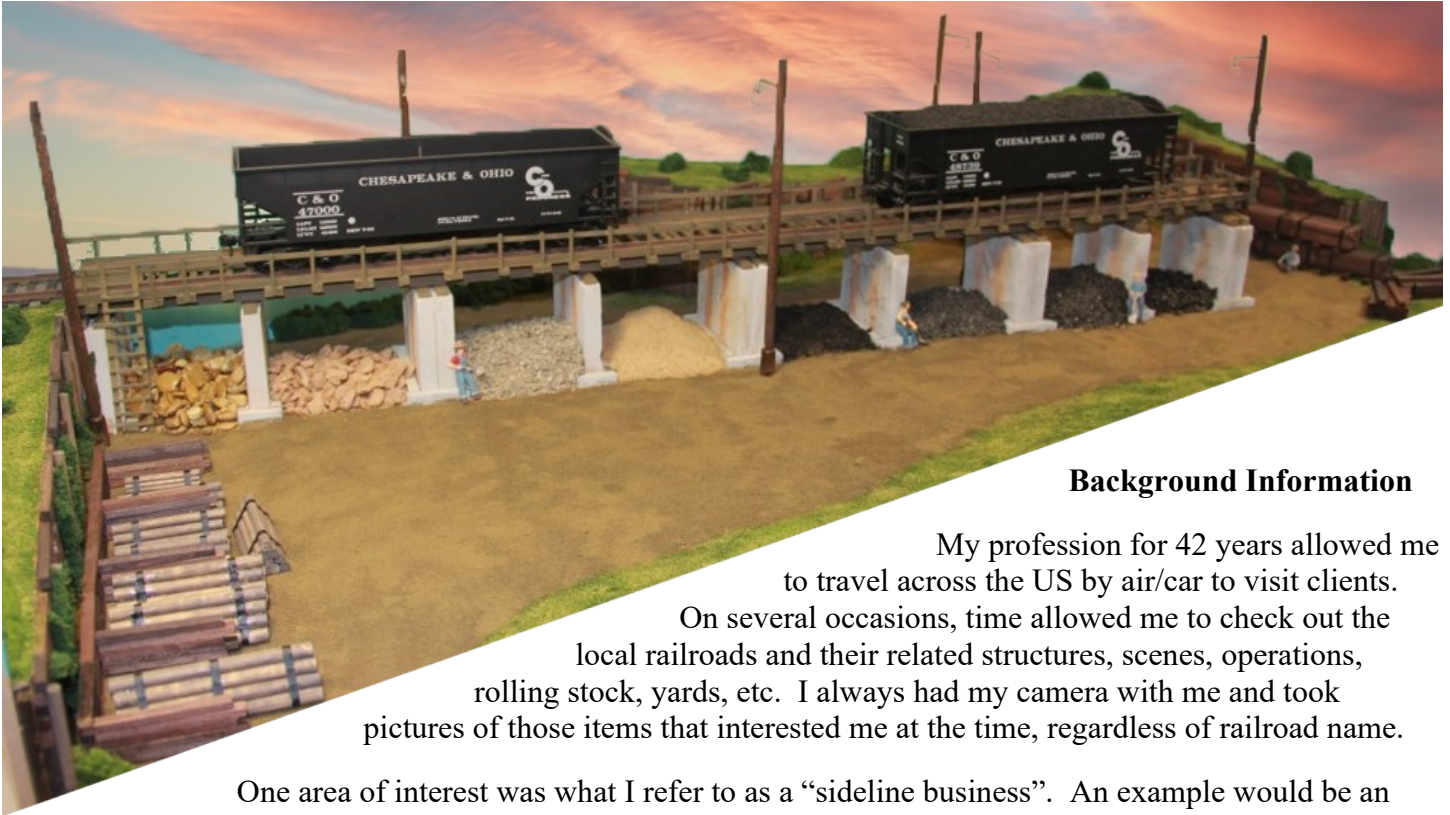


Here is a large section of my tunnel (below) that I used this method on and I think it turned out great so far. Once I put the topcoat on to cover the plaster cloth, I can continue on to finish the scenery. The top part of the tunnel was not covered in plaster cloth as it is a separate piece of foam. This will allow me to open the tunnel for maintenance and train related issues. Once finished, the cover of the tunnel and front of it will be blended together to appear as one piece with the cover still having access to the inside.



UNLOADING TRESTLE DIORAMA

By **Phil Camp**



Background Information

My profession for 42 years allowed me to travel across the US by air/car to visit clients.

On several occasions, time allowed me to check out the local railroads and their related structures, scenes, operations, rolling stock, yards, etc. I always had my camera with me and took pictures of those items that interested me at the time, regardless of railroad name.

One area of interest was what I refer to as a “sideline business”. An example would be an elevated unloading trestle for bulk commodities – coal, gravel, sand, stone, rip-rap – any bulk commodity that can be unloaded by gravity through open hoppers bays. A good example could be a local business that sold supplies to farmers/ranchers who needed large quantities of bulk items.

Having seen and photographed several examples of this type of business, I decided to scratch build a diorama of this type of structure. It is not an existing structure and not built to any specific standards or plans, but rather what I wanted to present as an example. Most real world unloading trestles are the length of just one hopper, possibly two hoppers, but mine was designed and built to handle three hoppers (not necessarily all at one time).

Physical Information

The entire diorama is built to O Scale size (1/4 inch = 1 ft.) (1:48). The diorama is 43 inches wide, 17.5 inches deep and 12 inches tall (at corners).

Concrete abutments are scale 10 ft. tall, scale 12 ft. wide and scale 18 inches thick. They are used for structural supports for the overhead track/wood structure and as dividers for the various bulk commodities.

