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O

RESOURCE

NEWS, REVIEWS, INFORMATION TO USE

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Volume 10 No. 6

July/August 2023

SCALE

**Canadian National Railways Sanmore Subdivision
2023 Update**

Old School Style Vintage O Scale 1926-54

Improved Uncoupling Magnets

O Scale National Convention

Getting to the Convention

New Tracks O and On30

New Tracks My Build

And So Much More...



BILL OF LADING

Published Bi Monthly

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Volume 10 No. 5

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Welcome to the online O Scale Resource magazine. The magazine is presented in an easy to use format. The blue bar above the magazine has commands for previewing all the pages, advancing the pages forward or back, searching to go to a specific page, enlarging pages, printing pages, enlarging the view to full screen, and downloading a copy to your computer.

Front Cover Photo

A CN local makes its way across the river on Serge Lebel's Sanmore Subdivision.

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The Model Railroad Resource, LLC publishes *The O Scale Resource* and *The S Scale Resource*. Be sure to look at both of our magazines. There are many articles in our magazines that are not scale specific and will be of interest to you. Click the magazine title in this announcement to see the magazine.

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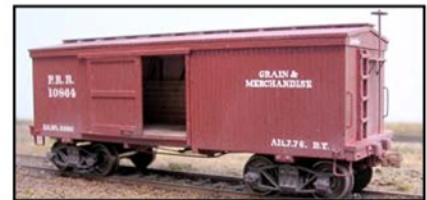
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- TCP-118 Providence & Worcester- Brown
- TCP-119 Providence & Worcester- Orange
- TCP-137 Dakota, Minnesota & Eastern- Blue
- TCP-138 Dakota, Minnesota & Eastern- Yellow



August, 2023 Set

12001: Southern-Central Freight Cars Set, #1

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- TCP-188 Missouri-Kansas-Texas- Freight Car Brown, 1944-1960's
- TCP-192 Gulf, Mobile & Ohio- Freight Car Brown, 1944 Forward
- TCP-201 Texas & New Orleans- Freight Car Red, 1949-1955
- TCP-220 National de Mexico- Freight Car Red, 1945-1960's
- TCP-240 Fort Worth & Denver- Freight Car Red, 1945 to Current



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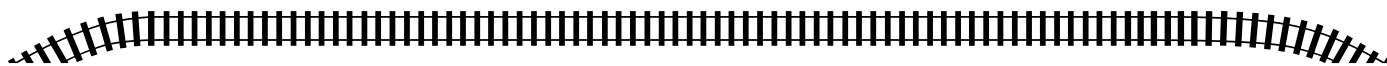
- TCP-5001 Glossy, Sprayable Medium Red
- TCP-5008 Glossy, Sprayable Medium Green
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NEWS YOU CAN USE



Kadee - The Coupler People showed their new NMRA standard metal O scale trucks at the O scale national. These include the trucks shown below. Note that these prices were for the show, and may not be the same when released.



 Made in the USA

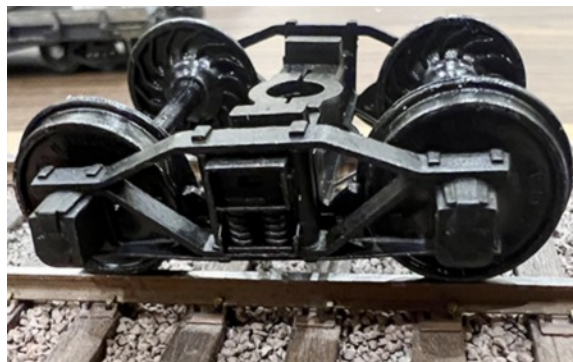
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No time frame has yet been announced as to when you can order these. I did buy a few of the arch bar trucks and brought them back to my layout. They roll well and look great.



O Scale Turnouts had a few new products at the national. A new #4 Wye – Rail-bound Frog and a #6 Wye – Bolted Frog.

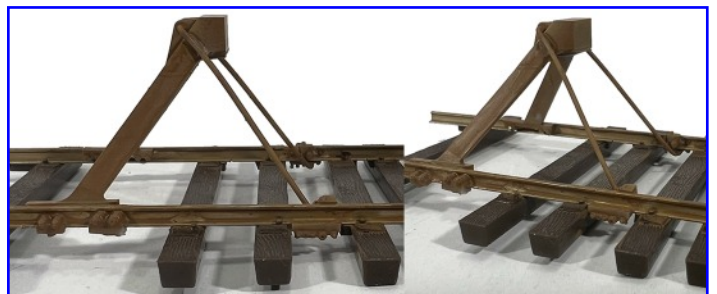


Check his Website for more details as well as his AAR Type A Trucks in both standard and P:48.

Check their Website for all their fine products.

Glenn Guerra's Limited Run O Scale Models delivered the first O Scale SW-1 Switcher in P:48 at the convention. This one will be headed back with Sarah Griessenböck. The next batch will be ready for people who preordered this July with small batches being delivered each month.

There maybe some extras after pre-orders are filled, so send Glenn an Email for information. Thanks to the people who pre-ordered the models. Your deposits made the project possible. He received 21 orders, and will be working on those first. When those orders are complete, he will offer the models again on a first come first serve basis until any remaining inventory is gone.



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Guest Editorial

National O Scale Meets

By: Brad Kowal

Those of us who attended the "O" Scale National Convention in Denver had a really good time. Especially those of us who added Railfan Trips along the way. Due to job related circumstances, I took about a 20 year hiatus from attending the O Scale Nationals until the 2021 Denver Meet. Sometime between the 1990's and now, attendance has dropped dramatically. Attendance was once comparable to the March Meet in Chicago, but now it's nothing close.

I understand that many who once attended have simply aged out (to put it as nicely as possible). That is understandable; however, there is ample evidence that younger people are out there in "O" Scale 2-Rail who should be filling in the gap, but for whatever reason, are not on a large enough scale. Yes, we see more younger people attending the meets, but there should literally be a few hundred more at each major meet.

For most of us who attend O Scale meets, there is so much more than simply walking around a swap meet. The ability to buy and sell is an important component; however, the social aspect is much more important. Those who attend the "O" Scale meets are in the mode and prepared to meet and befriend new people. Even for those who are not the most socially outgoing, it is easy to meet new friends whom you'll run into on a regular basis.

The National Meets provide a reason to travel and railfan a part of the country one might not normally cover. Railfan Trips are a lot of fun by themselves; however when you add the "O" Scale Meet destination, it doubles the fun for little added expense. It was really neat to travel 1200 miles in 2021 to walk into the Colorado Model Railroad Museum and hear somebody calling my name! It's also nice to know people from all over the Country, as well as the World, and to visit with them on a regular basis. Attending an "O" Scale Meet is similar to a Model Railroad Club where people from different professions and different walks of life get together and have a good time.

....And not everybody is flush with cash. Some folks will attend even when they're down on their luck and will share rooms, etc. in order to make the trip possible. Others have significant family issues that make it impossible to spend heaps of money at the meet, yet they still bring something to enter in the Model Contest and walk away with an award. Money, or the lack thereof, has not been an obstacle for many of us over the years. We knew the value of the Meet and we somehow made it happen. Not once has anybody ever asked another what their tax return looked like last year. Nobody cares about that. It's, "what have you been modeling lately".

Moving forward, we don't need to ask ourselves why we attend the meet. We need to ask the community if there is anything we could do to entice more people to attend. Does everyone thoroughly understand the value of the meet and what the meet is all about? Are we coming close, but still missing the mark?

There might not be anything we can do to save the National Meet, and our efforts might be better served supporting the remaining regional shows. If that's the case, we will adapt. But we, as the "O" Scale Community, need to be 100% sure before declaring the "O" Scale National Meet to be over and done.

There have been extensive conversations on Facebook, Groups. IO and OGR Forum covering the reasons why people don't attend. I think we thoroughly understand that. What we need to know is if there is anything within reason we should be doing that might increase participation. If you have any thoughts, please send an email to ChicagoMeet@yahoo.com. Information, except names and other source information, will be shared publicly within the various social media groups as well as O Scale Central etc..

Brad Kowal

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2023 O Scale National Convention

“The Dawdy Trip”

Another National has come and gone. Was it the largest? No, not like the ones I remember from many years ago. Was it well attended? Well, better than two years ago. Read our guest editorial from Brad Kowal in this issue, and let him know your thoughts on having a “National Convention”.

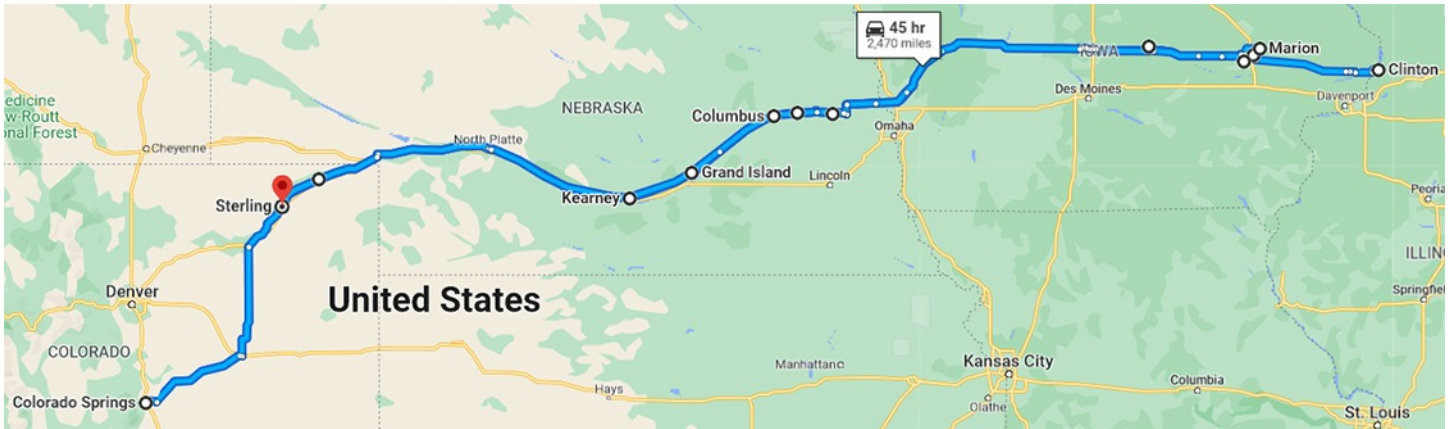


Click above to watch our video wrap up of the convention. What follows here is our trip out and the sites we saw. The convention wrap up will follow and then the display and contest models.

Amy and I did, as Brad did, made it a vacation and rail fanning trip, and if you have seen any of my Facebook posts, we also hit a lot of quilt shops along the way. That’s the key to any type of “vacation” like this. Make sure all the people traveling with you are engaged along the way. Sometimes getting there is half the fun. I bought a new Baofeng radio and used that for a scanner for the trip as we were going out on Highway 30 following the Union Pacific most of the way. We left early Saturday from our home in Dwight, Illinois and made it to the first quilt shop by 10:30 a.m. in Clinton, Iowa. Now I don’t mind these stops, and even look forward to them. I have to since many years ago I was the one planning vacations and hitting every hobby shop and railroad related “thing” I could find. You know what they say about payback!

Quilt shop stops in order: Clinton, IA, Mount Vernon, IA, Marion, IA, Cedar Rapids, IA (2), Fairfax, IA, Marshalltown, IA, Columbus, NE (2), Grand Island, NE, Kearney, NE, Sterling, CO, Broomfield, CO, Colorado Springs, CO, and on the return trip, Clive, IA.

The picture on the next page shows a wrap up of all the quilt stores we hit on our way to Denver and back.



As for trains, Marshalltown was the first stop. The M&StL/C&NW Marshalltown diesel house is now apparently used by EMD and has a working turntable.



Next, we caught an extremely long tank train with two UP units along with two CP units in Ogden, IA. Also, a bit farther down the track, was a waiting freight. (Next Page)





And below was what it was waiting for, a massive UP autorack train.



And then we come to this sad sight in Fremont, NE. Two years ago at least it was all together, but now it is gutted. Fremont and Elkhorn Valley Railroad SW1200 1219 nee Chicago and North Western 319, built by Electro-Motive Division in 1962. The dinner train went out of business in 2012 and some equipment was left in Fremont. The tracks are now being removed and on the follow page is what's left.



From our 2021 trip



From our 2023 trip

One of the cool things about staying off the main Interstates are the extra things you get to see. So many older diesels that are still working the mills and elevators. Western Rail Incorporated GP9 No. 1616 sits in Wood River, NE waiting for work.



Top: In Cozad, NE we caught SD18m Number 6634 owned by Debruce Grain.
Center: In North Platte, NE we see GP10 Number 7515 at rest owned by the Maywood Co-Op Association.
Bottom: SD20 Number 1021 owned by Farmland Service sits near Gothenburg, NE.





Gavilon Grain in Clarks, NE has this little guy on property.



Of course we could not come so close to UP's Bailey Yard without stopping again at Golden Spike Tower. The view of the yards is spectacular from the 7th-floor open-air observation deck and 8th floor with its a 360° view of Bailey Yard. Admission to the tower is Adults \$10.00, Seniors 62+ \$8.00, Children 6-16 \$6.00, Children 5 & under FREE.

If you are OK with ground level and the gift shop there is no charge.





One thing that Amy did set up for me was a stay at the [Hilltop Inn in Broomfield, CO](#). The [Burns Pub and Restaurant](#) sports over 500 types of whiskies. The food was great and the whisky selection was something else. I did a “flight” of five and was also schooled on the distillery of each. Now that was a night to remember, or not...



We finally arrived in the Denver area on Wednesday and had reservations on the [Georgetown Loop Railroad](#)





with a mine tour. The weather held, so far. The ride was very cool, although there was no steam on this day. The panning for gold resulted in about \$10.00 of gold and the mine tour was very interesting. All in all, a great day and well worth the visit. Since we could not set up until late Thursday afternoon Amy, wanted to go and see big rocks so Garden of the Gods it was.

Charles Elliott Perkins who was president of the Chicago, Burlington and Quincy Railroad bought two-hundred and forty acres in the Garden of the Gods for a summer home, and added more property later. After his death, the Perkins' children, knowing their father's feeling for the Garden of the Gods, conveyed his four-





hundred eighty acres to the City of Colorado Springs. So there is a railroad connection. That about wraps up the trip out. Five slow and easy days on the road to get to the convention. Coming back, we took the Interstate as we needed to get



home to work on Tuesday. After two days of driving, we were home unpacking all the neat stuff purchased along the way and at the show. Now, let's move on to the show itself....

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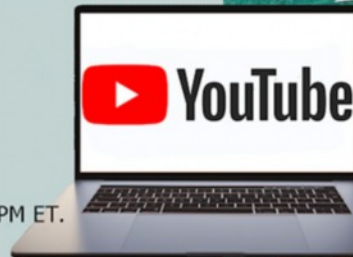
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2023 O Scale National Convention “The Convention”

The convention opened for business on Friday morning. To quote David Vaughn, “*The Convention was competently presented: the clinics were great; the layout tours were well-received; the model contest was credible; the trading hall was filled to capacity with tables, with lots of stuff and good prices; and the opportunities to socialize were well scheduled and presented. The folks who attended consistently report having had a good time. As they say in the movie business, it was an artistic success.*”

And it was, and there were many bargains to be had. Right out of the gate I saw the two engines below and there was another that just sold. I grabbed the GP35 on the left knowing a good friend might want it.



RGSRR Hobbies were there with a very large display of lightly used Atlas, MTH and Lionel cars, all two railed and some nicely weathered for \$35 a car. I grabbed a few of those for me even though I don't really need more cars. Sometime you can't just say NO!

The clinics were well attended, and the few that I was able to attend were very well presented, probably the best in many years of all the shows we have attended. My own 3D clinic was a success with many good questions, and I was able to put in simple terms how to judge all the hype around printer marketing. In the next issue, I'll have an article based on this presentation as I really need to update people on 3D printing information.



Speaking of clinics, it was a pleasure to again meet up with Sarah Griessenböck, and for the first time, Santiago Pineda. Both are extraordinary modelers and their clinics were well attended both times. Sarah on details, 3D printing and fabrication, while Santiago covered his reworking of the Exposition Flyer Train and Cars.

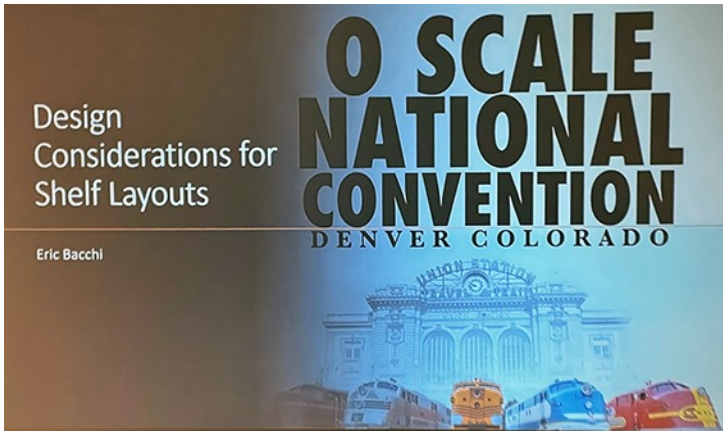
George Bogatiuk from [Soundtraxx](#) offered clinics on their new Blunami line of decoders while Bob Sobol covered P&D drive improvements.

David Stewart talked about modeling a working hump yard which was really good. Paul Hanson finished Friday with an introduction to Proto:48.

On Saturday Eric Bacchi had an informative clinic on design considerations for shelf layouts. We later went to Eric's layout and saw first hand his ideas. Look for more in an upcoming issue.

Charles S. Goodrich did a clinic on painting and weathering, with Eric Peterson following up on space for O scale/track geometry. And lastly, Rick Trinkle not only showed off his beautiful 3D printed hoppers, but talked about 3D printing model railroad production.





It's always fun to catch up with people you don't get to see if it were not for shows.

Top left: Bob Lavezzi, Kevin O'Connor and Boyd Reyes. Below left: Bill Thayer and his grand daughter Caty.

Below: Charles Goodrich with Brady McGuire.

Below bottom: Norm Buckhart with Jim Wolf.



Top left: Boyd Reyes.

Left center: Amy with Chris Lane and Shelly Yowell.

Bottom left: Eddy Wichman and Sarah Griessenböck.

Below: George Bogatiuk and Eric Peterson.

Bellow bottom: Gregg Laiben, Jay Criswell and Santiago Pineda.



To me, this is a big part of gatherings like this. The comradery, exchange of ideas, and seeing new ways of doing things all add up to a most enjoyable time. Ok, on to the eye candy, and once again, check out our video for even more images and [video here](#).





Top left: Ed Jakl. Oh yes, that KTM hopper went home with me also.

Bottom left: Jerry Snow's tables.

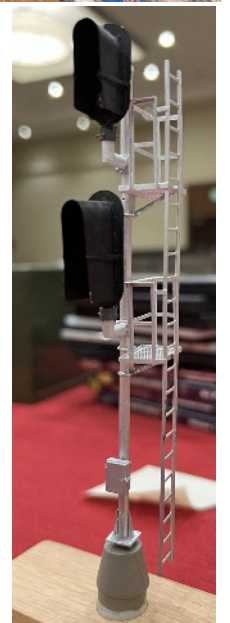
Top right: The Detroit Model Railroad Club's layout. [Much more on their layout in the video here.](#)

Below: Kadee showing off their new O scale trucks.

Bottom right, Rails Unlimited offerings.

Following pages, just more trains!