List of Materials

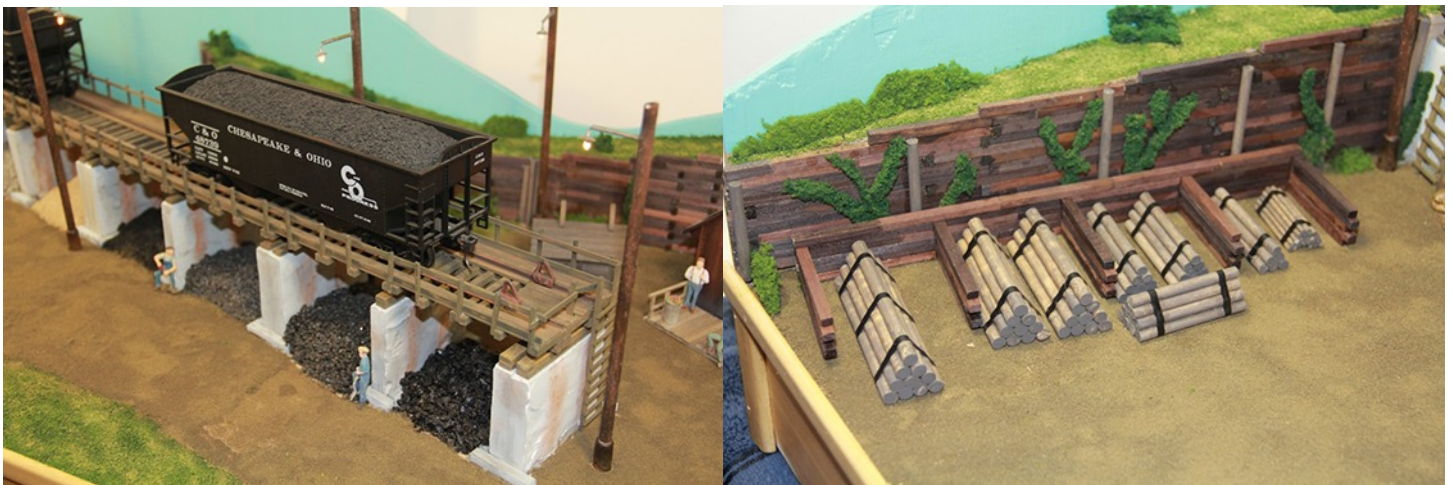
All wood products are Kappler brand strip wood (24” length) and scaled to O scale dimensions (2x4, 4x4, 2x8, 2x10, 10x12, etc.)

All scenery items are Woodland Scenic products – grass, shrubs, vines, earth/dirt.



All commodities in bulk are either commercially available items – various grades of coal or real sand used from a local supply store. The bulk commodities are secured to the base of the diorama using a mixture of clear glue, hot water and clear detergent (to help the water and glue mix thoroughly). Several applications were used with drying between to secure the product to the base.

All retaining wall items are stained railroad ties (glued to backer boards) and stained dowel rods (various diameters) used as support poles for the retaining walls.



Light poles are hollow styrene tubes which have been scratched on the exterior with a Zona saw blade to represent wood grain, and painted a dark brown to represent a creosoted pole (12 scale inch diameter). Wires are inside and connected to goose neck shades and bases using RCA plugs with receptacles mounted underneath the layout to facilitate ease of mounting the light poles. Lamps are 18 volt using a power pack with variable DC output of 12 volts – this allows for dimming lights and not overloading the bulb to burn out. Wiring underneath is in parallel with connector strips on back of display base to connect the power supply.

All O scale personnel are either Woodland Scenic or Artista brand products – I selected workers with shovels and one man wearing suspenders (boss). I also displayed a couple of lazy guys resting by the stacks of railroad ties.

The railroad ties in stacks are the same ties used for the retaining walls. Bundles of railroad ties and poles are secured by using pin stripe tape, from local hobby store, to represent steel bands normally used for bundles. Farmers/ranchers can always use fence posts or ties for various reasons.



The two structures (building and coal bunker) are also scratch built using Grandt Line brand windows and doors for the office. Porch wood has been weathered using an A-West brand product called Weather-It. The liquid gives an authentic looking old wood appearance.

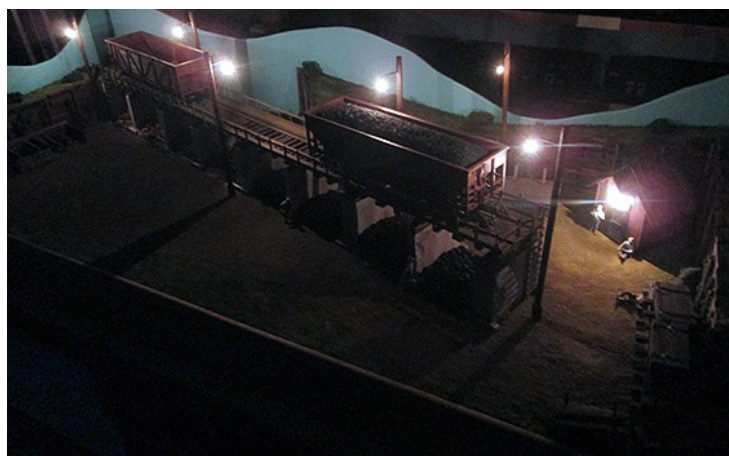
Concrete abutments are custom made using Plaster of Paris (from a local supply store). Abutments were made one at a time using a homemade mold configuration of styrene strips and flat styrene sheet for the base. Strips were taped onto weights (four individual weights) that can be positioned to create any size abutment needed and be able to be removed once the plaster has set up.

Weathering chinks were used to represent realistic colors of a real structure; such as, rust on the rails, rust streaks down the exterior of the concrete abutments, dirt/coal dust on the overhead ties and walkways, etc. The use of weathering chinks is a personal preference activity. The beauty of weathering chinks is that changes can be easily made to suit the builder. I purchased a set of weathering chinks from a local art supply store for \$5 – comes in a variety of colors to use or blend as needed.

My interest in railroading is primarily photography and building structures. This project is considered scratch building. Kit-bashing is changing an existing item by making alterations or additions to create a different end item. An example would be taking an empty flat car and repainting, decaling, adding wood railings/sidings, installing tool storage boxes and barrels/chains to produce a MOW railcar. Kit-bashing is not limited to just rolling stock – railroad structures can be kit-bashed as needed to fit a layout scene or area.

This is not my first scratch-building activity, but is something that I wanted to do to challenge myself. For first time scratch-builders, try a simple project to gain confidence and building skills. If this is your area of interest and ability, continue with projects that require more challenges in order to succeed – and have fun while you're at it!!

*Various scenes around
the coal dock...*





THE **O** **RESOURCE** NEWS, REVIEWS, INFORMATION TO USE **SCALE**

HERE IS HOW TO CONTACT US:

Phone: 815-584-1577
FAX: 800-783-0127
Email: amy@oscaleresource.com
 daniel@oscaleresource.com
Mail: The Model Railroad Resource LLC
 407 East Chippewa St.
 Dwight, IL 60420

www.oscaleresource.com **www.sscaleresource.com**

MAKING YOUR OWN USEFUL TOOLS AND JIGS

by Graham Gibbs

Graham shows us some of the jigs, tools and ideas he uses in his hobby of restoring mainly 1:43 scale diecast model cars. Many of these useful ideas can be applied to railway based projects...

My hobby revolves around collecting and restoring diecast Model Cars. Some of these projects and ideas are my own, while others are ideas that have been given to me by other hobbyists sharing their own techniques.

Some of these jigs should be a help to a range of other hobbyists who can then use them, adapt them to suit their individual needs, or make an improved version.

1. MY “SPRAY BOOTH”.



Fig. 1 - The beginnings of my “spray booth”. As you have probably worked out, I started with a recycled bedside cabinet for this project. I am anticipating that this cabinet will have a long “second life” as a spray booth. I do a lot of my work in the garage, so I have plenty of ventilation. Ventilation is a real consideration when working with spray paint as it has been the cause of many health problems. With any enamel or acrylic paint, please follow the Manufacturer’s recommendations for safe use.



Figure 2

Fig. 2 - The cabinet shown disassembled and converted to a spray booth. The door keeps dust and other foreign objects from getting on freshly painted models between coats. This is especially important as enamel paints have a longer drying time than acrylic paint. The Acrylic/Perspex front is not Necessary, but I had it on hand. I use it as a visual inspection of the painted object. I also added cup hooks to the inside roof of the spray booth to suspend parts for painting.



Figure 3

Fig. 3 - I used two of the original drawer runners to support a slide out platform for the model to be painted. More importantly, the model is able to dry inside the booth. When painting, I lay a sheet of newspaper on the table. An airbrush or aerosol will not lift the paper. Note that I did not fit any light, heater or fan into the booth. Apart from being unnecessary, because the models are small, a fan or a short circuiting globe could create a spark that might ignite any solvents.

2. HOLE PUNCH SET



Figure 4

Fig. 4 - An inexpensive tool that I have found useful is a set of hole punches. In my case, I paint model car wheel hubs and car headlights. I start by sticking masking tape on a piece of clean timber, then use a hole punch to cut the desired size hole. I then peel the masking tape from the timber and place it on a sheet of glass. I use a single edged blade to cut the masking tape to the desired size. The figure shows a completed wheel hub, one masked to paint the outside and one shaped like a donut and masked to paint the hubcap.

3. HARDENED GLASS PANE



Figure 5

Fig. 5 - A pane of glass, 6mm thick and hardened with all the edges smoothed, is useful when cutting masking tape.

I would not use any thinner than 6mm glass hardened glass. Being hardened, the glass is resistant to breaking if it is knocked in a doorway or on a bench when moving it around. I have accidentally knocked mine on many things, and up to now, have not had any breakages.

With hardened glass, I can use a cutting blade on masking Tape. The glass remains free of scratches, which on ordinary glass, may affect the accuracy of future cuts.

One tip I did get from a friend - when wanting to mask a straight edge, lay a length of the tape out on the glass then use a steel rule and single edge cutter. Then cut a new edge because the original edge of the tape can have a damaged or furry edge that can affect the quality of the line being painted.

4. CLAMPS

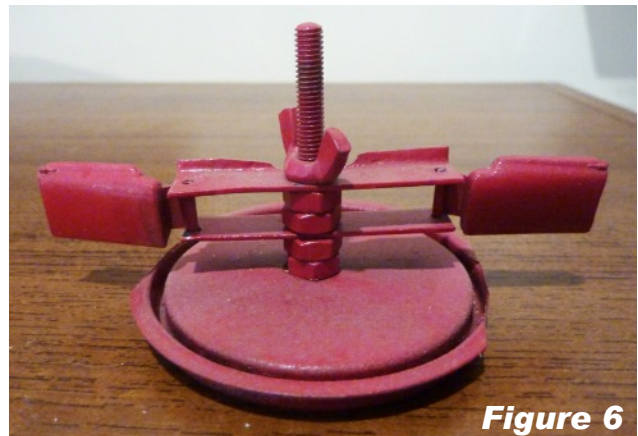


Figure 6

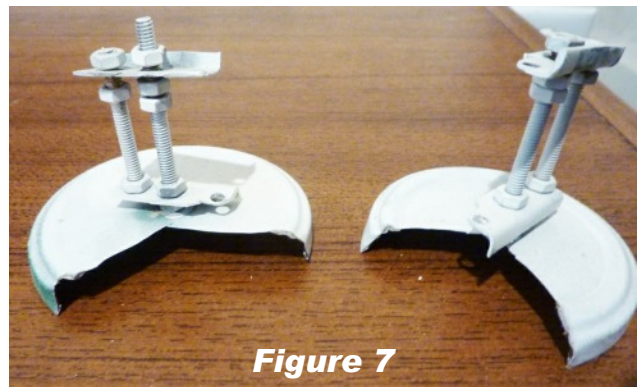


Figure 7

Figs. 6 & 7 Above - These clamps were made using jar lids so I can facilitate painting both sides of the doors at the same time. The cut out on the lid in the second picture is to allow access to paint the bottom edges of the parts.