MARCH O SCALE MEET

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O Scale National Convention

“Display and Contest Models”

There were many beautiful models this year at the O Scale National Convention this past June in Denver. Many were display only, but there were some judged models as well. Let’s sit back and take a look at some of these fabulous models. *Models shown are in no particular order.*



CP Rail Boom Truck - Bill Yancey - Display



CP Wood Combine - Bill Yancey - First Place Passenger Car

O Scale National Convention "Display and Contest Models"



CB&Q Boxcar - Jeff MacDonald - Display



BN Boxcar - Bill Yancey - Second Place Freight Car

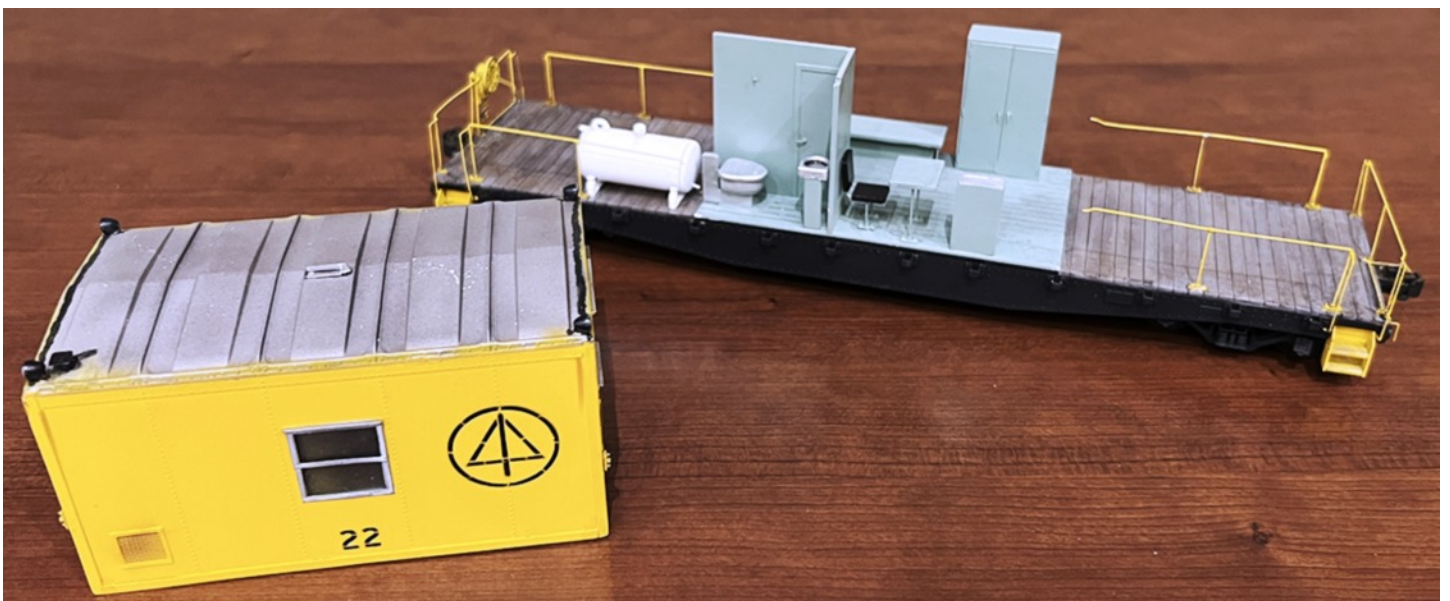


MNS Boxcar - Bill Yancey - First Place Freight Car

*O Scale National Convention
"Display and Contest Models"*



Vance Junction Section House - Don Vancil - Display



Oyster Bay Transfer Caboose- Jeff Briggs - Display

O Scale National Convention

"Display and Contest Models"



CONX Tank Car - Hugh Blaney - Freight Car



WC SW1200 - Shaydin Anthony - First Place Diesel Locomotive and tied with the Denver Union Station for the popular vote.



NP Boxcar - Mike Booth - Freight Car

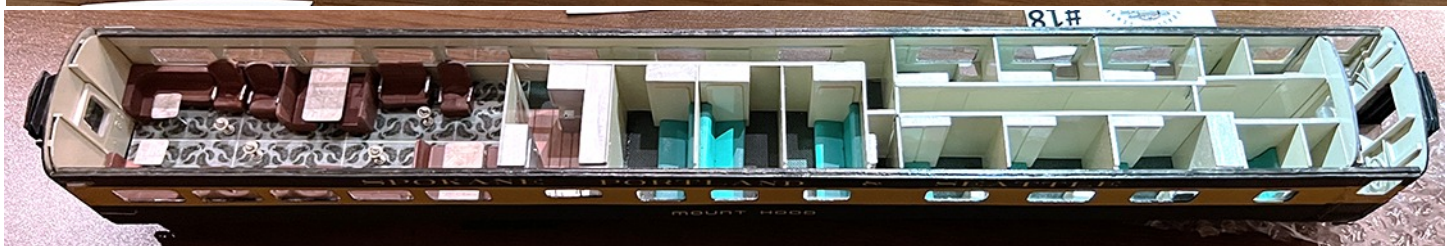
O Scale National Convention "Display and Contest Models"



CP Minibox - Jeff Briggs - Display

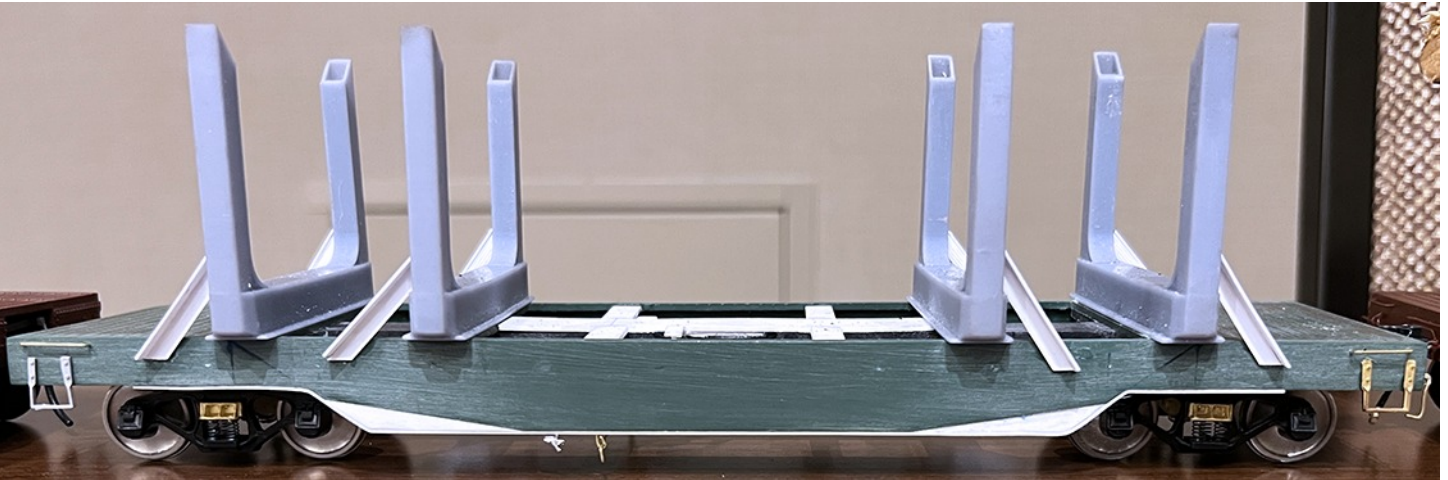


NP Challenger - David Johnston - Display



SP&S Passenger Car - David Schultz - Passenger Car

O Scale National Convention "Display and Contest Models"

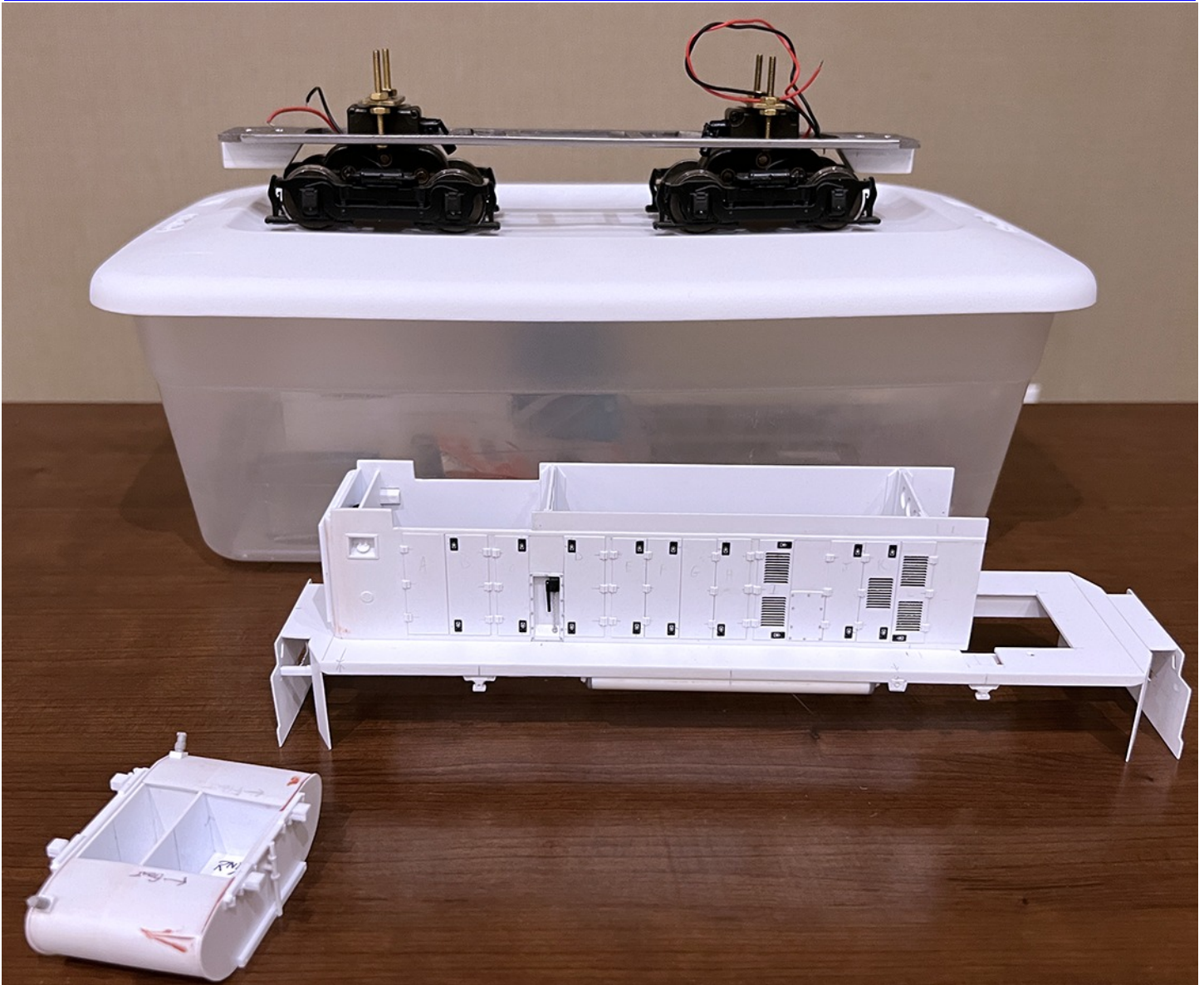


Milwaukee Road - James Schultz - Freight Car (Shows build progression)



CNW Coach - Jeff MacDonald - Display
The O Scale Resource July/August 2023

***O Scale National Convention
"Display and Contest Models"***



SW1000 under construction - Richard Bacon- Display



ATSF - Joe Norman - Santa Fe, NM Freight Transfer Shed - Structure

O Scale National Convention "Display and Contest Models"



Denver Union Station - Ron Maxfield - First Place Structure and tied for the popular vote.





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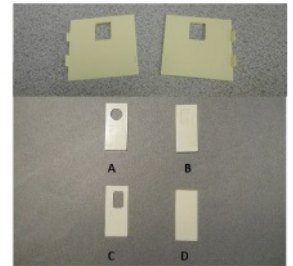
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Canadian National Railways Sanmore Subdivision

LAYOUT PROGRESS UPDATE - ENDLESS RESOURCES

By [Serge Lebel](#), photos by the author



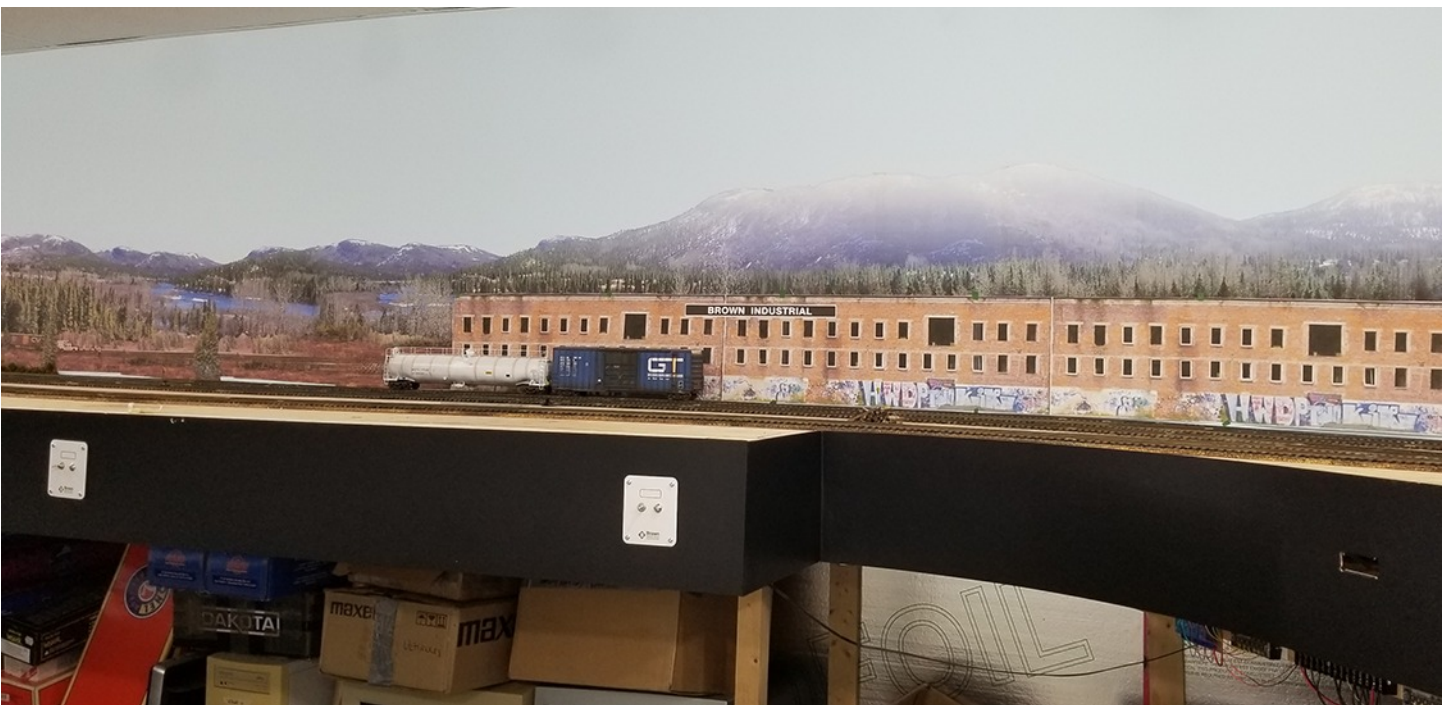
I sometimes look at my layout and think to myself that this thing is a bottomless pit that swallows everything I have. Building a layout, no matter how big or small, just seems to require endless resources; there is never enough space, time, money or energy to do it all. I am sure I am not the only one who thinks that one lifetime might not be enough to see my goals accomplished. Writing this progress article every year for *The O Scale Resource* magazine has been a real blessing, and a wake-up call. This is the only time I am forced to look back at what I have accomplished in the past 12 months and do an evaluation of my progress, and my modeling.

What does this progress look like? I am sure the answer to this question is different for everybody; for some, it might be that golden spike moment when you can finally run a train from one end of the layout to the other... for others, it is all about applying the final coat of paint on a scratchbuilt locomotive that has been years in the making. For me, I measure progress in the amount of enjoyment I get from reaching a certain goal. I could build a massive structure and never see any progress in it if I did not fully embrace the process.

I am sure you heard it before... it is sometimes best to just walk away. Well, that is exactly what I did last year, which I had mentioned in my last article. I took a 5 month break from model railroading after decades of non-stop modeling. This proved to be very beneficial to both my mental health, and to my hobby.

I always have projects lined up and deadlines for most of them, so by the time I came back to my modeling, I had some catching up to do! Looking back on the 7 months or so that I spent on the layout, I think I did better than what I had expected in terms of getting things done.

During my time off, I worked on my landscaping projects, and discovered a new hobby in radio controlled scale vehicles. I had a blast, learned a lot of new things, and even discovered items I could bring into my model railroad that would improve my models. By the time fall was well under way, I was happy with the time I spent away from the layout and was starting to itch for some trains, so I resumed my modeling in late October. Just before my break, I had started on the basic design of the Irving Oil transloading plant and the Donald Francoeur trucking company, and I had also covered most of the layout with the initial cardboard scenic structure in order to fill out all the empty benchwork spaces where trains could derail and land on the concrete floor. So my first order of business was to get the backdrops completed so I could finish up the hardshell, which I did on about 97% of the layout.



Having the layout covered, I then turned my attention to structures. I love scratchbuilding structures just as much as I enjoy operating the layout. For me, there is no greater satisfaction than to see a building take shape as my imagination runs wild on how this building will be used in my little fictitious world. But building structures is always a bit of a hit or miss situation for me, so I decided to try something I have seen many people do: place holders. A good example of this is my car shop in Chattaway Falls, where I made a paper structure to put in place just to see if the size was adequate. This structure will get replaced with a scratchbuilt one soon, but at least for now I get a good idea of what I think I would like to see here. This is very low cost in both time and material, and helps to avoid mistakes on the final model. I think I will do this for all the larger structures on the layout in the future.



Feeling nostalgic...

When I designed the layout, I knew I wanted to bring back an element from my N scale days. I modeled in N scale for over 22 years, and did a lot of scratchbuilding on my layouts. Back in the early 2000's, I built a

basement sized layout in N scale which depicted the area I was living in New Brunswick. Over there, Irving has a dominant presence, in the gas industry, forestry and transport. Since Douglas Cove on my new layout was on the shore of the bay, it was a logical area to set an O scale version of the plant I had done more than 20 years earlier.

I guess you could say that the plant is a dog's breakfast of the prototype, my N scale version, and the new and improved O scale vision I had. As with everything in O scale, space is always a consideration, but I did allow a 20 foot section for this structure in my original track plan. Filling this space was not a problem. On my original N scale version, there was the management building which holds the offices of the regional manager, as well as the sales office and customer service for both residential and commercial customers. I had also modeled the large capacity oil tanks. All of this was scratchbuilt based on the prototype, and so I wanted to do the same on this layout, but bigger and better. Having the extra space allowed me to have more fuel tanks, and a propane distribution area. Being so close to the bay meant that the petroleum products would arrive by ship and be transferred into the large tanks via a pipeline, so a proper pump house would be an important element.

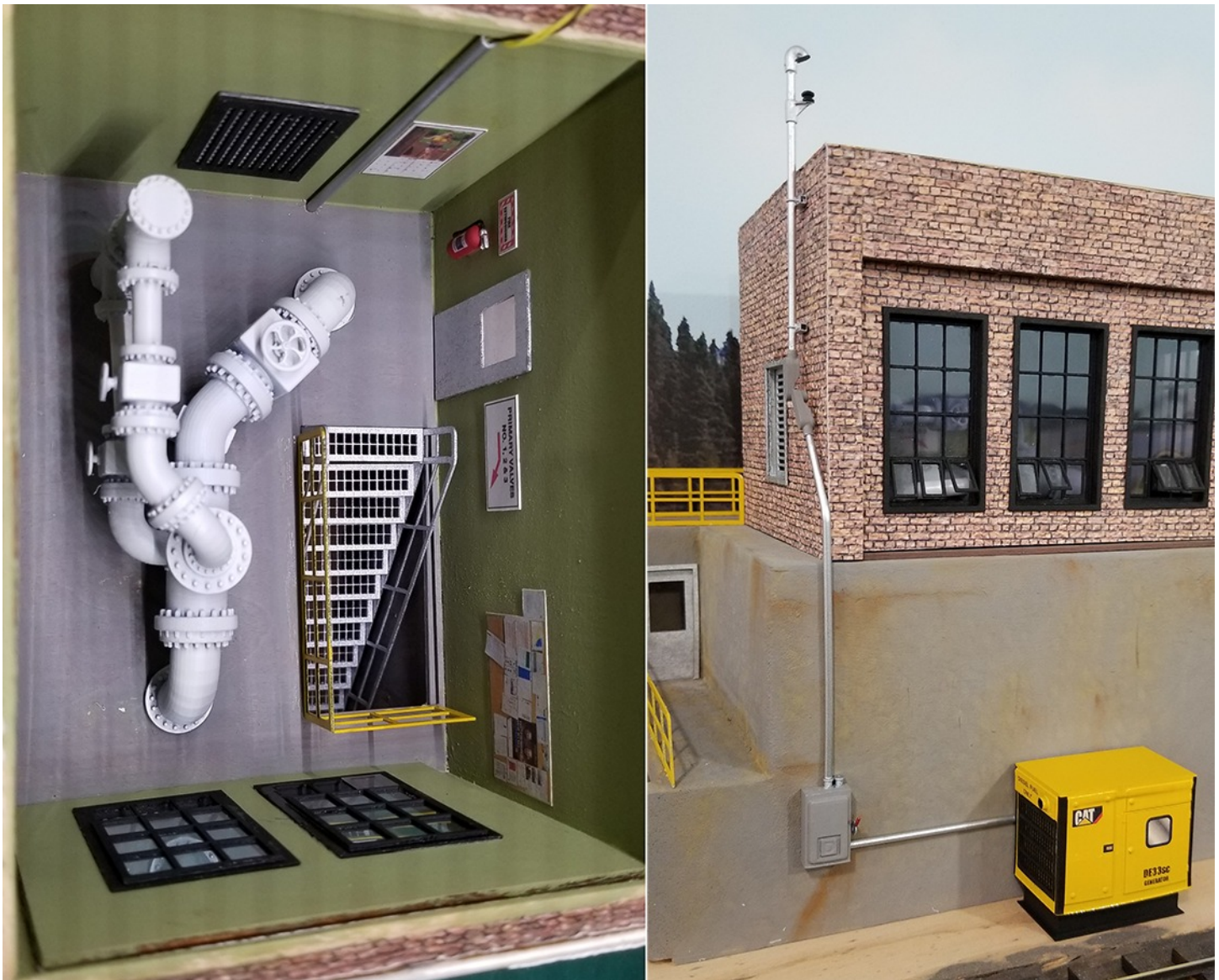


Now I am not going to do a build article on the various parts of the plant, but since this is a construction update, I have taken the liberty of adding a few construction photos and will do a quick overview of the various structures that makes this project somewhat logical...