Fig.8 Below – A car bonnet is being painted with one of the jigs shown in Figure 7.



Figure 8

5. ADJUSTABLE BASES

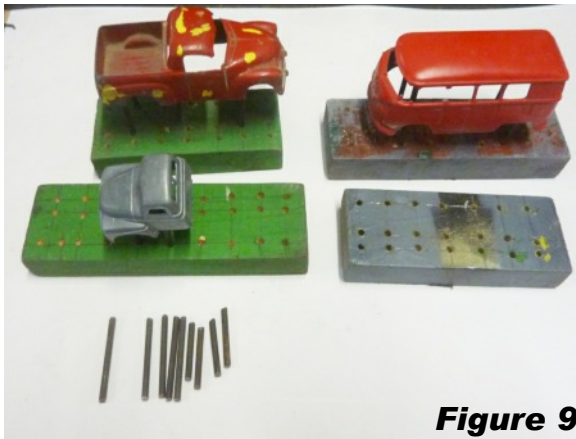


Figure 9

Fig.9 - A base with adjustable "pins" to allow different shape models to be level for painting. The pins themselves shown here are made from 3mm (1/8 inch) rod.

Editors Note: The pins can also be made by cutting appropriately sized nails and cutting off the the points and the head (optional) to the exact lengths you require.

By cutting off the head and leaving the points of the nail, you can use the nail as a drill and get an exact sized hole.

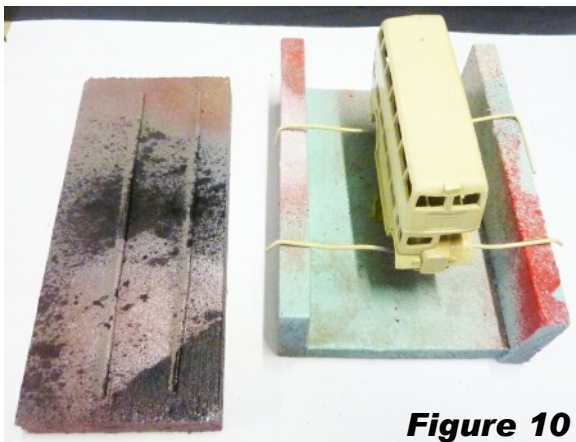


Figure 10

Fig. 10 – On the left is a base with two tracks. These are for resting a model on when the wheel hubs are unable to be removed.

The tracks in the base are spaced to allow the model to rest on the groove of the wheel hubs after the tires are removed.

On the right is a support for models such as the bus where the base could not be removed.

7. A ROTARY PAINT STAND

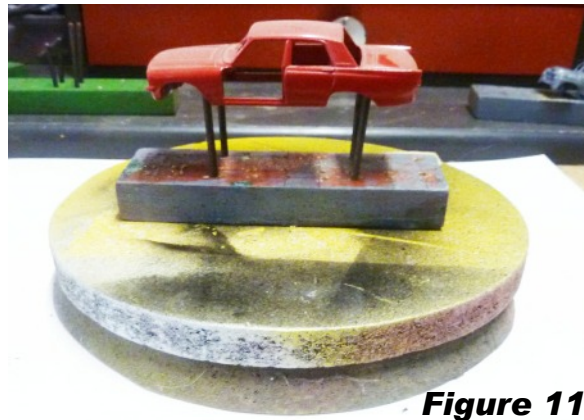


Figure 11

Fig. 11 - A homemade "Lazy Susan" type stand allows the model to be rotated during painting. Note the adjustable base is the one shown in Figure 9.

8. A THIRD HAND CLAMP

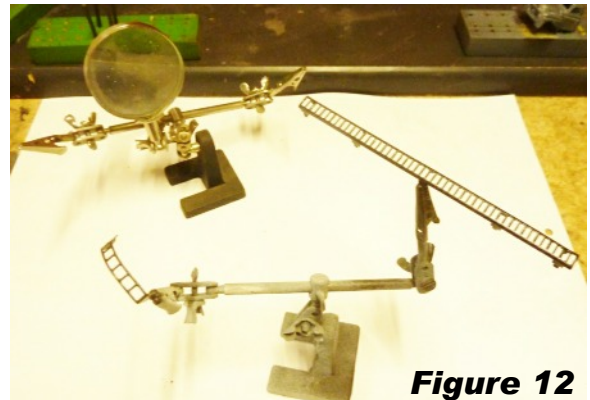


Figure 12

Fig. 12 - A clamp to hold smaller parts. These are available as a THIRD HAND CLAMPING DEVICE selling for around \$20 posted from the usual source. An original is at the rear of the photo.

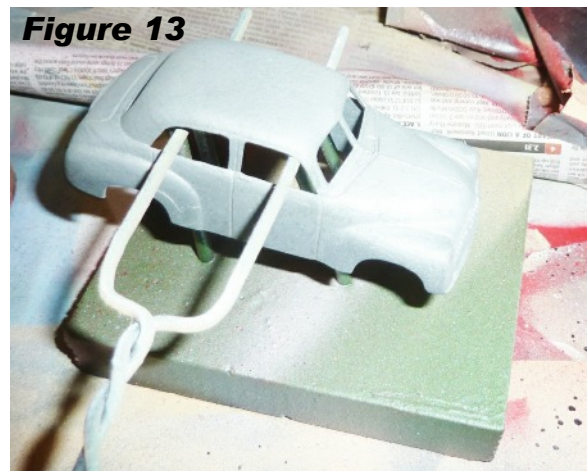


Figure 13

Figure 14



Figs. 13 & 14 – I made these forks to hold a model upside down to paint the inside and the underside edges.

These allow the model to be placed back on a base to allow the main body to be painted without finger contamination. I have made a couple of different sizes of these forks for different vehicles.