The pumphouse:



The pumphouse is a very simple two-story building that houses the large pumps in the basement, and the valves on the main floor. Not being based on any prototype, I used a bit of imagination as I am more of an artist and not much of a structural engineer! Still, I took the time to design the valve system so it would make sense, by routing each of the three petroleum products (gasoline, diesel and kerosene) to their respective pipeline connected to the appropriate tanks.



The basement also holds the oil burning furnace to keep everything warm in the winter, and a diesel powered generator for backup is located right outside in case of power failure. These items were designed and 3D printed on my Elegoo Mars.

The building was built out of thin plywood and was covered with paper brick for the upper part, and painted wood filler created the texture for the foundation. All the details were 3D printed and the windows were laser cut out of cardboard.

The fuel tanks:

I used 6 inch construction cardboard tubes (the type used to pour concrete for patios and fences) as the base of my vertical tanks. They were covered with styrene and detailed with laser cut parts except for the caged ladder which is by Plastruct. I printed some decals for the product identification.



The large IRVING letters were masked using vinyl stencils and painted with Vallejo paint. All the tanks were then glued to a hardboard base painted to simulate concrete.

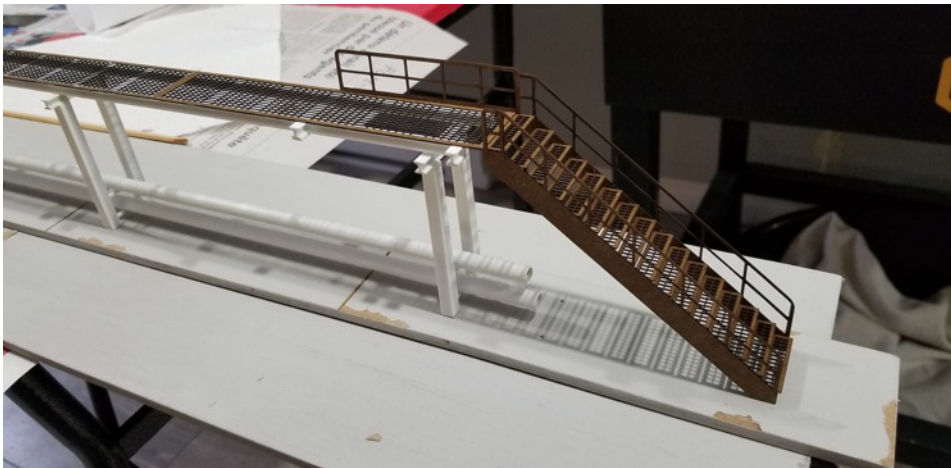


As for the large gasoline tank, I simply cut two half disks from hardboard and put some vertical posts all along the edges, and then covered that with styrene. The long staircase was cut out of cardboard and glued in place.



The loading ramp:

I wanted a loading ramp that would accommodate 5 tank cars on each side. I also wanted the center, or main ramp, to have smaller positionable drop ramps that would give access to each tank car safely.



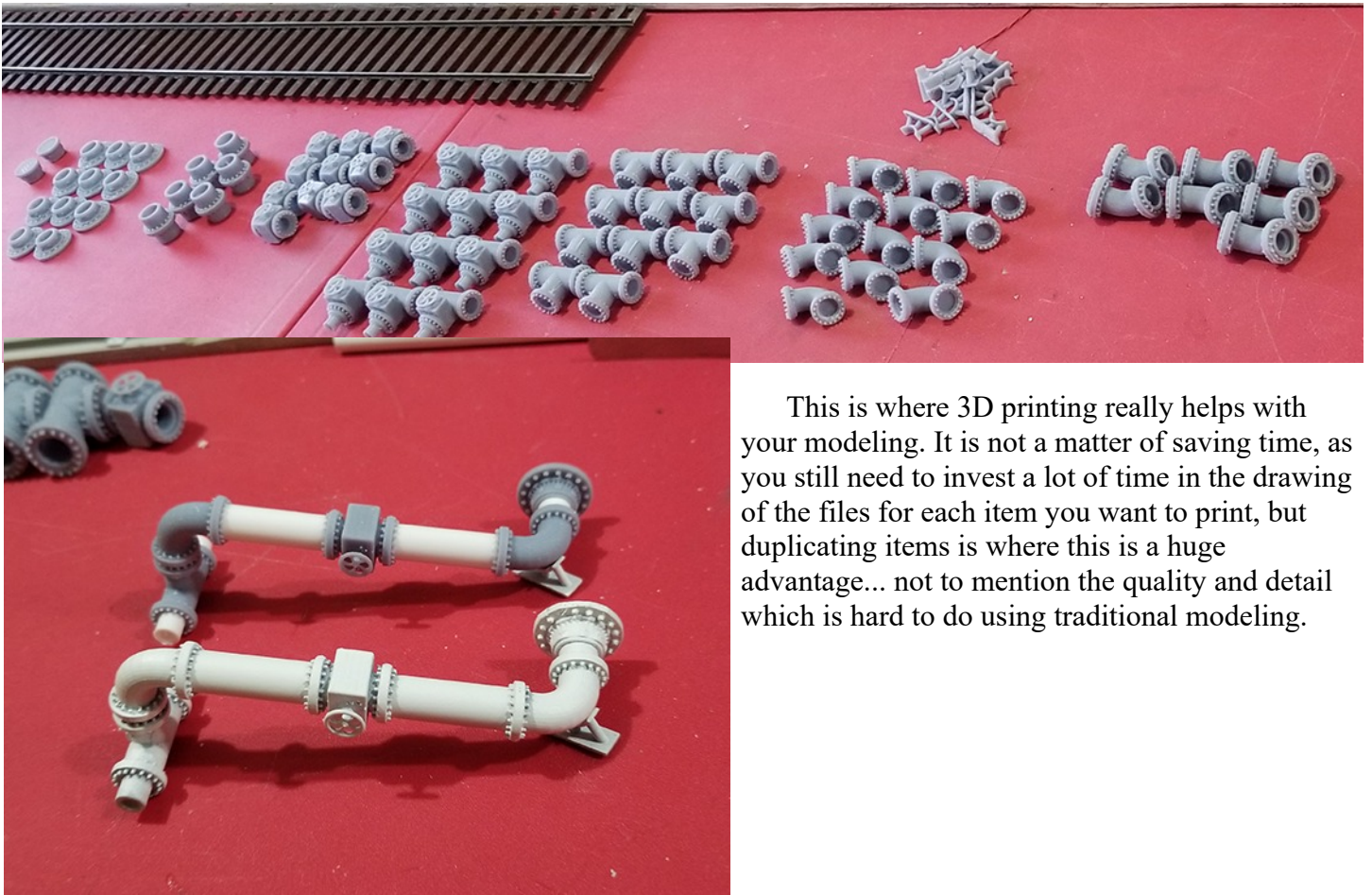
The main ramp was lasered out of laserboard and it is supported by Evergreen styrene H columns. The small drop ramps were etched out of brass. I also installed some lighted poles which are modified brass lampposts I bought on-line.





The pipeline:

Connecting all of these structures is a series of pipelines, which I build using styrene tubes and a good assortment of valves, connectors, vents, and angles which I designed and 3D printed. The small tank car connecting hoses at each station is just a piece of 10 gauge electric wire to which I added some connecting hardware.



This is where 3D printing really helps with your modeling. It is not a matter of saving time, as you still need to invest a lot of time in the drawing of the files for each item you want to print, but duplicating items is where this is a huge advantage... not to mention the quality and detail which is hard to do using traditional modeling.



The propane tank:



My plant would not have been complete without a propane distribution center. For this one, I had a short piece of PVC pipe laying around, so I used it along with some other parts I found in my scrap piles. I 3D printed the smaller propane tanks and made some decals.

The scene is not yet finished as I need to install the chain link fence (made with brass tubing and wedding veil material). The fence is seen here sitting on top of the plywood, but once I drill the holes and plant it in place it will look much better!



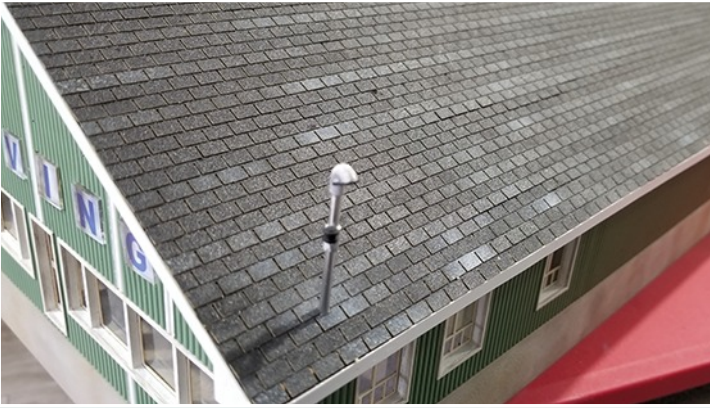


The office:

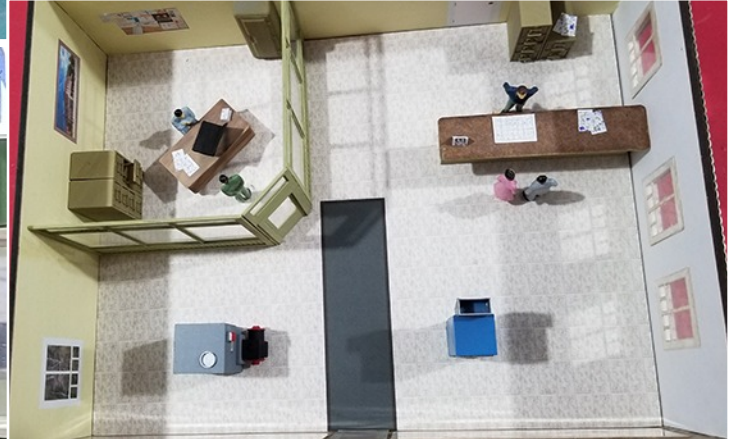
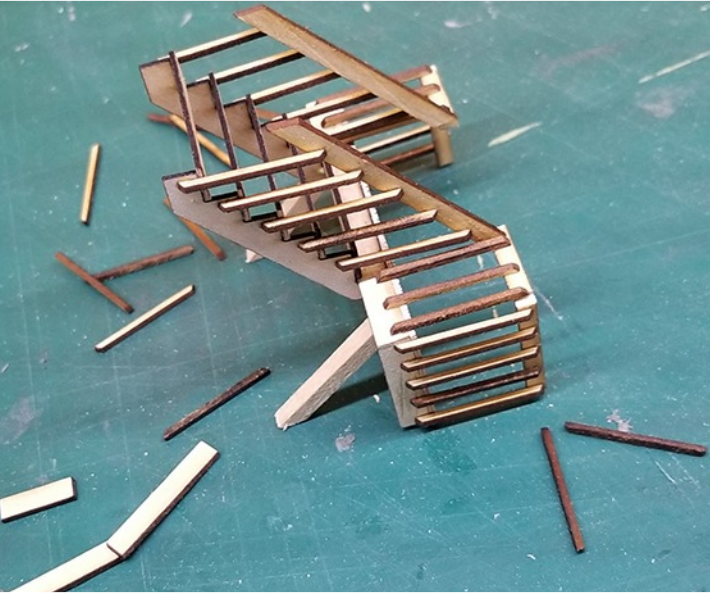
Of course, my plant would not be complete without the office. This one is a loose interpretation of the prototype, as I had to compress it a lot to fit the space. The building is made from Masonite as the base of the structure, which was covered with styrene.

The roof is laser cut sand paper, and I added a bunch of 3D printed details like the chimney and the electrical power cable mast.





I detailed the interior and installed LED lights inside, as well as on each of the individual lettered signs that makes the front of the building. LED is hard to capture in a photo, but this looks quite good in reality.



Having everything lighted will make for interesting night operations!



Just across the street from Irving Oil is the Donald Francoeur trucking company. This one is not based on any existing structure, but it is a dedication to one of my dear friends. The building serves as a transfer warehouse for Midland Transport and FedEx.



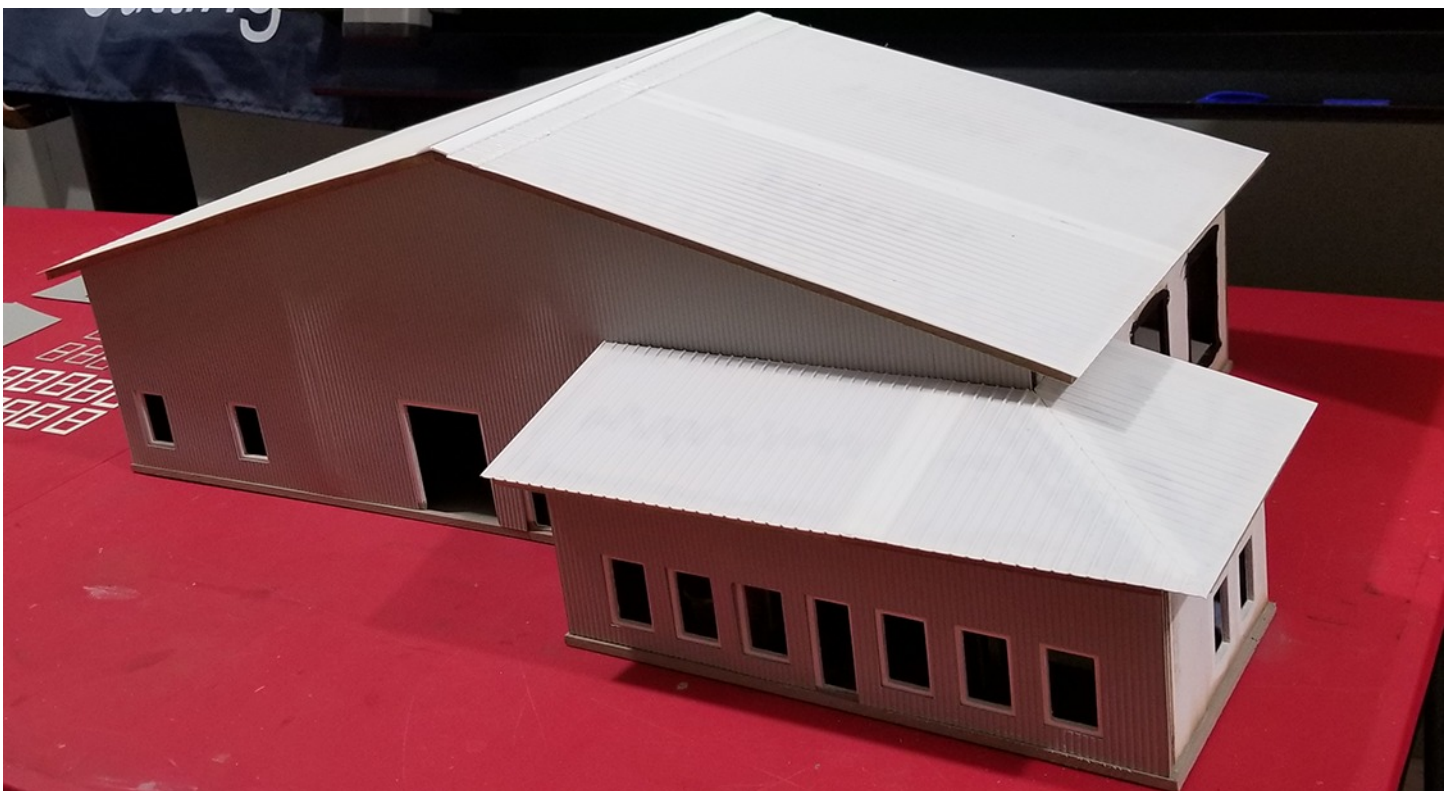


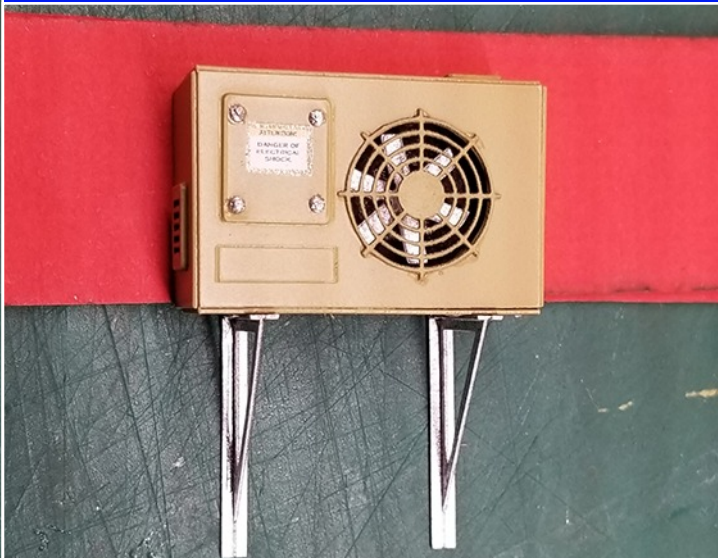
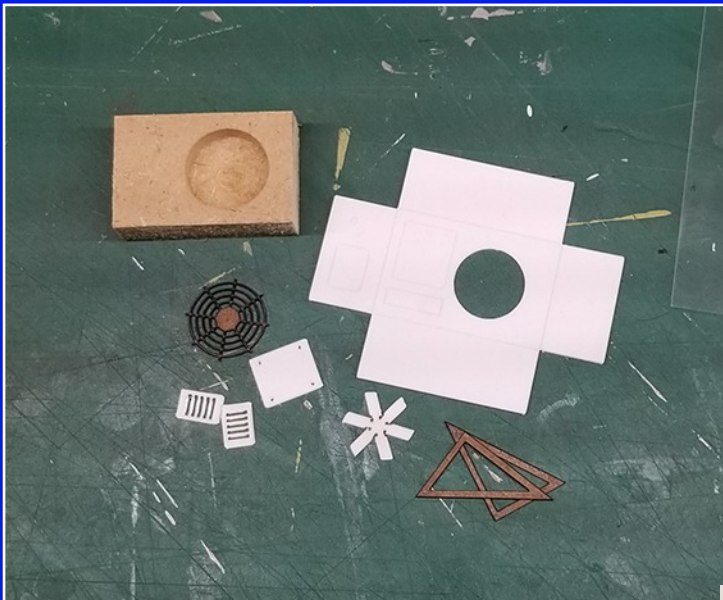
This building was built just like the Irving Oil office, using Masonite hard board as the bones, then adding a skin of styrene. The details are either scratchbuilt from styrene or 3D printed.

Once again, I detailed the interior and added some lights for visual interest.



I keep telling myself that I only want to create a stage for the operations of the trains, but somehow I always go down the rabbit hole of adding details that just end up taking more and more of my building time! This building has no ties to railroad operations on my layout, it is more of a space filler and an element to create a more believable scene.





Speaking of making things more real, I finally started on my scenery! This was one of the aspects of my modeling that I was most looking forward to. Doing scenery is a very creative artistic process which I enjoy a lot.



I have to admit it has been close to 20 years since I last did any scenery on a layout, and I was so surprised by the new products available today. Add to that all the fantastic YouTube tutorials by some of the most talented people I have ever seen... It just feels like I am learning a whole new hobby.

I knew that this phase of my layout would be a critical one in making or breaking the overall project. I put a lot of time and consideration in my backdrops and I needed the scenery to be an extension of this visual aspect. In order to blend the scenery with the photo backdrops, I used some of the natural elements that are found exactly where my photos were taken. I used dead tree leaves, tree moss, twigs and dirt as my base layer. I ran all of these in the blender and sifted everything.



I then added some grass tufts, static grass and other commercial products. Rock faces were done the usual way with plaster, and I added some real rocks around the scene. The plaster, the foam and the real rocks were all primed with artists' gesso, which is a medium that neutralized all the various underlying textures and accepts all types of paint to bring out it's characteristics. I then painted the rock faces using acrylic paints and oil washes.

The river was created by making a river bed out of small rocks, gravel and paint, which was then covered with bar top epoxy resin. I added some ripples using glossy Modpodge. I poured three coats of the resin in order to give it that deep look. The first pour was tinted very lightly with olive green paint, while the last two were just transparent.





Deciduous trees are blueberry bush (yeah, wild blueberry pickers around my stomping ground really hate me!). They are not finished yet, as I need to add some fiber paste to the trunks and re-paint them before I can glue them in place. Because my scenes are mostly very narrow, I opted to model early spring season so the trees would not have leaves yet.

This see-thru 3D effect makes the scenes look much deeper than they are. The backdrop is just visible enough to help create that illusion. The spruce trees are made from balsa sticks wrapped in floral wire, sawdust (for the bark) and static grass. I learned this method from watching Boomer Dioramas on YouTube... fantastic modeling by one of the hobby's best modeler! His method of doing trees made it possible for me to create dead, dried up trees also, which adds to the realism of the scene.





Well, there it is! I don't know if this qualifies as progress, but for me this was the most fun I have had since I started this layout 7 years ago. Looking ahead, I can see many hours of fun ahead as all the things left to do now are just the things I enjoy the most. To add to this euphoria, I started doing some "Shake down" operations. This brought to the surface all the small things that I need to revise on my handlaid track. Remember when I said I discovered things in my other hobby that improved my model railroad? One of the things that would happen often in operation sessions is switch points that would break from the throw rod. This happened because I soldered the points directly to the rod. When the rod moved side-to-side, it would create tension in the points and would cause them to break loose.

While working on one of my small RC trucks, I found some very tiny screws that fit perfectly on my throw rod, so I soldered a brass ring to the points and used the screws as a pivot point... (Next page) My turnouts never moved more freely. Once painted and with a few added details, this should look good enough... Not anywhere close to the standard of my idols, but for operation purposes it gets the job done.





It just goes to show that taking a break and exploring other hobbies can bring more enjoyment and help you move forward a lot more than you might expect.

So, do I have endless resources? Not at all, but the good thing is that I am beginning to realize that perhaps I don't need as much as I thought. I just have to learn to stop focusing on the end goal and be happy with what is in front of me.

It is now time for me to take another break and discover new resources which I will put to good use on my layout next fall... See you then!



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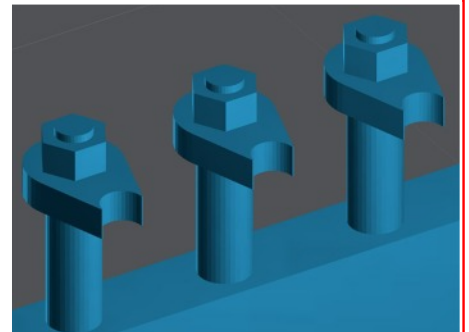
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By Jay Morris

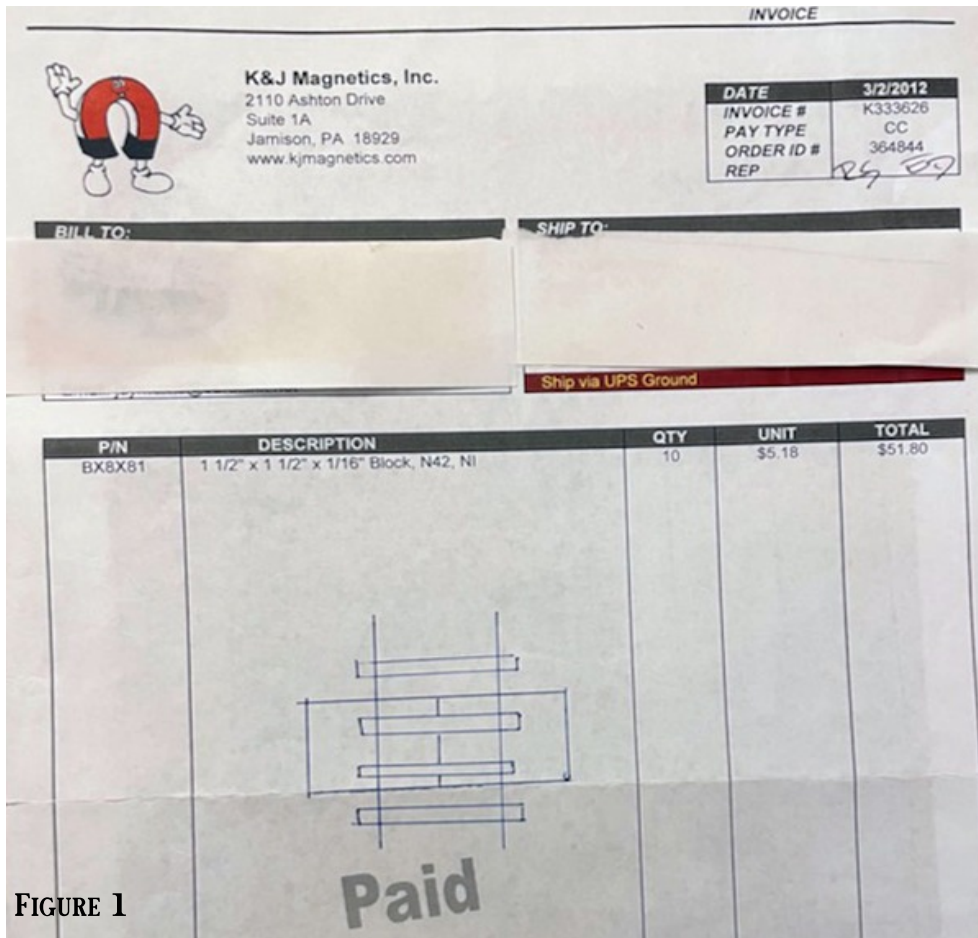


FIGURE 1

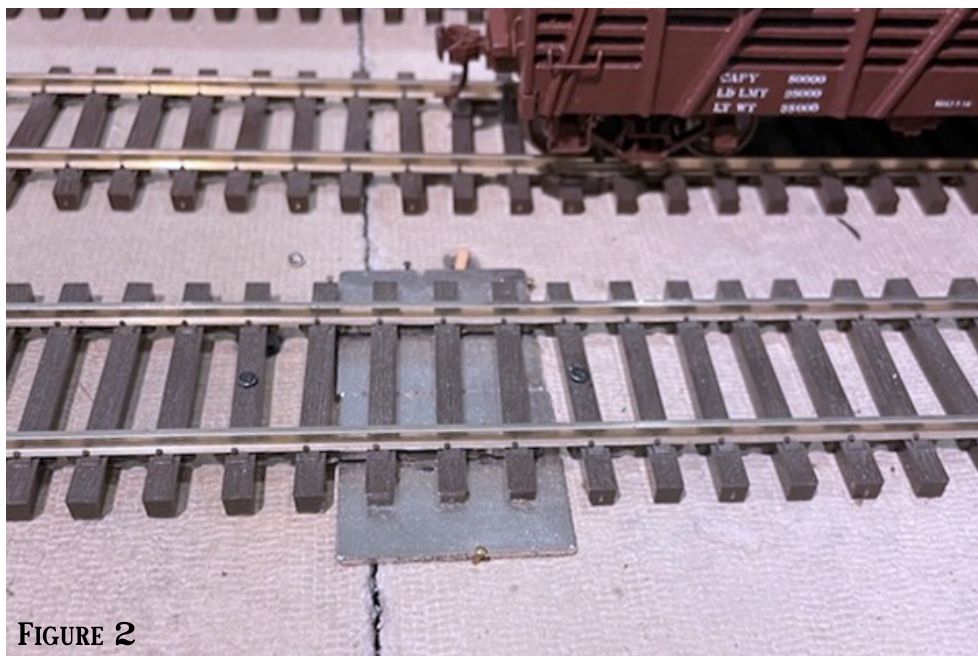


FIGURE 2

Kadee® makes an excellent product, and most O Scalers and three rail scalers consider it the best coupler on the market. Recently Atlas O, and before them K-Line, have produced copies of the Kadee® design as the base Kadee® patents have long expired. This article also applies to the most recent version of the Atlas scale couplers as well as the K-Line design. Nevertheless, for the Atlas coupler to work properly, the centering spring must be replaced with a Kadee® centering spring as these are softer and work better.

Kadee® offers both permanent magnet and electromagnet uncoupling mechanisms. The magnet offered by Kadee® is weak and generally needs to be placed above the ties for reliable uncoupling. The electromagnet has pseudo guard rails that project above the ties and are quite un-prototypical.

I have found a few ways to improve on these choices. When a permanent installation is acceptable such as on a siding or secondary track, I use the 1 1/2 x 1/2 x 1/16 block N42, NI magnets P/N: BX8X81 from K&J Magnetics, Inc. www.kjmagnetics.com. You will need two of these at each location and they are magnetically attached to each other side to side (Figure 1 – Note: prices may have changed since 2012). Be careful not to catch your fingers between magnets as these are very strong.



FIGURE 3

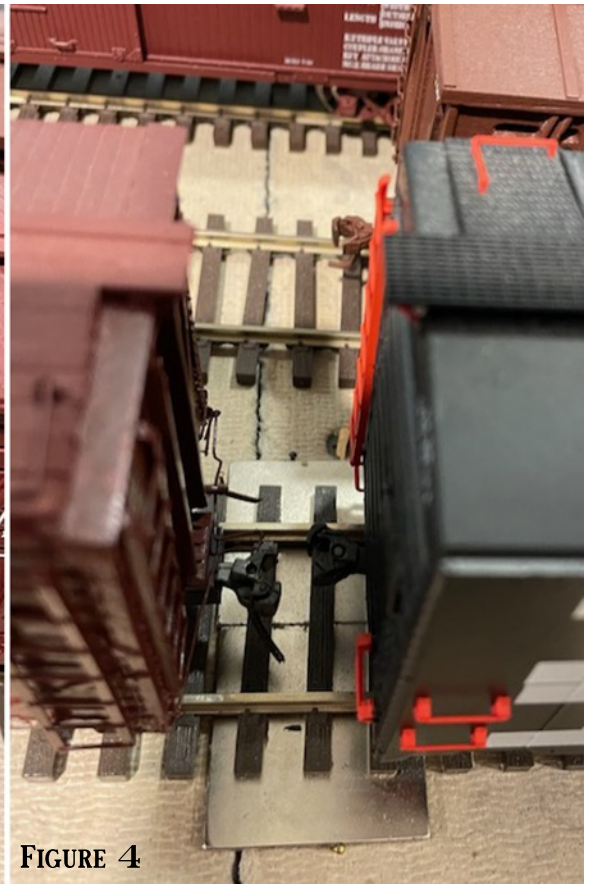


FIGURE 4

These super magnets are only 1/16 of an inch thick and either a slight depression in the roadbed or shaving 1/16" from the bottom of the ties allows for an installation that works perfectly and can be completely hidden by ballast. (Figure 2).

Remember to have a yard sign or similar so you remember where you placed them (Figures 3 & 4).

What about a run through track you ask? Too often, permanent magnets on run through tracks will lead to occasional unintentional uncouplings, always a pain when operating. To fix this problem, you will need a Fulgurex, PFM, Lemaco ACC-01 or similar switch machine intended for turnouts (these machines were offered by a number of companies and are readily available on the "bay" and elsewhere). See Figure 5.



FIGURE 5

Start by bending up a brass strip section about ¼" wide (or just less than a centimeter) or, if you have it, a soft steel section to fit in the moveable end section of the switch machine. The brass stock I used was narrowed to fit the arm of the switch machine. It needs to be just thick enough to be rigid and long enough to form the section shown in the pictures. Drill, tap and fasten with (two) short screws that don't interfere with mechanism movement. The above mentioned magnets can be fastened in position by magnetic force (such as a steel washer

sandwich) as they are very strong. Don't try to drill the ceramic magnets as they are very brittle and will fracture, magnetism is all you need as you will be creating a mechanism guide. See Figure 6.

Refer now to figures 7 & 8, cut out a magnet size rectangle in your underlayment (such as in Homasote). Drill a suitable hole for the moving shaft to clear and reassemble the magnet, arm and holding steel as was shown previously. Adjust turnout motor or arm height so that when extended the magnet is just barely snug

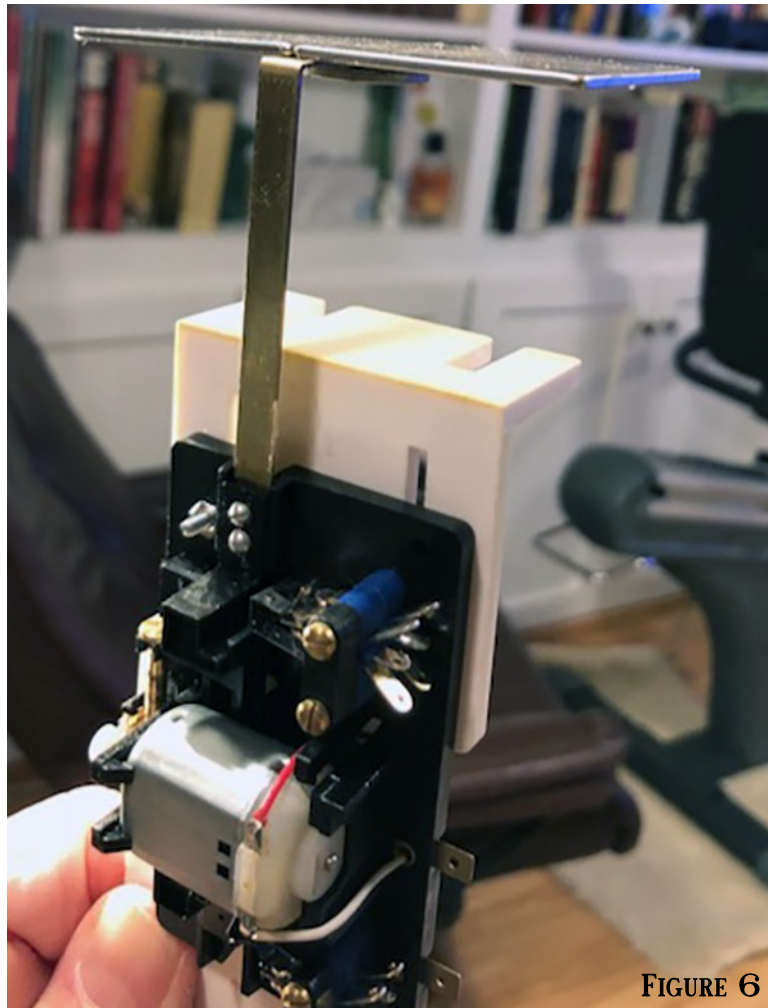


FIGURE 6

against the bottom of the ties when track is placed above. The switch machine is mounted with a Rix Rax 628 under the layout in a typical manner (Figure 9). I used a Rix Rax mount made for these type of machines as well as twin solenoid type, but a simple block of wood would do as long as you can address the height above the board. The advantage of using the Rix Rax is that the slots allow you to easily position the height to match the top of your layout under the track.

When the turnout motor is activated, it lowers the magnet about equal to the height of your underlay, say 1/4". Given that magnetism obeys the inverse square law, when the magnet is lowered even a short distance below the track, it will have no effect on passing trains. When the magnet is raised, it works perfectly and even allows for Kadee® delayed uncoupling.

A thin piece of paper can be placed or glued over the finished and tested mechanism to prevent ballast from jamming it at a later point in your scenic work. (Figure 10)

Regarding electrical hook-up, I recommend a standard DPDT switch to reverse the power to the switch machine motor.

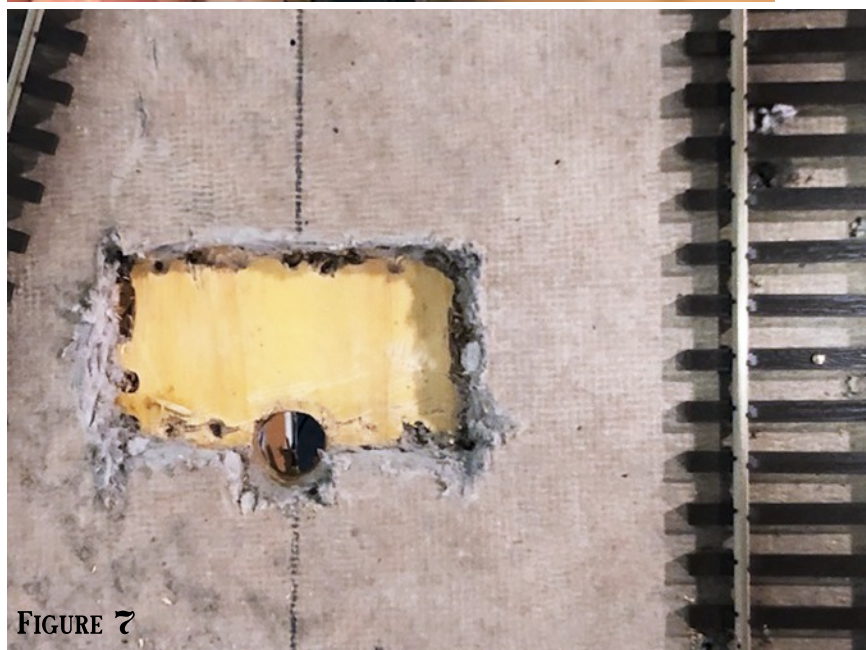


FIGURE 7



FIGURE 8

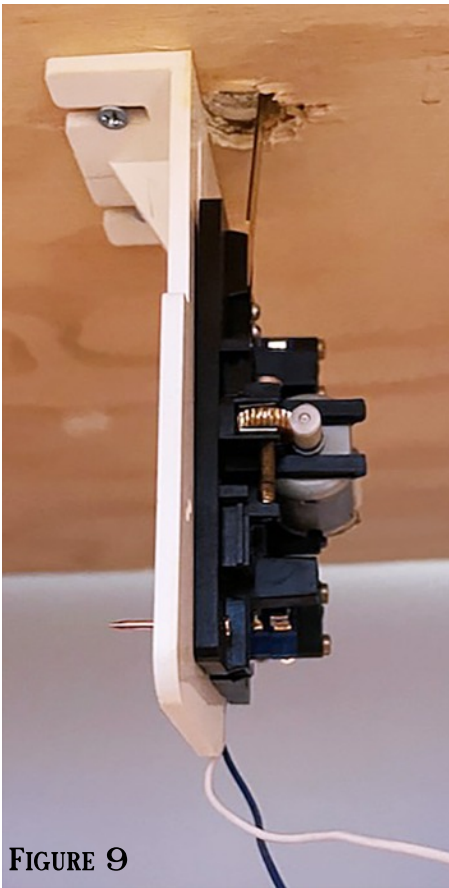


FIGURE 9



FIGURE 10

It should be wired the same as in the switch machine instructions for operating a turnout with same. On my panel, flicking the switch up raises the magnet and flipping it down lowers the magnet – intuitive.

I hope you all find this helpful and maybe even someone from Kadee® might read to upgrade their magnetic uncoupling offering.



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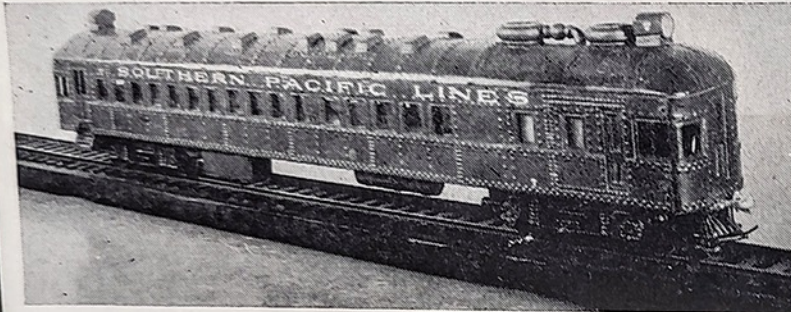
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By Carey Williams

Model Craft Brill Doodle Bug



ATTENTION! O GAUGE FANS! This new all-metal remote control reverse, **Brill gas-electric power car, \$32.50** complete, ready to run, painted and lettered, only.... Complete machined construction set with finished power and trailing truck, body completely assembled, and all parts finished to complete job, priced at only \$25. No special tools required to assemble this kit, a screw driver being the only tool necessary. There are

positively no wood or cardboard parts used. Car will haul four standard passenger cars. For two-rail operation, 95c extra.

SPECIAL for the month of September: Our new O gauge Duraloy rail. Positively rust-proof and the easiest-worked rail on the market. In 4 ft. lengths, at \$2.95 a hundred, postpaid anywhere in the U. S.

SPECIAL for HO Fans: 6 sets assorted car sides for only.. **\$1.00**

Now Ready for Delivery: The finest high-pressure industrial die castings for O gauge ever put on the American market. Complete set of samples will be forwarded for 50c. Watch this space next month for the finest line of O gauge box cars ever presented to the public. These are made to our moving picture studio precision and are priced right.

MODEL CRAFT MFG. CO.
3459 W. Sixth St. Los Angeles, Calif.

1938 Sep MR



Model Craft of Los Angeles took out a number of small ads in *Model Railroader* in 1937-38. Each monthly ad featured a different item from an HO switcher & Atlantic to O scale Switcher and Doodle Bug.

Their ads mention that the pieces are done to motion picture precision which is probably a hint to what Model Craft really did to make money, providing models for the motion picture industry and museum displays.

From the rarity of the Model Craft items, it's probably a safe bet to say they only dabbled in the hobby for a short time as the company was in business up until 1987.

The Brill Doodle Bug is a cast aluminum body with an early Walther's type power truck powered by a K&D #1 motor. Supplied RTR \$32.50 or kit from \$20.00.

When Model Craft closed, a small box of remaining model train parts remained from 50 years earlier showing some samples of their nice brass components and cast aluminum switcher.

Please see photo for some of the remaining parts.

The Model Craft catalog has quite a wide variety of items offered from HO, 00 and O scale. Some of the items in the catalog appear to be from other manufacturers and were just being sold by the company, whereas other pieces were made in house.

I'm sure in time, more pieces from the company will surface, but as of now not much remains.



[Please click above for video to watch the Brill doodlebug running https://youtu.be/GDbrRabw3QQ](https://youtu.be/GDbrRabw3QQ)

Thank you very much for reading the article. Daniel suggested there might be an interest in covering some of the vintage O scale pieces.

If you, the reading audience, approve will try to do an article each issue covering one piece to help add a little history behind O scale 1926-1954.