Figure 15

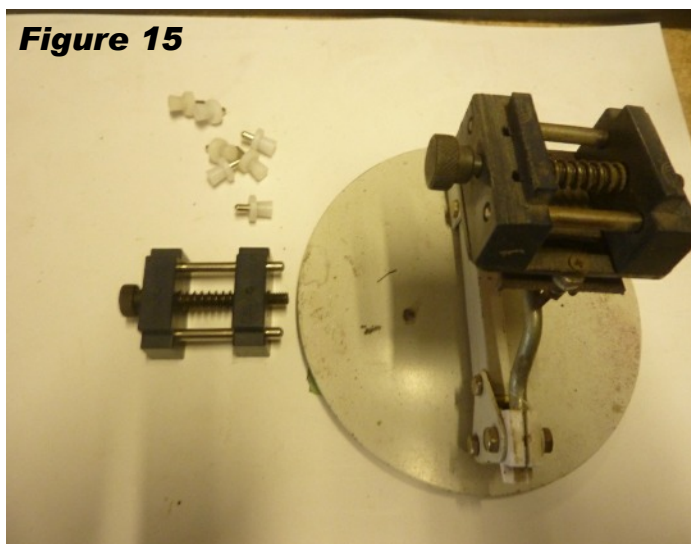


Figure 16

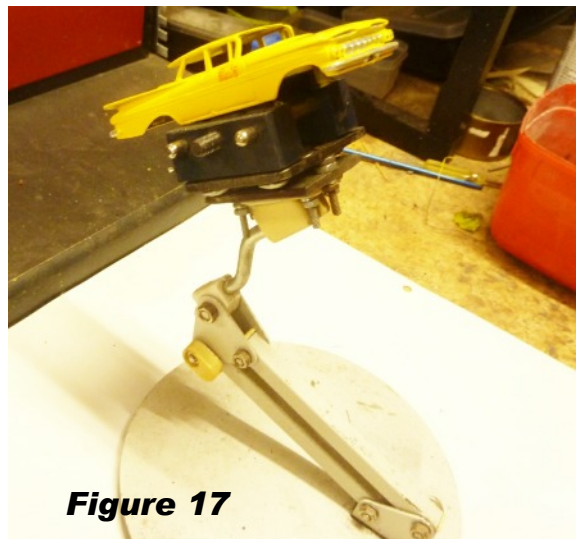


Figure 17

Figs. 15, 16 & 17 – This stand is made from a typist stand base and a modified clamp to hold a model.

This allows a model to be held at a comfortable angle to assist when applying fine details. The clamp is sold as a WATCH VISE or VIE.

11. A SIMPLE WHEEL PRESS

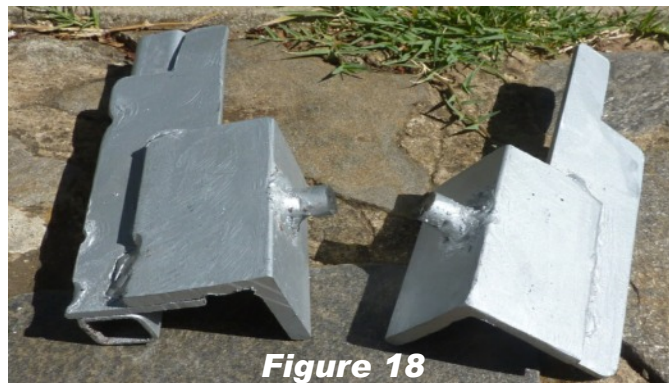
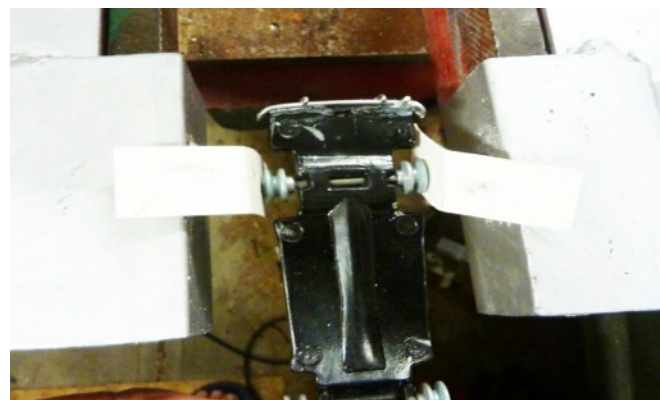


Figure 18



Figs. 18 & 19 – This is a simple jig that fits into a bench vise's jaws.

The tool enables painted wheel hubs to be pressed onto the axles. I use the cardboard to prevent damaging the fresh paint. - RMA.



THE **S** **RESOURCE**
SCALE **ONLINE**
NEWS, REVIEWS, INFORMATION TO USE

Don't forget we also publish The S Scale Resource Magazine. Click here to see what's going on in the scale S world as well as other articles of interest to all model railroaders.

WHAT'S ON YOUR WORKBENCH?

This series shows our readers what other modelers are working on. All that's needed is a simple snapshot of what your workbench looks like and the project on it. Send us a picture or two along with a short description of what you are working on so we can share it here. If it's a project under construction, send it in. Repair job, send it in. Completed project, send it in. Send your pictures and descriptions to daniel@modelrailroadresource.com

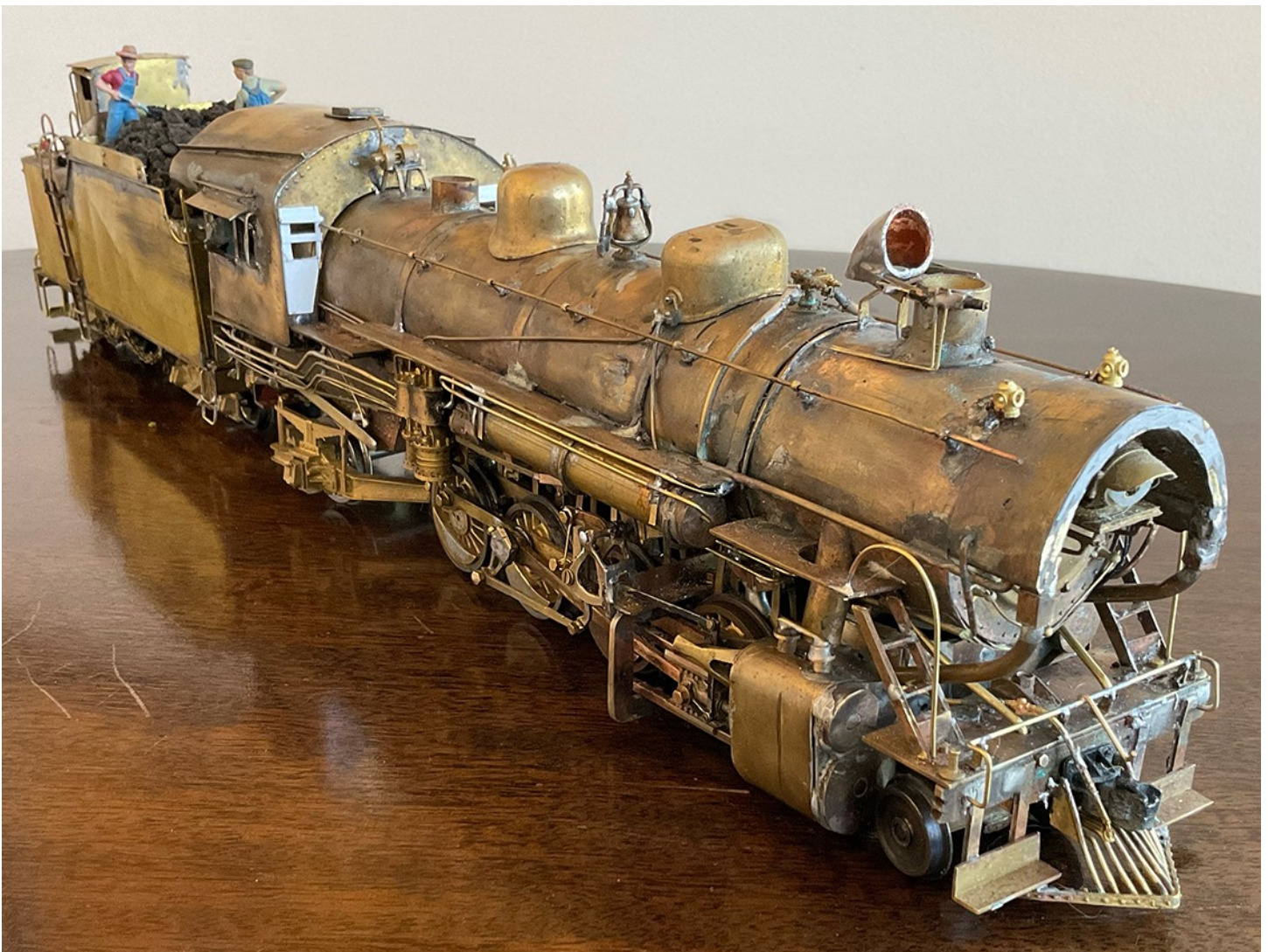
The last time we saw [Bob Fredrickson's](#) locomotive was in the [January/February 2018 issue of What's On Your Workbench](#).

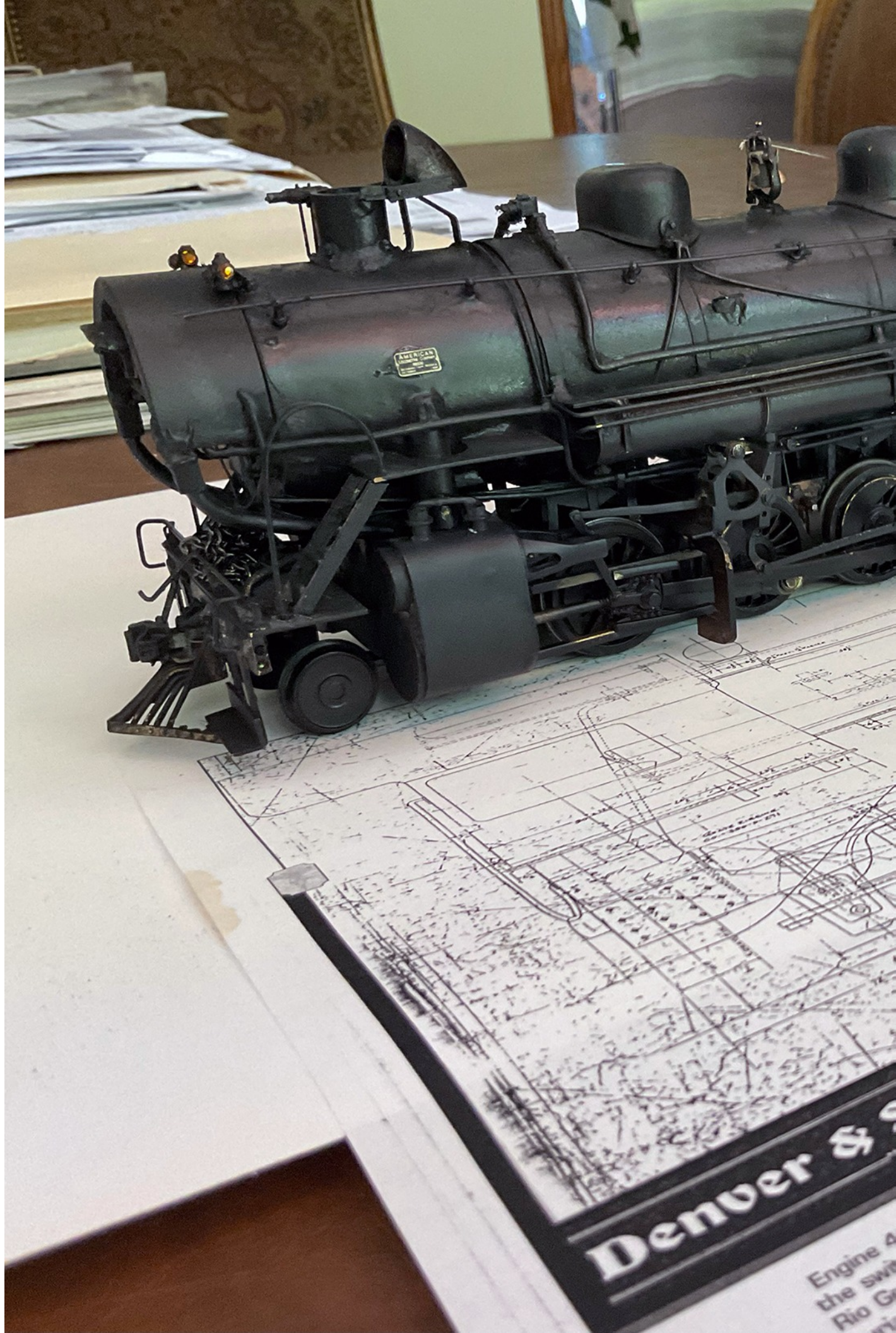
Now it's finished and Bob says, "Here's a picture of my completed Denver and Salt Lake ALCO Mikado. I initially sent a starting picture you published in early 2018.