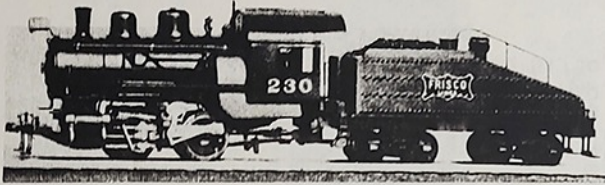
When doing research on these pieces, new information always seems to come available after an article has been penned ... All information is as accurate as found to date ... If you have additional information or corrections, please let me know.

Cheers, Carey Williams

wasp3245@aol.com



SWITCH ENGINE



NO. 0 SL-1

SUPER DETAILED 040 USRA SWITCH ENGINE WITH PARTS CAST IN BEAUFUL ART BRONZE, VERY SMOOTH AND DURABLE.

COMPLETE CASTING KIT WITH MOTOR AND FINISHED GEARS, FINISHED TRUCK WHEELS, AND AXLES, BELL, WHISTLES, BRAKE CYLINDERS, ETC. \$ 25.00

NO. 0 SL-2

MACHINED, HOBBY KIT WITH ALL IMPORTANT HOLES DRILLED AND TAPPED, WITH SIDE AND MAIN RODS AND FINISHED VALVE, GEAR, NICKLE SILVERED, READY TO ASSEMBLE \$ 38.50

NO. 0 SL-3

ASSEMBLY KIT WITH CHASSIS, COMPLETED, READY TO RUN \$ 41.00

NO. 0 SL-4

COMPLETED LOCOMOTIVE, PAINTED AND LETTERED FOR YOUR FAVORITE ROAD \$ 65.00

ABOVE KITS POWERED BY K & D MOTOR



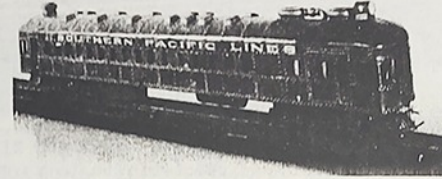
0 GAUGE PASSENGER JBI BIG 4 HUDSON LOCOMOTIVE

COMPLETELY MACHINED HOBBY KIT READY TO ASSEMBLE WITH SUCH TOOLS AS SCREW DRIVER, PAIR PLIERS, SMALL FILE. THIS CAN BE ASSEMBLED BY THE AVERAGE AMATEUR. DRIVERS ARE SPRUNG, TRIPLE CUT WORM, WITH BALL BEARING THRUSTS. COMPLETELY ENCLOSED GEAR HOUSING, SIDE RODS ARE SPLIT TO OFFER PERFECT MOTION. COMPLETE KIT AT ONLY \$ 47.50

SAME KIT FOR 2 RAIL OPERATION ADD \$ 6.50

SECTION I	\$ 11.00	SECTION IV	\$ 8.00
II	9.00	V	11.00
III	6.00	VI	5.00

0 GAUGE BRILL GAS ELECTRIC POWER CAR



NO. 0 BL-1

COMPLETE CASTING KIT CONTAINING ALL PARTS FOR BODY, MUFFLERS AND HEADLIGHTS, WITH SIDE FRAMES FOR TRUCKS, AND BOLSTERS, BUT NO POWER TRUCK \$ 10.00

NO. 0 BL-2

COMPLETE MACHINED KIT WITH ALL HOLES DRILLED AND TAPPED, INCLUDING FINISHED MOTOR TRUCK, ETC. \$ 20.00

NO. 0 BL-3

ASSEMBLY KIT WITH ALL HOLES DRILLED AND TAPPED WITH COMPLETELY FINISHED POWER TRUCK AND TRAILING TRUCK, READY TO RUN \$ 22.00

COMPLETELY FINISHED CAR, PAINTED AND LETTERED READY TO RUN \$ 32.00

BODY CONSTRUCTION OF LOCOMOTIVE CAST IN FINEST GRAIN ALUMINUM. TRUCKS AND RUNNING GEAR IN THE FINEST ART BRONZE.

MOTOR TRUCK POWERED BY K & D MOTOR, WORM AND DIRECT GEAR TO ALL 4 WHEELS, QUIET OPERATION.

ADD \$1.00 EXTRA FOR 2 RAIL OPERATION.

MOTOR TRUCK READY TO RUN \$ 9.00

CAN BE FITTED WITH VARIOUS SIDE FRAMES STATE TYPE REQUIRED.

Tell Your Friends About

The O Scale Resource Magazine

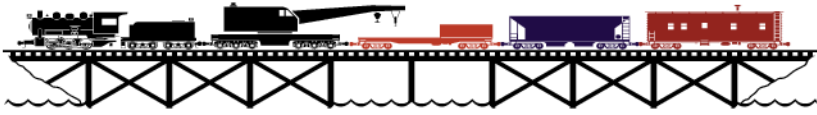
Help get the word out about the

most widely read

O scale magazine



NEW TRACKS MODELING



By Contributing Editor [Jim Kellow MMR](#)

MENTOR DEFINITION: A TRUSTED COUNSELOR OR GUIDE

**MODELING WITH MENTORING FROM
TALENTED MODEL BUILDERS AND EVEN
FROM MODELERS IN OTHER HOBBIES**

“LETS REMEMBER”

A Memorial page is now on our website to recognize model railroaders no longer with us.

We are adding a page on our website to recognize those model railroaders who are no longer with us. If you have a friend or member of a club or organization you belong to and would like to see them added to our page please send their name to [Steve Sherrill](mailto:Steve.Sherrill@newtracksmodeling.com) at Steve.Sherrill@newtracksmodeling.com. Steve will make sure they are added and recognized. Thank you for your help.

Now for some news:

Bad News: Campbell Scale Models is gone! Gosh, I hated to hear this. I have built their great kits over many years. We can only hope someone comes along and brings the products back to life. This was posted on their website.

“Campbell Scale Models has closed down. Thank you for visiting Campbell Scale Models Online!

A big THANK YOU to all our dedicated customers, some of you have been with us for over 60 years, but everything must come to an end. As those actively involved in the railroad hobby become older and older, it becomes harder for any RR product manufacturer to survive.”

Some good News: Frank Deuter told me that the Corddry Company Crossing Model Railroad Club is having their 3rd summer camp for young, 8-16 year old kids June 27-29, 2023. Sounds great to me. Never heard about this kind of club project before. [Click here to view](#).

Is your club doing anything like this? Please let me know.

More Good News: [What a great club at Penn State.](#)

When I saw this, I had heard that the University of Illinois-Urbana and MIT have model railroad clubs and I understand Rensselaer Polytechnic Institute in Troy NY used to, but I did not know about Penn State's club. I wonder if any other schools have such clubs? If so which ones? I found this information at the MIT site:

“Other College Model Railroad Clubs”

We believe this is a complete list of all collegiate model railroad clubs in the US. Please contact us if you know of an active club at a college or university that is not listed here, or can otherwise help us correct the list.

- Rensselaer Model Railroad Society, Rensselaer Polytechnic Institute
- RIT Model Railroad Club, Rochester Institute of Technology
- Penn State Model Railroad Club, The Pennsylvania State University
- Society of Model Engineers, Milwaukee School of Engineering
- Purdue Railroad Club, Purdue University
- Illini Railroad Club, University of Illinois at Urbana-Champaign
- Rose-Hulman Model Railroad Club, Rose-Hulman Institute of Technology
- The Railroad Club at the University of Minnesota, University of Minnesota
- Illinois Tech Model Railroad, Illinois Institute of Technology
- Iowa State Railroad Club, Iowa State University, actually a railfan club rather than a model railroad club
- NSCC Model Railroad Club, North Seattle Community College

Fallen flags: as far as we can tell, the clubs listed below no longer exist.

- Carnegie Mellon Railroad Club, Carnegie Mellon University
- WPI Model Railroad Club, Worcester Polytechnic Institute

I wonder what makes these schools have model railroad clubs? What do the students going to these schools know about having model railroading in their lives, that students at other schools don't know? Or is it the faculty and administration spearheading this?

Does it have something to do with the STEAM program emphasis or influence at these schools. I am going to try and get representatives of some of these schools, and any others I can find, on our Zoom Show. Any comments, suggestions or help in contacting representatives of these clubs from readers are appreciated. My email is: jimkellow@newtracksmodeling.com

Some Great News: Dennis Brennan owner of Brennan's Model Railroading will begin two special monthly show segments this summer.

On the July 12 show, he will begin a series on “Model Railroad Photography “ and on August 9, begin a monthly series on “Model Railroad Modeling is Art Sculpture”.

Dennis is a great modeler and model railroad manufacturer and is well credentialed to discuss both of these subjects. Dennis is the only model railroader, I know of, who has had his own show for his photographs. He told me: “As I am thinking about creating model railroad art, I am not consciously thinking about creating art.

Yes, I graduated from Rutgers with a degree in art with a concentration on filmmaking and photography. However, in my Intro to Art course we had assignments in painting, drawing, and sculpture as well as photography and filmmaking. My intro instructor became my advisor and he suggested I do sculpture. Although I didn't follow his advice, I now understand that he saw something in my work that I didn't. What I am presently doing when I create a diorama or a model is sculpture. It's innate. When I photograph a model railroad scene, there is the art of photography in capturing the scene that was created.”

I believe we will all learn a lot from Dennis and perhaps take a slightly difference attitude when viewing our “Modeling Art”.

New Tracks Starts a Monthly Newspaper Column In Local Paper.

In my last article, I told you I am doing everything I can think of to promote modeling and mentoring. I even showed you the newspaper article that was written about me. Guess what? Because of that article, the local Chronicle newspaper asked me to do a monthly article on model railroading! My two articles appeared on April 5 and May 3.



Now because of these articles, I was asked to join a digital business group called **Alignable**. As a result of this, NEW TRACKS MODELING was designated as Highly Recommended to other businesses.

Writing these articles in a non traditional channel of communication, at least for me. It has shown me the value and reach, to a very broad and diverse audience, our local newspapers can provide NEW TRACKS and our hobby. I think we should work with them to promote our hobby every way we can. If you think your local newspaper is interested in publishing my articles, or in you writing articles for them, I hope you will pursue such opportunities. Please let me know what your local newspaper says.

I believe having New Tracks, or any hobby company, recommended by non model railroading private companies to their customers and viewers can only be considered a significant accomplishment and benefit. For example, this was posted by a local company that certainly has a great involvement with real railroading.

By: Bruce Nickerson from Sea Port Property Maintenance

"Jim and his crew operate a first class educational program weekly to further the knowledge and expertise of all miniature modelers. It is done in good taste. Each program has something for everyone. He is also allowing air time to some of the best modelers and small model builder companies to air their products and share their expertise with the those who have an interest in improving their skills. All in all it is a fine tribute to our industry."

Now for some NEW TRACKS regular programs

New Tracks Mentoring Scholarships: Today’s modelers made it happen, “Thank You!”

Our 2023 Scholarship.

We have successfully raised contributions to pay for the first year of our New Tracks Modeling Mentoring Scholarship program. The Scholarship program helps young model railroaders continue their post High School education in a Science, Technology, Engineering, Art, or Math (STEAM) program at an Institution of Higher Learning. Thanks everyone who contributed to make this happen. Thanks also to all the organizations and businesses who are supporting our program.

Last issue, I thanked Atlas for their support and now I want to thank the NMRA for their Facebook posts of support for New Tracks and our scholarship program. This Appeared on Facebook from the NMRA.

NMRA - National Model Railroad Association, February 14 at 10:21 AM

Jim Kellow MMR and NMRA member, is looking for students interested in applying for a scholarship sponsored by his New Tracks Modeling shows. If you know someone, please share this with them. For more information on Jim's weekly shows or the scholarship, please use the QR code or visit <https://newtracksmodeling.com/scholarship>. #NMRAExperience #NMRAFamily

In addition, the other day I got the GSMTS, Timonium Show, Report from Scott Geare, that he sends to all the show participants. Scott included the following comments about our Scholarship Program. Thank you Scott for your help and interest.

“I write to all our exhibitors, whether or not you were present at our February 4th and 5th Great Scale Model Train Show (aka “The Timonium Show”).

“JIM KELLOW and the STEAM SCHOLARSHIP: You know what “STEM” stands for, don’t you? Of course you do: Science, Technology, Engineering and Math. That’s the current emphasis of the national educational agenda. But what does “STEAM” stand for? Same thing, with the addition of “ART.”

“Jim Kellow, himself a credentialed Master Modeler, has been working to establish a scholarship program for youth who are involved with serious modeling. Thus, he and his associates have established the “STEAM” scholarship program, which adds “ART” to the STEM program, making it “STEAM.” And indeed, what is model railroading, if not art, even “fine art?” (As Howard Zane would say).

Learn more, here: <https://newtracksmodeling.com/scholarship/>

Are you the parent or grand-parent of a kid who needs some financial help with college? Or do you know such a youngster? If that youngster is involved in model railroading or any modeling art, connect him or her with Jim’s scholarship program. You may also want to contribute to the fund yourself. I did.

Visit this web page: <https://newtracksmodeling.com/scholarship>”

Thank you Scott Geare for including New Tracks in your show and also in your show’s report which goes to all your vendors. We look forward to being in your future Timonium shows.

I am also pleased that the National Retail Hobby Stores Association (NRHSA) included New Tracks and our scholarship in their Monthly Magazine “Hobby Merchandiser”. Having the support of this association and local hobby stores is very important in helping to get our message out to potential applicants and donors.



Banner and hand outs were at the O Scale National Convention in Denver this past June.

So now let’s make sure every potential model railroader applicant, knows about our program and, if needed, gets help in completing their application. The deadline for submitting an application is July 1, 2023. Specifically what can you do? Find out below.

What The New Tracks Team can do To Help Notify Applicants about our Scholarship

Bob Davidson donated three banners through his company, Exhibits And More, which will be shown at train shows and rail expos across the country. Each banner is

The O Scale Resource July/August 2023

3' wide and about 7' tall and has a QR code on it to link you directly to our website to get more information including the application to apply.

It's a retractable unit that is very easy to ship and set-up. So if you want one for your event, let Bob Davidson know at: bobdavidson@newtracksmodeling.com.

Look for New Tracks at any show you attend. We hope to be represented at as many shows as possible. If you want to take one of our banners, or help man a table, or let us be a part of your table, let Bob Davidson know at: bobdavidson@newtracksmodeling.com.

Remember, one of these years, your younger kids or grandkids may benefit from the New Tracks Modeling Mentoring Scholarship program that you helped start this year. I truly hope so. Thank you.

Thank you to everyone who helped us display the banners at shows around the Country. We are currently taking reservations for our banners for our second year's (2023-2024 Scholarship Program).

New Tracks 2024 Scholarship Program

Our Scholarship Team has set high expectations for 2024, and have goals to offer a significant number of high value scholarships. To be successful, we will need all your continued support and help.

New Tracks is the only organization we are aware of, comprised solely of volunteers from the model railroad community who have created a nonprofit company whose purpose is to provide scholarships throughout the United States for the benefit of young modelers who are pursuing their STEAM education after High School graduation, in a college, university or accredited technical school.

We are in the process of finalizing our donation process, and we hope you will consider making a tax deductible donation to this very worthwhile scholarship effort. The contributions by current model railroaders can help the next generation of modelers continue the traditions of our great hobby. For more information, including the Officers and Board of Directors for the Florida Nonprofit Company which operates our NEW TRACKS Scholarships, please visit our website: newtracksmodeling.com/scholarship.

Don't miss the New Tracks Build Along at the 2023 NMRA National Convention. It will be live in person by a team member.



New Tracks is honored to have been asked by the NMRA 2023 National Convention Clinic Chairman to host a Build Along with Phil Edholm at the upcoming NMRA National Convention in Dallas, Texas on August 21-26, 2023.

The Clinic will be on Thursday evening of the convention and last approximately three hours. Alan Rogers is the manufacturer of New Creations Model Railroad Building LLC, whose Handcar shed kit will be built. The kit is available in N, HO, S, and O scale at a significant discount to modelers attending the convention who "Build Along" with the New Tracks Mentor Phil Edholm. Phil will build the structure and offer help and advice to the modelers participating in the "Build Along".

We look forward to working with the Long Beach California Convention Clinic Committee and offering

other New Tracks clinics at their convention in 2024. If anyone is interested in being a part of our Clinics, please let me know.

A new segment starting July 19, 2023, on New Tracks hosted by Steve Sherrill is called: “What do you want to talk about?”

Several modelers have suggested we have a segment on our weekly Zoom show that is about any modeling or model railroad subject anyone wants to discuss. Sounds like a good idea that we have not tried before so we are going to see where this goes.

If viewers like this idea, as we think they will, it opens New Tracks to model railroading subjects we have not previously explored. That is great. The more we can respond to viewer’s questions and issues the better. Tell us: “What do you want to talk about?”

It’s Official: New Tracks Presentations can earn you NMRA Achievement Program Points for either the Author or Volunteer Certificates.

I am pleased to announce that New Tracks presentations including BUILD ALONG, WATCH ME BUILD, and other modeling presentations are eligible to be counted as a regional clinic toward earning either the NMRA Author Certification, or Volunteer Certificate points. I want to thank Pat Rivard for bringing this issue to my attention and to Frank Koch, the NMRA National AP Chairman for his help in outlining what the presentations need to include to be awarded points.

Frank explained eligibility: “If the presentation meets the requirements as a clinic - 30 minutes, instructional or educational and not just a blog or discussion, and has a reference handout that is available online to anyone, the fact that all our presentations are recorded and available on our YouTube channel meets this requirement, then it can be counted as a regional clinic and earns 4 points toward Author. Since it is an electronic presentation, it is covered by the 50% of total points for that type of “publication”. If it is a clinic that has been presented before in any format, then subsequent presentations earn Volunteer points, but no additional author points.”

Frank then outlined this exception: “The only caveat is that if it is just basic kit building, then "no" it won't count, as there is no original planning or work involved. There are many simple blog videos on "how I built and finished this kit". They would not count. If it were more than that – tips that one might not consider, how to learn a new weathering technique (more than "apply and wipe white paint off bring to simulate mortar"), kitbashing is good, etc. It has to be more than a basic narrative of 1,2,3,4, etc.”. Since I believe “New Tracks” presentations go far beyond a simple 1,2,3, etc. basic build narrative, I am not concerned about our presentations meeting this exception.

I have appointed Pat Rivard, our AP Chairman, to implement this “New Tracks” NMRA Achievement Eligibility Program for our show. Thanks again Pat for your help. Pat can be reached at Patrick.Rivard@newtracksmodeling.com

Therefore, if you have made in the past, or want to make a presentation on one of our shows in the future, and want it to be given NMRA Achievement Program Author or Volunteer point credits, please let Pat know and he will provide you the documentation you need for the NMRA award.

Now, please meet some very talented modelers who may be able to help improve your modeling.

I met Ron Maxfield when he won one of the Manufacturers Contest Drawings I wrote about in one of my previous articles. I was very impressed with his scratchbuilding projects and know you will be.



Ron Maxfield

I began the hobby about 40 years ago. I had always enjoyed history and model kit building in my youth, and saw that a lot of the very picturesque structures of my hometown were being destroyed and I thought I might try my hand at recreating them in miniature.

I had no experience at scratch building. Just "seat of the pants." A lot of trial and error in the beginning. I first had to decide what scale to build. I thought about HO Scale at first because that was such a large resource of material. But I thought it too small to capture the detail I was looking for. I next thought about G Scale due to the great visibility of detail, but I decided it was not practical in the long term for displaying an entire city.

O scale seemed the most logical with its good visibility and practical display space requirements. I began searching through O scale publications and hobby shops for the materials and resources I needed.

I discovered another plus for O scale is that it was also a crossover for many Doll House materials. I have found very good O scale suppliers over the years and have had help outsourcing some pieces and casting my templates with renowned model builder, Wayne Wesolowski.

My first miniature was of the Old City Jail. All my traction models were custom built using photos and factory plans by the late, Jean A Deschenes. All of the other photos I took myself. I have learned a few tricks over the years in sculpting decorative pieces and scaling buildings using photographs.

Thank you Ron for sharing your photos and obvious scratchbuilding skills. Ron can be reached at:

Ron.Maxfield@newtracksmodeling.com.



I am sure everyone has heard of Bar Mills. Please meet Art Fahie, owner and one of the nicest gentleman, and greatest supporters of our hobby I have every met. He will go out of his way to help all modelers.

Art Fahie

I can remember my Lionel trains circling our tinsel-laden tree back in the early 50's... I was born in 1947, and Christmas was a time for me to lay on the floor & stuff my trains with as many toy soldiers as they could handle....

My first real layout was in “HO”, it was called the “Tuxedo Junction Northern” named after the Glenn Miller tune. The layout was 5’x10’ and squeezed into an area that would only allow me to reach the first foot or so of the layout without crawling on top of it. I remember, I had a double dog bone and my favorite thing was my double crossover. It looked so cool I had to have it. Of course it was never motorized. Plasticville was still pretty much the thing, mostly left over from my Lionel years although I started building a couple of Campbell kits. Those were THE thing back then. After building two kits in about three hours, I decided they were just crap. Of course I had no real tools, never had anyone to teach me. I was too young to drive and get to the local model railroad club, but I did love my trains. The experience sent me back on my way to building plastic models. Little did I know.

I used broken ceiling tiles to build up terrain.

After leaving the hobby for years while starting college & my career, I re-discovered the hobby & reached out & met both Mike Tylick & Earl Smallshaw, both very well known modelers & MR authors. I’m still friends with Mike ‘til this day. Earl was a dear friend who help me set realistic goals. He died a few years back, we saw each other often, even traveling to model railroad shows together.

What I learned from Earl was that he never had to reach into his pocket to build his amazing “Middletown & Mystic Mines R.R” a combination of Yankee ingenuity (he lived in Connecticut) & authoring for magazines covered his actual cost of building his layout. Naturally, I started doing the same, authoring for N-Scale Magazine (I got back into the hobby in “N” to help a modular group) as well as *RMC*, *Narrow Gauge Gazette*. and an article or two in *MR*.



Northeastern Models in Massachusetts called me and asked me to be their Product Development guy. I learned tons there, especially how not to run a modeling business. They fired me. I probably deserved it, they weren't fond of customers calling to speak to me. I had been getting a name in the hobby, and it something they weren't prepared to deal with.

Two years later I found a need for a laser in a sign shop I owned, and started Bar Mills, then came the billboards and the rest is history.

At this point, I suggested to Art he offer a Contest drawing and he immediately agreed. The winner of the Contest Drawing will receive the kit, "Jerry's Small Engine Repair".

How to enter the Bar Mills Contest Drawing

**ENTER HERE TO WIN OUR
BAR MILLS DRAWING**

To enter the Bar Mills drawing, each modeler must complete the form [here](#).

The winner must agree to have their completed model featured in a future article highlighting their experience building the kit. Good luck - I look forward to seeing the finished model!



Campbell's is still the company I look to for inspiration. They did great things and seemed to treat folks well. I never knew there was such a thing as a limited-run kit back then.

There's a lot of ego involved in the craftsman kit business... both as a business owner and modeler. I don't get involved with other manufacturers. I like doing my own thing, but admit I'm very old school... it's all about answering the phones & trying to keep things as reasonably priced as possible, and that is getting harder by the minute.



Myself & my crew have given clinics all around the country. If a convention wants to raise money by featuring the Bar Mills crew, we won't do it as it's about sharing the hobby for us, not a money grab (although we will accept travel expenses, they can be crippling).

I know you asked for photos of what we do, but anyone with Internet access or a subscription to *Railroad Model Craftsman* can easily stumble upon our projects.... of course there was a time that manufacturers would actually charge to see their modeling videos. I found that offensive so we came out with our own for free.... it seems now to be the way things have gone.



As for minority scalers? My feeling is simple. Even though I build every Bar Mills kit in N/HO/S&O scales I personally model in "N" & "Sn2" (Very narrow gauge). Just because you're not in "HO" doesn't mean you don't want great quality structure kits, it just means your "not in HO" so we make it a point to service all scales. Although not much "S", both the N & O scale guys have been folks we've traditionally paid a lot of attention to.

Art, thanks for your help and interest. Art can be reached at Art.Fahie@newtracksmodeling.com.



I was introduced to this next modeler by Tom Yorke. That was a lucky day for me as I am sure you will feel the same. Please meet: **Bob Poole**

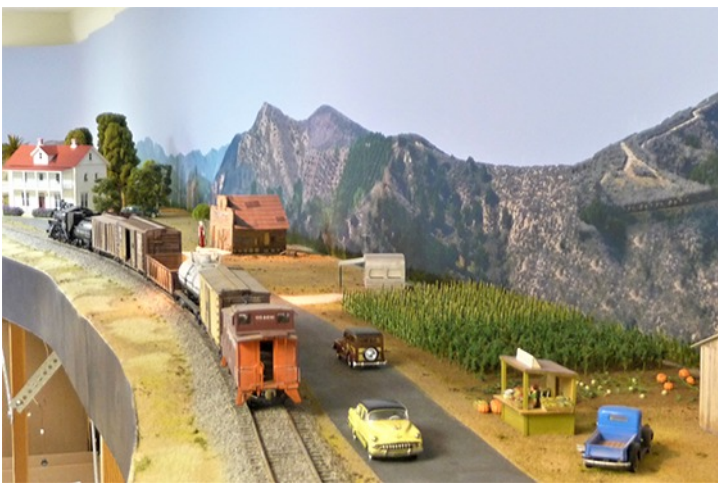
Here are a few shots of my layout. It was custom-designed by John Armstrong, one of the last layouts he designed prior to his death. The room is 30 by 50 ft. The layout depicts the Southern Pacific coast line route from Taylor Yard in Los Angeles to Santa Barbara, circa 1956 (the last year that steam locomotives operated on the SP, along with early diesels). Among the locations depicted

along the route are Glendale, Burbank, North Hollywood, Chatsworth, Santa Susana, Simi Valley, Moorpark, Camarillo, and Santa Barbara. I began construction in August 2004, and as of now the layout has buildings and scenery on 90% of the benchwork.

I began with a Marx wind-up train on a small plywood layout in the corner of my bedroom around age 8. For Christmas on my 10th birthday, I got my first Lionel train, which put me on the path to building an actual layout, with buildings, streets, operating accessories, etc. It was expanded several times and ended up extending along two walls in my small bedroom (and extending over the bottom bunk of a bunk bed which my parents bought so the layout could be extended (lower bunk) and I slept on the top bunk).



Taylor Yard icing platform



Simi Valley train



Santa Barbara depot

I subscribed to *Model Railroader* as a teenager, which is where I discovered the amazing work of John Allen, as featured in ads for Varney products. I decided that making truly realistic scenes would be an ongoing part of my model railroading. I also saw an ad for All-Nation Hobbies and bought their catalog. It was eye-opening to discover O scale and to see how much more realistic those models were (trucks, couplers, grab irons, etc.). I bought and built several All-Nation freight car kits, but outfitted them with couplers that would mate with Lionel so those cars could run in my trains.

Five years at MIT consumed my time and attention, and I told my folks they could sell whatever they could of my layout, saving only the Wabash GP-7 (that I'd bought with money I made from yard work and a small paper route) and my Dinky Toys automobiles. (I still have both of these, as memorabilia.)

Marriage and a new job, including a 2-car garage, primed me for the introduction of affordable ready-to-run O scale in the early 1970s, when the Atlas-Roco products hit the market. I bought several diesels and about a dozen freight cars, and with cousin Tom Yorke's help, built a plywood table-top layout in part of the garage. But an impending divorce led to all that being put into storage for 5 years, until I bought a new house and reassembled that 4' x 16' starter layout in the new house's 2-car garage. After re-marrying and moving from Santa Barbara to a larger house in Los Angeles, that layout was housed in one of the bedrooms. It was a switching layout, modeled after a plan in *Model Railroader*.

A later move to a still larger house led to that starter-layout being incorporated into an around-the-room layout in the house's former carport, expanded into a train room. I began acquiring brass locomotives



Glendale car dealer



Chatsworth depot



Santa Barbara SP roundhouse

and paid someone at Whistle Stop in Pasadena to paint and letter them. That layout was about $\frac{3}{4}$ completed (buildings and scenery) when we decided to move to South Florida. There was no realistic way to move the layout cross-country, so I dismantled it, packing up locos, scores of freight cars, structures, automobiles and trucks, etc., but having the garbage service take away all the modules (of various sizes) that had comprised the benchwork.

The house we bought near Ft. Lauderdale had a 30' x 50' garage that the owner had expanded from a 2-car garage. I saw that room and told Lou (my wife) "This is where I'll build my ultimate layout." Since we had about 2 years between buying the house and a major remodeling/reconstruction, I got in touch with John Armstrong, filled out his Givens & Druthers questionnaire, and sent him a dimensioned drawing of the train room. He designed my layout of the SP Coast Line route, LA to Santa Barbara, reflecting the changes in elevation along the way. It looked hugely complicated to build, and I built a small cardboard mockup in order to be sure I understood the main level, an upper-level place for an extra town, and lower-level storage/staging tracks. I laid out a grid of 3' x 3' squares, drawn in pencil on the floor, to correspond with John's plan. I started building benchwork in August 2004, with help here and there from Lou, laying track and wiring as I went. Given the constraints of a full-time job, with a lot of travel, it took 5 or 6 years to get to our golden-spike photo. By the way, due to some problems with poor track installation on the LA layout, for this layout I laid out easements for all the transitions between straight and curved trackage on the mainline and super-elevated all the curves.

Each year since then, I make up a plan for the coming year on which projects I hope to complete — usually getting about 75% of them done in that year. I started with Taylor Yard in Los Angeles, installing a turntable big enough to handle my cab-forward and scratch-building a 7-stall roundhouse. In the 1980s, I'd joined the SP Historical Society and have bought all their books related to the Coast Line route, plus books

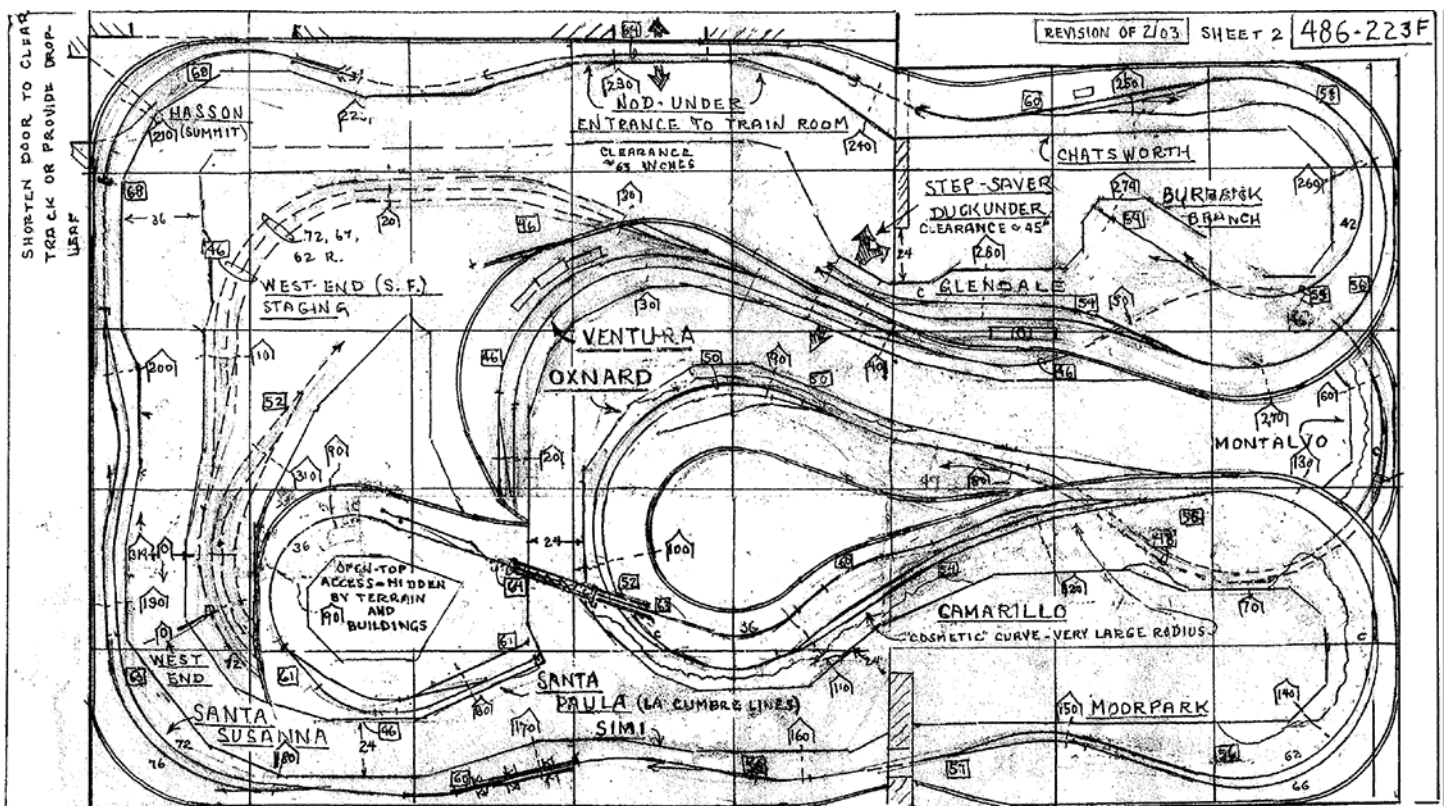
of plans, including the Taylor Yard roundhouse. By today, 2022, I have finished all the places on the layout except Oxnard (under way) and the invented town of La Cumbre, served by an invented short line that interchanges with SP near Moorpark.

South Florida is mostly an O scale desert, so I have been a solo layout builder and modeler. One inspiration besides John Allen's layout and modeling was the huge O scale layout of Lorell Joiner in San Antonio, which I visited twice during O scale conventions in that city. That layout made me appreciate what can be done in O



North Hollywood A&W

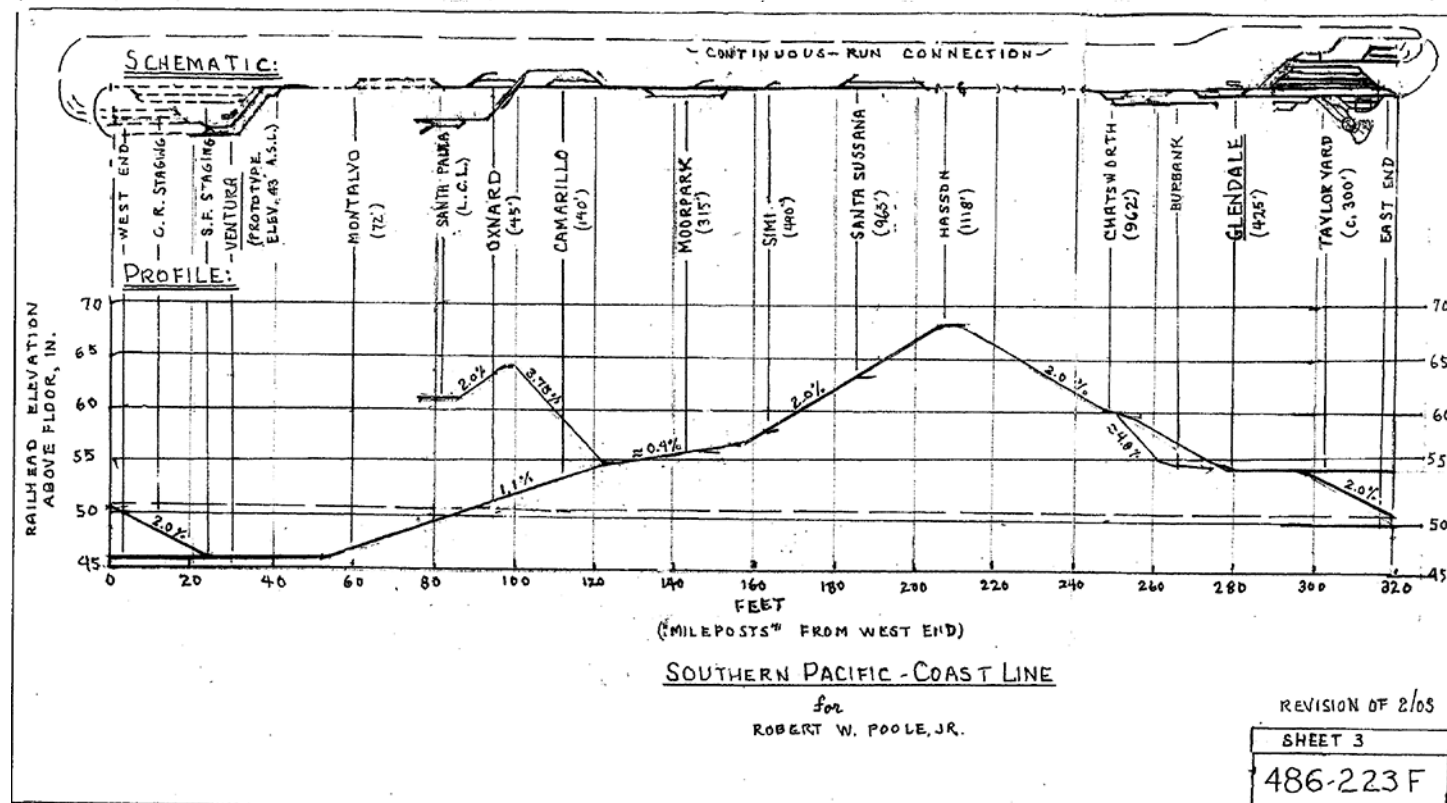
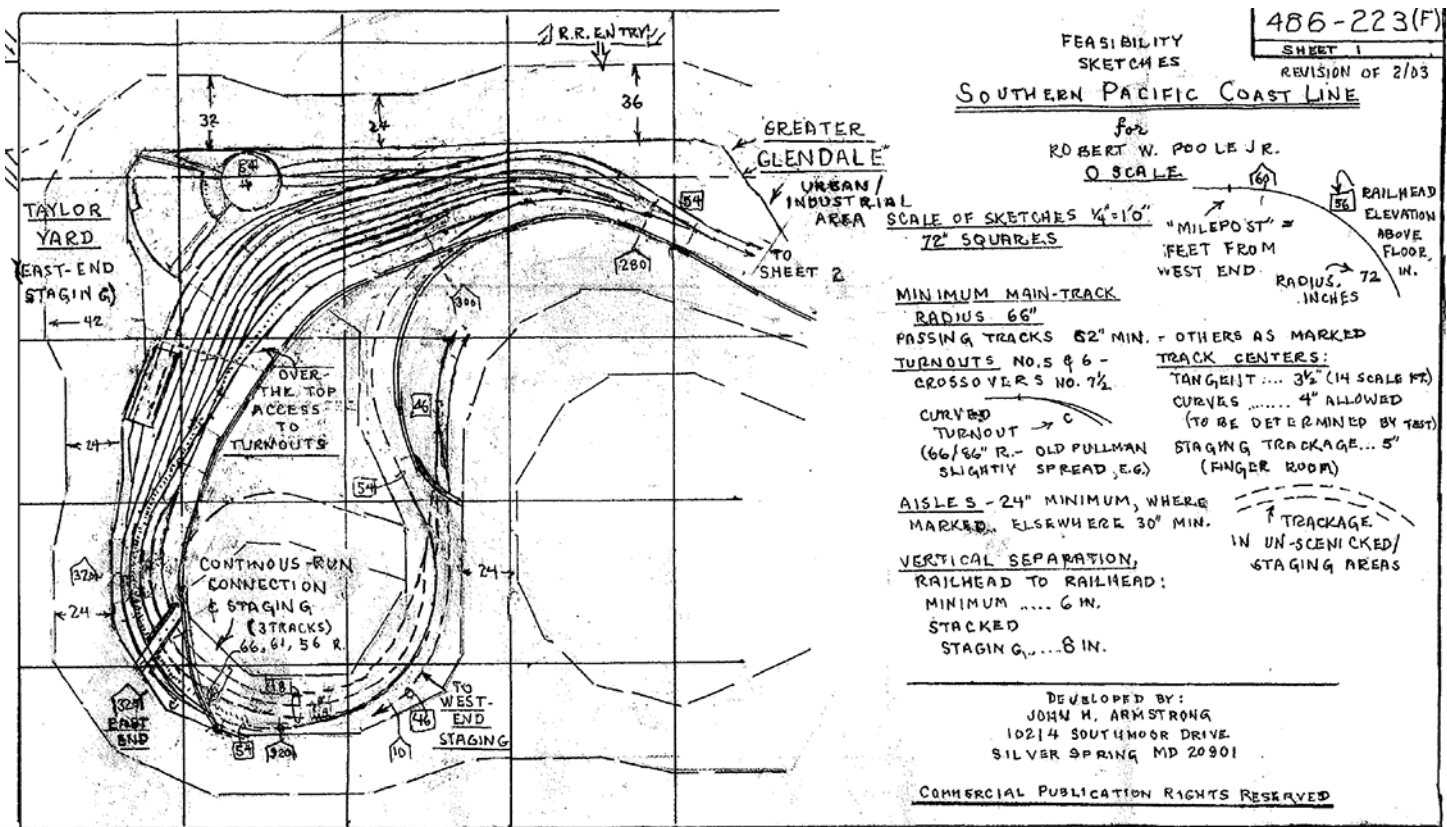
scale if you have a large enough space to work with. I've also learned a lot from layout tours during O Scale West and O Scale National Conventions.



Rod Miller, whom I got to know via O Scale West, became my loco doctor, upgrading several locos that I bought pre-owned at conventions and later on, equipping them with DCC and sound. This was nearly all done after we moved to Florida; would have been far easier when we both lived in California. So we kept UPS in business for more than a decade of shipping locos back and forth.

As I said on the program, Tom Yorke has been a real-go-to guy for me during all these years, from that starter layout in Santa Barbara (during a couple of years when he lived there) to longer-distance relationships by phone and then mostly by email. I was an early buyer and builder of his O scale kits, and most of them are still on my current layout.

I encouraged him to go into custom-building when his later kits faced a lot more competition and did not do too well. I continue to ask him about construction techniques and materials, painting and weathering, etc.