It's finally done and operates well. I built it from the Erection Card in the D&SL Historical Society Magazine Corona Telegraph.

I made the frame from bar stock and the superstructures from brass sheet and K&S brass shapes. It has some castings from Dennis Mashburn, Precision Scale, and Bob Stevenson. The drivers are PSC castings with tires from Bob I machined and put together.

The pictures show the final build, along with the final paint and finishing.



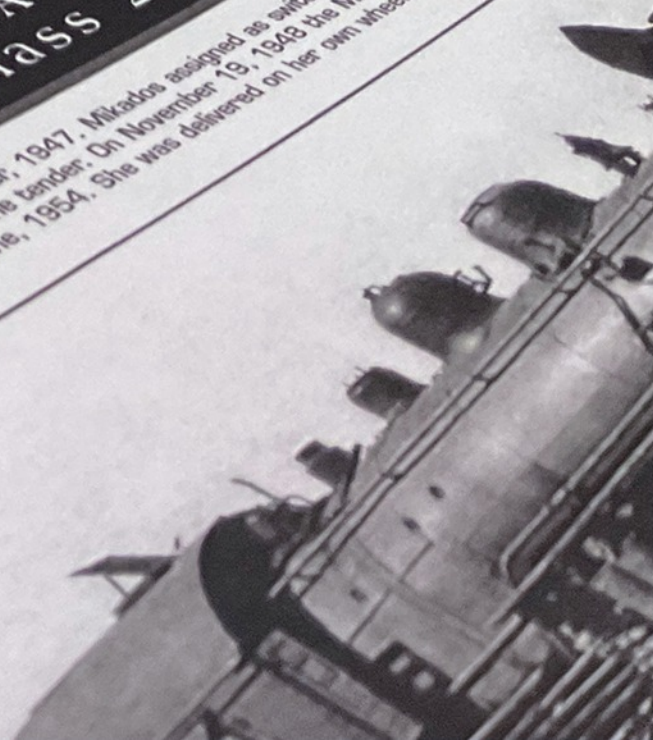




The Salt Lake Railroad
MOFFAT ROAD

LIMA LOCOMOTIVE COMPANY
Class 2-8-2 • Engines 401 - 407

402 performing switching duties near Pecos street September, 1947. Mikados assigned as switchers can be identified by
Schman's platform on the pilot and lack of coal boards on the tender. On November 19, 1948 the Mike was renumbered to
1222 and worked the yards around Denver until June, 1954. She was delivered on her own wheels for scrapping to
Bros. (CF&I) in Pueblo after 39 years of service.



READER CLASSIFIEDS

BUY ~ SELL ~ TRADE

To submit a wanted to buy or sell non business classified ad please click the link below.
<https://ribbonrail.com/railroadresource/Classified/> 725 Characters \$10.00 less contact information.
Please read all instructions on the classified page form.

NEW

READER CLASSIFIEDS

That's right, after many requests we will now be offering non business reader classified ads for buying and selling.

Fill out our secure on-line form here.

Please read all instructions, including magazine dates and deadlines to make sure your ad is published in the proper issue of *The O Scale Resource* or *S Scale Resource*.

The \$10.00 Basic Ad contains 725 characters less contact information. Two extra blocks of 725 characters are available for \$7.50 each. For larger ads, please contact us. Your Email address will be linked so anyone reading can contact you at immediately while reading the ad.

CLICK HERE
to place your ad today!

**WANTED: BRASS MODELS SCALE 0
WANTED ONLY IN NEW, NOT USED
CONDITION**

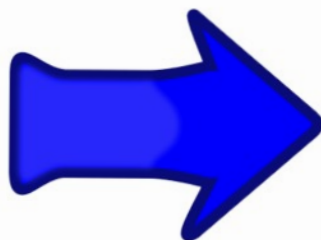
C&O Allegheny, Key Imports/Samhongs
SP GS-5 or GS-4, Black, Precision Scale
SP EMD E9 Black Widow, Key Model Imports
SP EMD SD-7, Precision Scale
SP EMD SD-9, Precision Scale
SP FM H-24-66, Car&Locomotive Shop/Boo-Rim
D&RGW Rio Grande Prospector Passenger cars,
Pecos River Brass/Train World
D&RGW ALCO PA A-B. 6001, Key Model
Imports
UP Big Boy, Safue/Japan
Herbert Beranek
Email: herbert.beranek@gmail.com

**WANTED: Looking for an Overland caboose,
Union Pacific (CA 11-M).**

David Richter
Email: eostrains@yahoo.com
Phone: 434-929-1013

**Contact our
Advertising
Manager
Jeb Kriigel
today!**

**ADVERTISING
WITH
US**



O SCALE SHOWS & MEETS

Have an upcoming O Scale event? We would like to help publicize it. Send us the information up to one year in advance, and we'll place it here along with a direct link to your Website and/or Email. [Click here to send us your information.](#)

42nd National Narrow Gauge Convention September 1-4, 2022

42nd National Narrow Gauge Convention. Join us in the beautiful Pacific North West for outstanding clinics, world-class narrow gauge layouts, stunning modular displays, your favorite exhibitors/vendors, and The Contest. All under one roof at the chic Hotel Murano in downtown Tacoma, WA.