I still have a full-time job, which means whatever free time I have is devoted to continuing to build the layout. I'd be glad to answer questions from other modelers, but I'm not really in a position to be a mentor, as long as I am full-time employed.

Bob is also the Director of Transportation Policy at Reason Foundation, a think tank that he co-founded in 1978. Its HQ is in Los Angeles with a branch office in Washington, DC. Take a look at the Reason Foundation at: <https://reason.org/about-reason-foundation/>.

Therefore Bob's knowledge includes both model railroading and real railroading. I can't wait to discuss land based policy issues concerning real railroading with Bob. Down the road, today's real railroading will definitely impact our future modeling. I wonder how?

Thanks Bob for all your interest and help. If you want to talk with Bob you can reach him at: Bob.Poole@newtracksm modeling.com.

Now, please meet a modeler who does not let scale get in the way of his modeling – **Joel Bingham**.

I started with HO at age 3 in 1973. My grandfather built a card table layout for my brother and I, for Christmas. And we've had trains ever since. By age 6, my grandfather taught me to build a 4'x8' table layout.



I was born in Atlantic City, NJ and grew up in Ocean City, NJ on the Pennsylvania-Reading Seashore Lines. I watched PRSL RDC cars come in and out of town as I could see them from my porch. Model-wise I liked Reading, PRR, CNJ and PRSL.

My great uncle was a Reading Steam modeler. He made his own boilers out of copper pipe. And a Reading RR style cab out of copper that fit perfect. I inherited model trains from three great uncles, my grandfather and great grandfather.

I couldn't understand how my uncle could kitbash and scratch build steam locomotives.

By age 12, we moved to Santa Barbara, CA. I told my mom you have to take me to this place, Tehachapi CA. They have 50 trains a day there. As we got to Tehachapi, I said this is it, I want land here and own a steam train we could ride. I'm 52 now and I'm living the dream. I'm 5 miles from the tracks in Tehachapi. With a coal fired 970lb, 2-4-4T. 7.5" gauge. 3.75" scale.

In Jr. High in 1983, I started to model Southern Pacific in HO. I had to have an SP/SSW SD45T-2. I took an Athearn GP38-2 and SD40T-2 and chopped them to make a close SD45T-2. The SD39 was next....

Later, in 2003, I bought a hobby shop specializing in DCC and sound installs. It was at that point I switched to On30. My late friend Ajax Mart invited me to his house where he and Timmy Landreville built a huge On30 empire. Timmy passed away in 2010 and Ajax in 2019. Also at this time, Joe Hanacheck took me under his wing and showed me hand laying track. He told me, you're not a model railroader until you hand lay a switch, and it works. Joe hand cut 5500 ties





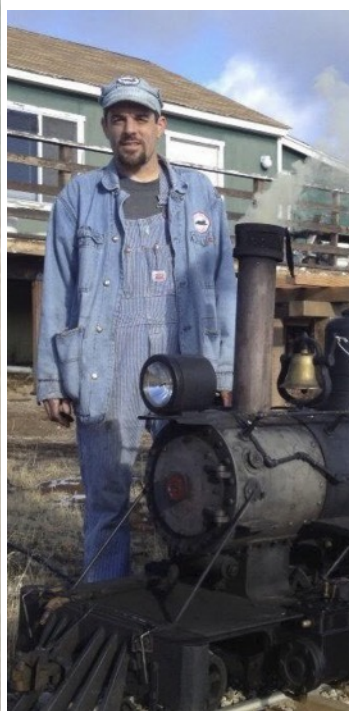
for my current 15'x25' around the walls layout. It's code 70 hand laid. I even did a 3-way stub switch.

I always have to alter any On30 train I get. I love to chop, cut and paste to make priceless gems or hot rods as I often call them. Joe Hanacheck also showed me *Narrow Gauge Gazette*. Here is where I started looking up to Gordon North, Alan Carroll, Allen K Littlefield, Paul Scoles and others. I was an HO rivet counter for 30 years. But On30 was different. I could model 2 foot stuff, 3 foot stuff or 30".

I was modeling the Maine two footer Bridgton & Saco River, then Southern Pacific Narrow Gauge which ran 130 miles from my home in Tehachapi. I've always made other engines that were the Quail Valley look. Quail Valley is outside Tehachapi up Sand Canyon. My live steam train is Quail Valley Railway. After Ajax passed, I inherited half of his trains. The other half went to Ajax's long time friend Dennis Shutt. It was at this point, I merged my Quail Valley into the Sespe Western, which was Ajax's railroad name. Sespe was a flag stop on the SP Santa Paula Branch.

I also ran the train at the Santa Barbara zoo for over 10000 rides. And put in 800 ties while there for two years.

Later, I worked at Fillmore & Western. Where we did film movie shoots with trains. I was the brake mechanic, brakeman, weed sprayer and track worker there.



Later, I used to fix private rail cars in Los Angeles, Saugus and Salinas, CA.

I used to ride the train to work, work on trains, then come home with my On30 and 7.5" gauge trains... trains, trains, trains. Friends called me train man.

Then I got a job machining parts at a trolley manufacturer in Chatsworth, CA. Great I only had to go 5 stops on the commute instead of 10 to LA.



My second train ride, we hit a truck and derailed. We left the rail at 55 mph. There were flying bodies and mass destruction as we stopped in about 3 train lengths. The engineer was a friend and he died a week later. He broke his neck and punctured his lung. I had to stop riding and working on trains. I couldn't look at my On30 trains. It's been 7 years and I'm still in a court case. Once Ajax passed, I had to force myself to do the On30. I started back in kitbashing and putting sound in all my engines. Since 2003, in On30 I've built over 100 locomotives, doodlebugs and critter.

I am here for anybody wanting to put sound and/or kitbash motive power. I also stretch and shorten cars in On30.

I've had more fun in 20 years of On30 than I ever did in HO.

My big train in my yard could be a whole program in itself. It's on 2.5 acres with 3% grades. I just chopped my 2-4-4T into an 0-4-0 and tender. And I'm going from 3.75" scale to 5" scale . Almost half size.

Thank you Joel for your help and interest. You can reach him at: Joel.Bingham@newtracksmodeling.com.

Anyone know how to use the BLENDER 3D Program to make people figures?

I received this email from Earl Hackett, a member of our New Tracks team. “Blender” is a 3D free form modeling program that has great potential for creating figures and other amorphous shapes, shapes that are not defined by numerical measurements. I have it on my computer and every so often I pull it up and kick the tires. I haven't been able to get it to do anything useful, but I've seen results that are incredible.

Thinking of getting a CAD Program to help your modeling?

I just got this from Neil Cooke, Onshape Director of Technical Services, EMEA and thought it would be beneficial to many of you. No question many of you are getting more involved with CAD and 3D printing and laser technology and maybe his comments will be of value and help to you. He stated:

“Whether you're considering switching CAD systems or buying one for the first time, choosing the right CAD system is a business-critical decision. (For hobbyists there is a free version of Onshape and other CAD programs).

[Click here for the 7 key criteria that you should consider when determining the best system for your team”.](#)

I get information like this from member companies of the APMM. If you found this information useful, I encourage you to go to the APMM website www.modelmakers.com and check out the membership options.

Next up on the Agenda: NewTracks Merchandise Program

New Tracks is responding to suggestions to start offering merchandise with its logo on it for for sale and to use as incentives for specific programs. This is the design we will be using. I believe it says what New Tracks is all about.



All of the money “New Tracks” earns will go toward paying for its operational and programing costs. But, as usual, we need volunteer help.

If you have experience or knowledge that can help make our Merchandise program successful please contact me at jimkellow@newtracksmodeling.com. We need volunteers to help us develop our new merchandise marketing program.

We want this merchandise program to meet with your approval, and hope we sell a bunch of stuff. Not only do we hope you buy it, we hope you wear and use it. In doing this, you become a visible member of our New Tracks team of mentors and supporters. You will be easily recognizable by other team members. Please help us take New Tracks to the next level of mentoring and modeling in model railroading. Let's make sure the next generation of modelers learn and use the modeling skills and techniques, and experience the emotional thrills only viewing a model you have built can provide. This is what has made the model railroading hobby as great as it is today. Thank you.

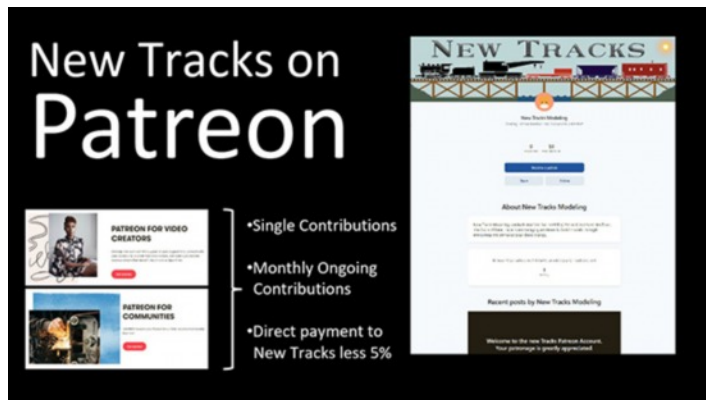
You, the New Tracks team of volunteers and donors are making NEW TRACKS happen. Thank You.

Thanks to all of you for the financial and volunteer support New Tracks is receiving for our shows and new Scholarship Program, it is obvious to me that “YOU”, the donors and volunteers who produce our shows, and others who do all the many jobs necessary to participate in our shows, create the website, YouTube posting and marketing, Zoom posts and other marketing, etc. are doing an outstanding job. I know this because of the favorable emails I get, the growth of our viewer community and the interest shown for our programs by the wider model railroading community. Thank you all.

I truly hope each of you are as proud as I am of your contributions and achievements that are creating New Tracks as the modeler and mentoring place to be for current and future model railroaders. This is what sets us apart: “Mentors Helping Modelers Build”. And the great part is we are only getting started in helping the younger modelers develop into the modelers and mentors of tomorrow. With your continued help, we have a lot more coming. Volunteer Help is now needed in a lot of areas (help wanted slide)

Help fund our New Tracks shows

As with any volunteer operated effort, there are always out of pocket costs that must be paid. These costs are not associated with our Scholarship Program, but cover the out of pocket costs of just getting the show to you, be it Zoom, YouTube, our website, etc. We have a [Patreon account, New Tracks Modeling](#), for your donations to help fund our shows. We hope you can make a financial contribution. Please click the Patreon link



on the left to donate to help pay for our NewTracks Shows.

Monetize our YouTube New Tracks Modeling Videos

We also have monetized our YouTube account, New Tracks Modeling. If you like the advertising placed on our videos, click and watch them so we can earn a little money from YouTube. Thank you in advance for your consideration and help. This is a easy way to help us fund our Zoom shows.

Volunteer your time to make our shows possible

We are fortunate to have as many fantastic volunteers as we do who donate their time to make our programs possible. But, as you can see, New Tracks is growing rapidly. As our show’s audience continues to grow and the types of segments viewers ask for increase, we are always in need of more volunteers. Specifically, we need help with our website to keep it up to date, help being a part of the production team who run our shows, and help with ideas and preparation of our advertising for our shows and fundraising efforts. Please help us keep New Tracks going strong.



As I write this, we specifically need volunteers to help keep our website up to date. Our website is the view NewTracks shows the world. It is our main source of advertising our shows, articles, and donations, so we need to make sure it has current and useful information for visitors. Our webmaster, Jim Alan, needs to expand our web team. Please contact Jim directly at webmaster@newtracksmodeling.com if you can help.

A second area we need help in is as back up for the two modelers who run the technical parts of our Zoom shows. Please contact Phil Edholm at philedholm@newtracksmodeling.com or Pat Rivard at Patrick.Rivard@newtracksmodeling.com to volunteer.

A third need is for digital marketing volunteers who can help us make sure we are using our Facebook, website, and YouTube channel to the best advantage to gain viewership. Brent Haeseker owner of [NetSource Technologies](#) sen me the following with some suggestions:

‘Hi Jim, thank you for your inquiry. Unfortunately, we can't do unpaid work. However, I did look at your YouTube channel, website and Facebook page. One suggestion I can give is more cross-promotion between

your platforms - link your YouTube and Facebook account to your website and vice versa. Also, optimization is needed on your YouTube videos. For example, make your video titles more descriptive instead of names like "Me" and add a keyword rich description and fill out all the other data fields YouTube offers. Video titles show up in Google searches if they match what the user is searching for - you'll get more views from search engines if the search engines have a better idea of what your content is about - they get a lot of this info from what you type in the title, description and data fields when uploading you video. Keywords to use are simply descriptions of what each video is about."

I think his ideas sound reasonable, but we need a volunteer who can help us implement them.

Lastly, we need an Accountant or CPA to join our Scholarship Program to help us establish our financial systems and procedures to account for and report our donations and expenses to the proper governmental agencies. Please contact me at jimkellow@newtracksmodeling.com if you can help.

Everyone who watches our shows has some contribution they can make. Any amount of time and any kind of help you can provide will be greatly appreciated and is definitely needed. Contact me at jimkellow@newtracksmodeling.com and let's discuss.

Special Upcoming New Tracks Live Events Wednesday Evening at 7pm EST

Subscribe to our website newtracksmodeling.com to get all the current information about our future shows and weekly emails with the log in links for our shows. Join us live on Zoom or on our YouTube channel, New Tracks Modeling. We have over 800 videos of our past shows on our YouTube channel for you to enjoy. They are all free!

NewTracks MY BUILD

Sponsored by Model Railroad Resource LLC

See the My Build section at the end of this article.

Join our weekly Zoom shows with our host, Chris Course. Our monthly New Tracks MY BUILD segments provide opportunities for viewers to have their work featured. You can show your latest project, discuss your modeling technique, or a new tool you found, or give a tip you learned that helped your modeling, or just show us what you have been up to.

Thanks to all the modelers who have participated and shared their modeling skills our with viewers.

You can see all the past MY BUILD models shown and the modelers comments on our YouTube channel New Tracks Modeling.

If you want to participate in our next monthly MY BUILD, send your model photos with a brief description and your name to Chris Coarse atrailrunner130@hotmail.com. The next MY BUILDs are scheduled for July 19, 2023 and Aug 23, 2023. This is the time to plan projects you want to share at these upcoming shows.



The O Scale Resource July/August 2023

Our Build Along modeling experiences provide you a personal mentor and great discounted prices on the models to be built. Join us on our Weekly Zoom shows for a great learning experience that will help you improve your modeling.



Wm. K. Walthers, Inc.

Starting September 6, 2023 Gary Shurgold MMR will build one of the Walthers Cornerstone Company House pkg(2) Kits. Gary will be building and finishing the model using modeling techniques that make the completed structure appear to be made out of wood and stone not plastic. Gary's work is absolutely amazing!

Plus, Walthers is offering New Tracks modelers who Build Along with Gary, a 30% discount on the kit starting August 1, 2023 and ending September 23, 2023. Please visit the [Walthers website](#) during this period to order the kit. You will need to enter the code NTRACKS at check out to get the discount.

A Walthers representative and Gary will appear on our July 26, 2023 show to discuss the Walthers Company, the kit, and to discuss any special tools, stains, paints, glues, etc, needed by a modeler to build the kit.

In addition, on the May 24, 2023 show, Walthers awarded one of the kits as a door prize to one lucky viewer, Alan Rogers. To win you had to be watching the live show on Zoom or our YouTube channel New Tracks Modeling. Congratulations Alan, we will look forward to seeing your completed models.

Also, every "New Tracks" viewer who builds the kit will be invited to join Gary on the "New Tracks" My Build segment on October 18, 2023. I hope everyone who builds this model using the techniques Gary demonstrated will show us their models.

I hope every modeler, regardless of scale or gauge, views Gary's demonstrated techniques on finishing plastic models. These techniques will work on all scales and gauges. Thank you Walthers and Gary for opening our eyes to, at least for me, a new modeling material to use, and new plastic kits to investigate.



Dylan Lambert Locomotive Works

Starting October 4, 2023, Bill Davis will build Lambert Locomotive Works new Commuter Coach shown in HO scale. More details concerning price, discount for New Tracks modelers and ordering dates will be provided soon on our website.

Rail Tales Hobby Shop

Bret Jones, owner of Rail Tales Hobby Shop, informed me of an illness in his family that has required him to postpone his Build Along until later this year. Well, that time has come on October 11, 2023. More details will follow shortly. We are so pleased that Bret's family illness is over and he can join us.

Brennan's Model Railroading

Starting November 1, 2023, Dennis Brennan will build his Electrical Substation kit, shown on the next page in O scale. There will be a 10% discount for NEW TRACKS modelers from October 1, 2023 to November 29, 2023. More details including ordering instructions will be provided soon on our website.

More Build Alongs to Come



I hope you want to participate in all the "Build Alongs". The modelers and manufacturers, who are making these events possible, are doing them to try to help you improve your skills and have more enjoyment and confidence in your modeling. They are a true learning experience that have helped many modelers. So if you have been sitting on the sidelines for awhile, give model building a try.

I am looking for more Modelers and Manufacturers to be involved in future "Build Alongs" in 2023. Contact me if you are interested at: jimkellow@newtracksmodeling.com

"Watch Me Build"

These segments provide modelers the opportunity to build a model to demonstrate their modeling skills and techniques. Viewers can ask

questions and learn from experienced modelers in various facets of our hobby.

Upcoming Segments:

- September 6, 2023, Pete Steinmetz, Glues to Use
- September 13 and September 20, 2023 Charlie Rausch MMR and John Sokash, Brass Modeling
- September 27, 2023, Pete Steinmetz, Paint Brushes to Use
- October 4, 2023, Jeff Jordan builds a structure in Styrene.
- October 11, 2023 Myles Marcovitch, scratching Victorian era structure using 3D printing, laser cutting technology and old school methods.



On the next page, take a look at his type of scratchbuilding structures and techniques he uses. Myles told me: "What follows is what's sitting in my queue. I lean towards buildings in the latter part of the 19th century.

Here's the "Early Sunday Morning" project showing the drawings I've made to get it going. I haven't done the detailed work to 3D print the details, but I have done the 2D drawings for the walls.

This is the Hopper painting. But first, here's the actual street scene that he used to inspire the painting. Hopper was not a photo-realist. He was a realistic impressionist (or something like that). Notice the window count, the mullions and the shutters. He changed them. Simplified the windows and added one. He also changed the nature of the store fronts and added another door that seems to lead to the apartments above. I am choosing to build it like

And here's my drawing. First a rendering and then the actual SketchUp file. Notice the detailing around the door to the apartments. This detail is barely visible in Hopper's painting, but it's there. Part is hidden under the folded awning.

The Hopper print to the side was superimposed over the drawing which is how I do these. I estimated an 84" door and used that to scale the entire drawing. There's a way to do this on SketchUp which is elegant and easy.

Here's what the 2D laser cutting drawings look like. I export flat views of each face and scale them in CorelDraw to do the detailed drawings including laying in the brick engraving pattern. The width of 13.5" is well within O scale ranges with the width a bit over 5". The bricks are much sharper in the actual Corel file. The screen print kills the resolution.

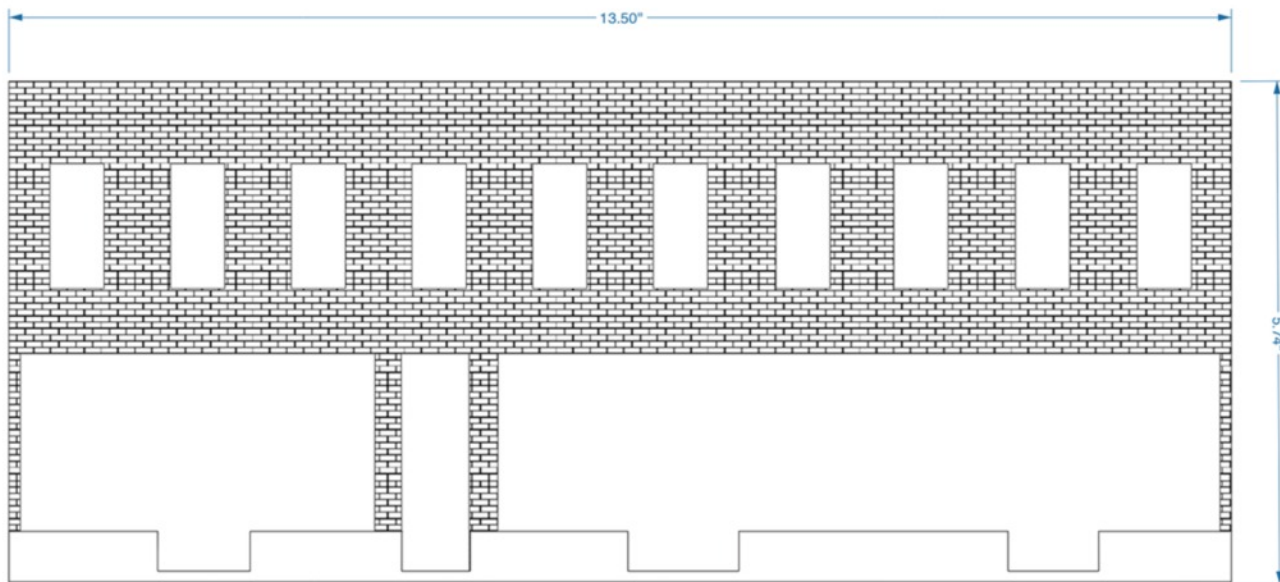


The movie theater (shown on the next page) is more complex brick-wise, but doesn't have all that store front detail. That's my wife, Michele, in the doorway for scale. She's 5'3" and I'll use that to scale the rest.