Email: registration@seattlenngc.com

Website: <https://www.seattlenngc.com>

Southern New England 2 Rail O Scale Show September 24, 2022

161 Chestnut Street, Gardner, MA 01440

Train show with a large selection of dealers specializing in everything O scale! Ow5, Proto48, On30, On3. Free Parking and on site refreshments available!

Show Hours: 9:30am-3:00pm. Admission: Free

Email: sneshowchairman@snemrr.org

Web Address: <http://www.snemrr.org/index.html>

Southwest O-Scale Meet October 14-16, 2022

The Southwest O-Scale Meet is returning to Fort Worth this year. Event includes dealer tables, swap meet, mainline and traction modular layouts, Friday evening dinner, Saturday box lunch, and local layouts tours. Email: swoscalemeet@gmail.com

Web Address: oscalesw.com

Strasburg 2 Rail Train Show October 15, 2022

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$5, wives/children/military w. ID free, tables \$25 for first table, additional \$20 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068

[Click here for info.](#)

The Cleveland O Scale 2-Rail Train Meet November 5, 2022 @ 9:00 am - 2:00 pm

The website (<http://www.cleveshows.com/>) has updated information on the show. The show will still be at the UAW Hall (5615 Chevrolet Blvd., Parma, Ohio 44130) and happens from 9am to 2pm. Admission is \$7.00. Contact Sam Shumaker at (440) 248-3055 for table and additional show information.

Email: j3a5436@gmail.com

Website: <http://www.cleveshows.com/>

O Scale March Meet March 16-19, 2023

Westin Lombard Yorktown Center
Lombard, IL

The March O Scale Meet is a 3 day gathering of vendors, customers, clinics, and fun held annually in March in the Chicagoland area. This is the Chicago O Scale train show you've heard of.

Website: <http://marchmeet.net/>

Email: ChicagoMeet@yahoo.com

Strasburg 2 Rail Train Show April 15, 2023

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$5, wives/children/military w. ID free, tables \$25 for first table, additional \$20 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068

[Click here for info.](#)

O Scale National Convention June 8-11, 2023

Denver, CO

The O scale community will head to Denver June 8-11, 2023 for the annual convention of all trains scaled 48 to the foot. This will be the main event for O scale modelers looking for great layout tours, modeling clinics, and hundreds of vendor tables loaded with new products, as well as, treasures going back for more than 70 years. More details coming soon.

Website: <https://oscalenational.com/>



Of course we're on Facebook! Check us out.

MANUFACTURERS

"the people people"
Fun & Games
unparalleled craftsmanship

Phone: 573-619-8532
Email: orders@scalefigures.com
Web: scalefigures.com

PRE-SIZE
MODEL SPECIALTIES

Free Shipping
See our web site for details


Cast resin items for your S & Sn layout.
Tunnel portals, bridge piers, abutments, culverts, and retaining walls.
Trackside details and sage brush tree armatures.

WIT AND WISDOM MODELS
INNOVATIVE MODEL KITS IN O
AND OTHER SCALES
BASED ON JOHN ARMSTRONG DESIGNS
WITANDWISDOMMODELS@GMAIL.COM

SHOWS & MEETS

March O Scale Meet
April 1-3, 2021
<https://marchmeet.net/WP/>
Ph. 414-322-8043

DEALERS

Caboose Stop

Hobbies

[Caboose Stop Hobbies](http://CabooseStopHobbies.com)
301 Main St.
Cedar Falls, IA 50613
Phone: 319-277-1754
trainguy34@gmail.com
Web: www.caboosestophobbies.com

RIO GRANDE
SOUTHERN
RAILROAD
HOBBIES

RGSRR Hobbies
1102 Dyer Rd
Whitewater, CO 81527
PH 970-245-5100
www.rgsrrhobbies.com

Advertisers Index

Affordable Model Railroads	Pg. 6
All Aboard Trains	Pg. 15
All Nation Lines	Pg. 37
Allegheny Scale Models	Pg. 14
Allegheny Scale Models Hill Collection	Pg. 6
Along the Railroad by Howard Serig	Pg. 63
American Scale Models	Pg. 37
Back Drops Only	Pg. 6
B.T.S.	Pg. 15
Berkshire Car Shop	Pg. 15
Berkshire Valley Models	Pg. 6
Clover House	Pg. 6
Delta Models	Pg. 14
Des Plaines Hobbies	Pg. 15
Excelle Lubricants	Pg. 62
Grand Central Scenery	Pg. 2
March O Scale Meet	Pg. 3

Millhouse River	Pg. 14
Model Railroad Resource 3D	Pg. 5
New Tracks Modeling	Pg. 36
Norm's O Scale Trains	Pg. 14
O Scale Turnouts	Pg. 14
Protocraft	Pg. 14
RailFonts.com	Pg. 36
Right-O-Way	Pg. 14
Scale City Designs	Pg. 36
Stevenson Preservation Lines	Pg. 15
Streamlined Backshop	Pg. 5
The Public Delivery Track	Pg. 6
The S Scale Resource Magazine	Pg. 107
Trainz	Pg. 37
Tru-Color Paint	Pg. 63
Wasatch Model Company	Pg. 15