What's really neat about his building is a few things. It was built in 1834 as a meeting hall and rebuilt in 1889. The brick detail around the horizontal buttresses is interesting with the stair step layering of each course as they move outwards and inwards. This is easily seen at the side entrance.



And still another interesting subject which I noticed this week while sitting in the car in Lambertville, NJ. Both Newtown Bucks County, PA and Lambertville, NJ are Revolutionary (or older) towns that have wonderful architecture from both the 18th and 19th Centuries. This one is dated at 1879 and has wonderful detailing around the windows and massive stepped corbels at the roof cornice. Before 3D printing capabilities I wouldn't have known where to start on structures like these. Now I look at them and already start noodling





about how I'm going to attack this or that. It used to be "How can I build that?". Now it's "How can I draw that?" If I can draw it, I can probably figure out how to print it. If I can print it, I can build it.

Shown is a close up of those corbels. And look at the pierced work over the windows. Stunning.

Most of the details can either be printed or laser cut. The store fronts are an exception which I tend to scratch build old school with styrene.



So this gives you some things to think about before the October sessions. I think the first step in enlightening the viewer is to build the curiosity to really look at the classical buildings all around them. Everyone has a high-resolution camera in their pocket and can take both long and close ups in an instant. Just about every small town in the USA has examples of Victorian, 2nd Empire and Arts & Crafts architecture that can now be modeled with the technology available to the modeler without massive investment. Once I stepped into the world of doing custom buildings, this aspect of model railroading overtook running trains as the area of greatest satisfaction for me. When building your own structures you can go into much greater detail and complexity than the kit manufacturers. They have to build to the lowest common denominator and make kits that the average builder can successfully complete.”

These segments have modelers who want to share their modeling efforts, their skills, and discuss their modeling

techniques so others can benefit. These segments can be for one or more shows depending on the details included for the model building presentation.

I hope you will share your modeling experience in this segment so other modelers can benefit from your knowledge and techniques. As I always say: “Jump on in, the water is fine!”.

Now Some More “New Tracks” upcoming Show Announcements

Modeling Lifestyle: A NEW Bi-Weekly YouTube SHOW Thursday evenings at 7pm EDT on our YouTube channel, New Tracks Modeling

Sponsored by [Narrow Gauge Modeling Company](#) (NGMC).

New Tracks has started a new, bi-weekly live series called: Modeling Lifestyles. It is available to watch live only on our YouTube Channel, New Tracks



Click image to view [Narrow Gauge Modeling Company's video introduction.](#)

Modeling. This is your opportunity to meet and chat with some of today’s outstanding modelers and a few of their friends. You can see all of the interviews we have conducted so far on our YouTube channel, New Tracks Modeling.

For a complete list of the modelers and their friends who will be on our shows, please visit and subscribe to our website newtracksmodeling.com.

Please also subscribe to our YouTube channel: New Tracks Modeling, and ring the bell to get prior notification of the shows and be able to join us live.

New Weekly Wednesday Show Segments

Find your Mentor at Local CLUBS and NMRA DIVISIONS, A bi-weekly segment about local organizations supported by the NMRA.

This bi-weekly segment introduces local model railroading organizations where many of us have found mentors, modeling help, and met some really outstanding modelers. I credit these local organizations with providing much of my modeling education and believe such organizations can also help you.

Phil Edholm, a NMRA Division Superintendent of the NMRA PCR/Coast Division, which includes the San Francisco Bay Area down to Monterey in California, hosts this segment.

Every other week these segments features an NMRA Division or local club leader to discuss what activities they do, how they engage with members, how they recruit new members, and other topics. The focus is a discussion around what they have done that is successful and unique. The goal is to have actionable ideas come out to be used by other organization in their local activities.

Each time we try and identify a few key nuggets that all of us in model railroading leadership positions can use. Each segment is live streamed to our YouTube channel New Tracks Modeling. You can see all of the previous interviews on [YouTube HERE](#).

If you or someone you know is actively involved in an NMRA Division or a local club and you would like to discuss how your organization is thriving (or surviving) in the new world, please contact Phil Edholm at coastsup23@gmail.com or at 408-832-5618.

Let's Go to Our Local Modeling Hobby Store

A monthly segment with hobby store owners.



Supported by the National Retail Hobby Stores Association (NRHSA). “Local hobby shops need modelers and modelers need local hobby shops”. This segment shows just how true this statement is.

Mark Poggendorf, a NMRA member working on his MMR, a hobby manufacturer, an owner of Poggies Trains Hobby Shop, and a member of the National Retail Hobby Stores Association (NRHSA), started a monthly show segment concerning common issues facing hobby shops and modelers. The focus of these segments is on how both groups can work together for their mutual benefit,

and hopefully introduce the value of your local hobby store to help you become a more knowledgeable and more skilled model builder.

Mark’s involvement in our hobby as a modeler, manufacturer and, hobby shop owner, who not only has a brick and mortar store in California but also attends, as a vendor, many of the train shows throughout the Country, makes him well suited to lead these discussions and provide insights into the problems and issues facing both the hobby shop industry and modelers. Who knows, you also may find a new source for your modeling products from this series.

If viewers have a hobby shop to recommend to be on our show, or you are a hobby shop owner who is interested in being on our show, please let Mark know at Mark.Poggendorf@newtracksmodeling.com.

It’s that time again, for me to return to my workbench and start working on something that I fell in love with



and just have to model. It happens all the time. My current project is a 1938 Buick “Y” model in brass. But I just found this photo. It will be my next project!

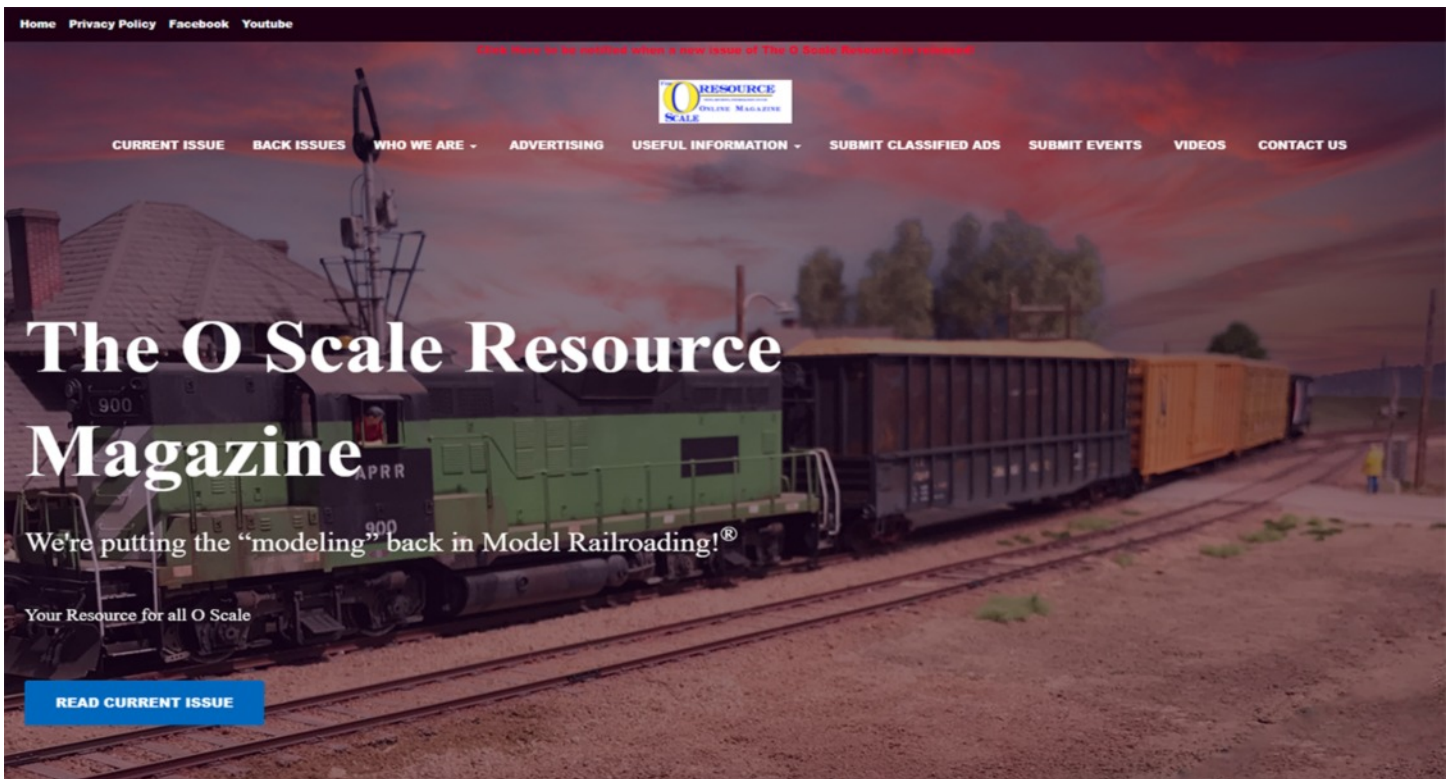


While I am modeling, you can help New Tracks by:

1. Volunteering to help us produce and develop our New Tracks shows.
2. Considering a contribution to our Patreon account to help pay for our out of pocket cost to run our shows. Click here to donate <https://www.patreon.com/posts/welcome-to-new-72881040>

3. Subscribing for free to our YouTube Channel, New Tracks Modeling, and ringing the bell to get advance notices of our YouTube shows including our Modeling Lifestyle series. If the advertisements on our YouTube videos are of interest, please watch them so we can gain a little revenue to produce our shows.
4. Subscribing to our website, NewTracksModeling.com, which provides log in links to our Wednesday Zoom events and also provides information about what New Tracks you can travel.
5. Spreading the word about our New Tracks Modeling Mentoring Scholarship program to every young model railroaders you know or you meet. Get the word out. Details at our website: newtracksmodeling.com/scholarship
6. Subscribing for free to *The O Scale Resource* and *The S Scale Resource* online magazines so you don't miss any of my articles and also see some great modeling by various modelers who may become one of your mentors.
7. Writing to me! I love getting your comments, suggestions, and modeling ideas. I so enjoy hearing from you and having a conversation. My email is: jimkellow@newtracksmodeling.com

Thank you again for reading this far. Until next time with more New Tracks, happy modeling!



Check out our Website. All back issues are available for download. Submit your events and classifieds ads online, or request advertising information. Need information like drill sizes or prototype pipe dimensions, nut bolt dimensions or even Westinghouse brake diagrams? It's all here. Check out our videos also!

Everything you need on one place!

NEW TRACKS MAY 24, 2023 MY BUILD

Greg Cassidy - gcassidy2@verizon.net - N scale - LifeLike Baldwin Station. Mudd Creek Barn. Mudd Creek Freight House. Mudd Creek General Store. Mudd Creek models all had modifications.



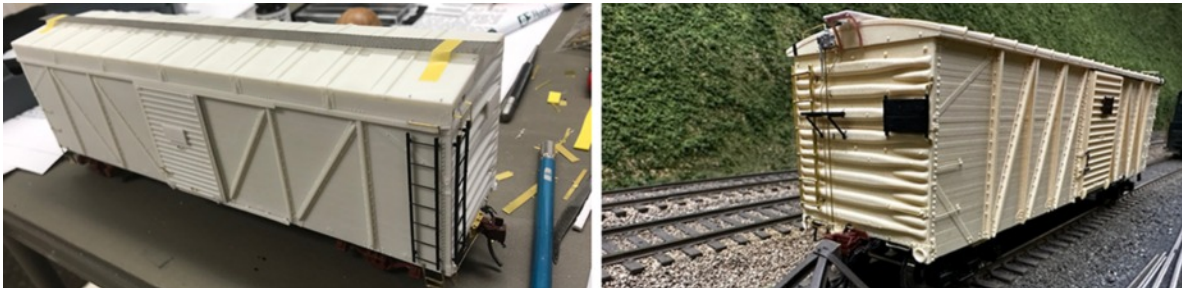
NEW TRACKS MAY 24, 2023 MY BUILD

Gary Shurgold - gshurgold@gmail.com – HO scale

Jordan Stagecoach & Woodland Scenics Daniels Outfitters.



Top 6: Jeff Jordan - jordan.jordan54@verizon.net - O scale – Banta kit with Wiseman interior pieces.
 Woodland Scenics Plug-And-Play Lighting. Schomberg Scale Models – Hydrocal building with resin details.
 Roof was swapped out for paper corrugated roofing from Clever Models.
 Bottom 4: James Taverna - jtaverna1@cox.net - O scale - Various rolling stock.



NEW TRACKS MAY 24, 2023 MY BUILD

*Cliff Flatten - O scale scratchbuilt building for a customer using various materials. S scale practice pieces.
The smaller buildings are East Coast cottages/beach houses that will be made into kits.*



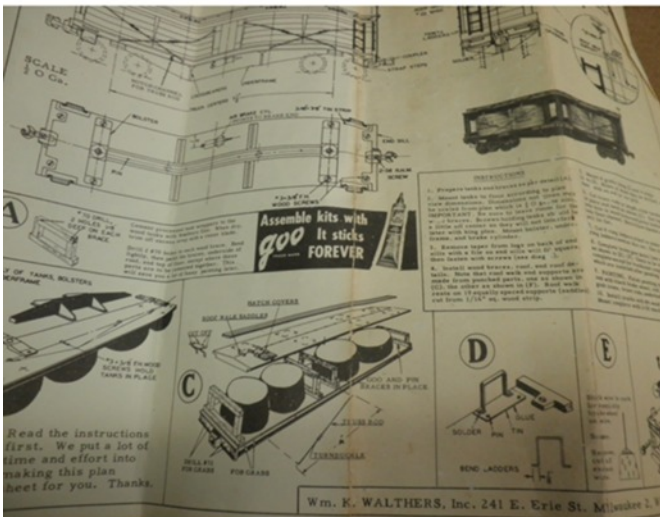
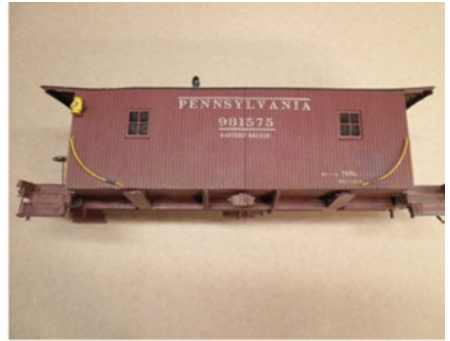
NEW TRACKS MAY 24, 2023 MY BUILD

Alan Rogers - eauchiche@gmail.com – O scale – <https://newcreationsvictorian.com/> New Creations Victorian Railroad Buildings - 1928 Bungalow, Weller Gas Station, Weller Store.



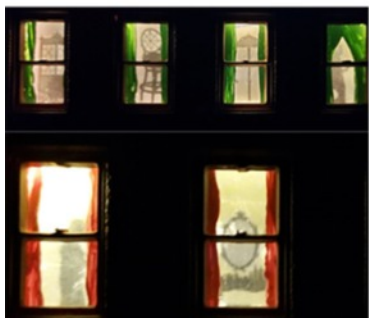
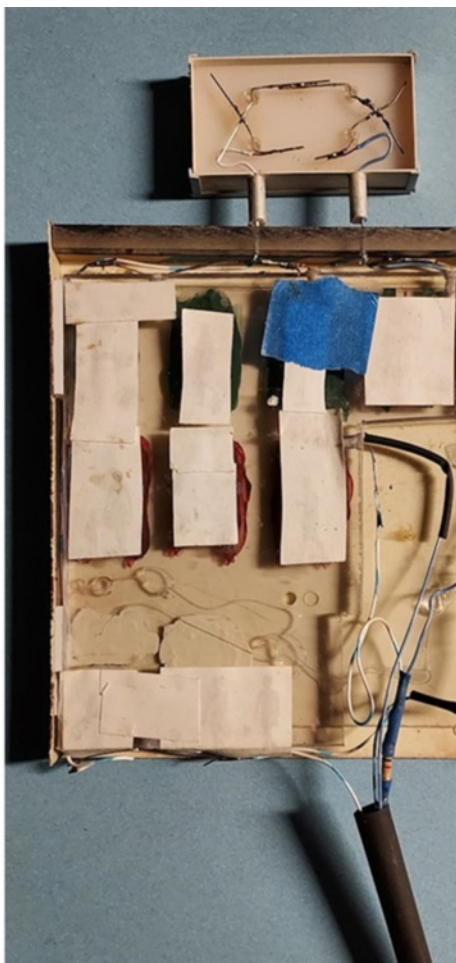
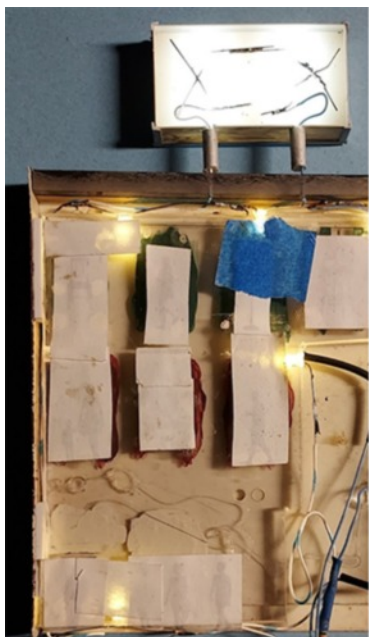
NEW TRACKS MAY 24, 2023 MY BUILD

Martin Brechbiel - martinwb@oscalemag.com - O scale - 2 cabooses and a Walthers pickle car.



Top: David MacLean - dmac4101@gmail.com - O scale- CPR's "Big Otis" Gondolas - Designed in Fusion 360 and 3D printed on my Prusa MK3.

Bottom: Kirk Bucher - kirkbuch@aol.com - background flats that demonstrate the use of black and white drawings in window scenes.



O SCALE SHOWS & MEETS

Have an upcoming O Scale event? We would like to help publicize it. Send us the information up to one year in advance, and we'll place it here along with a direct link to your Website and/or Email. [Click here to send us your information.](#)

Strasburg 2 Rail Train Show

August 12, 2023

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

43rd National Narrow Gauge Convention

August 30, 2023 – September 2, 2023

Denver, CO

Full roster of informative clinics presented by well known model railroaders and railroad specialists, to the largest dedicated narrow gauge retail space anywhere, the convention hotel will be the hub of the week's activities. Both on-site model railroad displays as well as home layouts will be on the schedule. Website: <https://www.43nngcdenver.com/>

Strasburg 2 Rail Train Show

October 14th, 2023

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

Southwest O Scale Meet 2023

October 27th – 29th, 2023

7301 Dutch Branch Rd, Ft Worth 76132

Fort Worth Academy Gym - rear of the school campus Located within minutes of the Hadley / Ross / Walter layouts. From the website, you can print a paper reservation form to mail by USPS.

On line registration is under construction and will be available soon.

<https://swoscalemeet.com/>

2023 Cleveland 2rail O Scale Meet

Nov 4, 2023 9AM -2PM

6' tables \$47 \$50 after Oct 1st

Admission to all \$7

Friday night load in 1PM to 5PM

440-248-3055

Website cleveshows.com plenty of free parking.

2023 Danville Indiana Train Show O/S

November 18, 2023

This will be the 16th year that volunteers from the CID have put on this outstanding multi-scale/gauge Train Show. The 2023 Show will highlight O-S Scalers in an exclusive 2,000 square foot room for vendors, layouts, and socializing. Last year we had 14 vendors with 32 sales tables dedicated to O-S Scale Trains. Make your plans early to attend. We expect to sell out the O-S Room. [Please download the O & S Vendor Invitation here](#) to secure your sales table.

NOTE: Please register and make payments to the CID contacts on the Vendor Flyer.

S Scale Contact: Charles Malinowski,
bmry1905@gmail.com, 317-730-3187

O Scale Contact: Eric Peterson, ericgpet@gmail.com,
904-607-7035

O Scale March Meet

March 14-17, 2024

Westin Lombard Yorktown Center
Lombard, IL

The March O Scale Meet is a 3 day gathering of vendors, customers, clinics, and fun held annually in March in the Chicagoland area. This is the Chicago O Scale train show you've heard of.

Website: <http://marchmeet.net/>

Email: ChicagoMeet@yahoo.com

MANUFACTURERS



Phone: 573-619-8532
Email: orders@scalefigures.com
Web: scalefigures.com



Free Shipping

See our web site for details

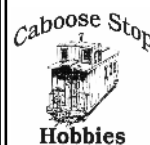
Cast resin items for your S & Sn layout.
Tunnel portals, bridge piers, abutments, culverts, and retaining walls.
Trackside details and sage brush tree armatures.

SHOWS & MEETS

March O Scale Meet
March 14-17, 2024

<https://marchmeet.net/WP/>
Ph. 414-322-8043

DEALERS



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301 Main St.
Cedar Falls, IA 50613
Phone: 319-277-1754
trainguy34@gmail.com
Web: www.caboosetophobbies.com



RGSRR Hobbies
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Whitewater, CO 81527
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www.rgsrrhobbies.com

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