

# O RESOURCE

*THE*

**NEWS, REVIEWS, INFORMATION TO USE**

**We're putting the "modeling" back  
in Model Railroading! ®**

**Volume 11 No. 2**

**November/December 2023**

# SCALE

**10**  
CELEBRATING  
**YEARS**



**The J. Robb Warehouse: Scratchbuilt Structure  
Container Business for the Mountain Electric  
Build a Small Portable Layout  
O Scale... Old School Style  
New Tracks & My Build  
Backshop Solutions  
and so much more...**

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**November/December**  
Volume 11 No. 2

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Welcome to the online O Scale Resource magazine. The magazine is presented in an easy to use format. The blue bar above the magazine has commands for previewing all the pages, advancing the pages forward or back, searching to go to a specific page, enlarging pages, printing pages, enlarging the view to full screen, and downloading a copy to your computer.

Front Cover Photo

*A small scene from Brian Scace's article on building a small portable layout.*

# BILL OF LADING

- 2 Bill Of Lading**
- 5 Guest Editorial**  
*By: David Vaughn*
- 10 News You Can Use - New Items of Interest**
- 18 Exhibition, Apartment, or Office: Build a Small Portable Layout**  
*By Brian Scace*
- 29 The J. Robb Warehouse**  
*By Dave Lull*
- 33 A New Container Business for the Mountain Electric**  
*By George Paxon*
- 58 O Scale... Old School Style Vintage O Scale 1926-54**  
*By Carey Williams*
- 63 New Tracks Modeling**  
*By Contributing Editor Jim Kellow MMR*
- 96 New Tracks September, 2023 My Build**
- 102 Backshop Solutions**  
*By Ross Dando*
- 106 So, What Are You All Up To?**
- 110 Show Schedule**
- 111 Classified Ads**
- 111 Advertiser Index**



The Model Railroad Resource, LLC publishes *The O Scale Resource* and *The S Scale Resource*. Be sure to look at both of our magazines. There are many articles in our magazines that are not scale specific and will be of interest to you. Click the magazine title in this announcement to see the magazine.

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### THURSDAY

Fox Valley O Scalers  
Operating Session: 12 pm - 5 pm  
Layout Tours

### FRIDAY

Dealer Setup: 1 pm - 9 pm  
Layout Tours  
Evening Discussion Meetings

### SATURDAY

Dealer Setup: 8 am - 9 am  
★ Show: 9 am - 5 pm ★  
Model Contest, Clinics  
Layout Tours

### SUNDAY

★ Show: 9 am - 2 pm ★  
Layout Tours



The cost for admission is \$30 per person (spouses are free of charge), children 15 and under are also free.

This \$30 admission fee covers the entire weekend, from the meetings beginning Friday night until the close of the show Sunday afternoon, including all layout tours and clinics.

You can register in advance by clicking this ad and going to the registration form tab at the top of the page and printing off a form and mailing it in with your payment. This ensures that you will not have to wait in line at the show and will have a preprinted badge waiting at the show for you.

You can also purchase admission at the door at any time.

The registration form is the same for people who just wish to attend the show and vendors who wish to rent tables at the show.

# Guest Editorial

The OS2R Revolution will be live – and 3D Printed

By: **David Vaughn** President, O Scale Central



*Sarah Griesenboeck at her clinic at the Denver National O scale show. Also showing her many designed parts in both Resin and FDM.*

*David Vaughn photo*

**Great things are happening in OS2R. I am going to connect some dots for the scale.**

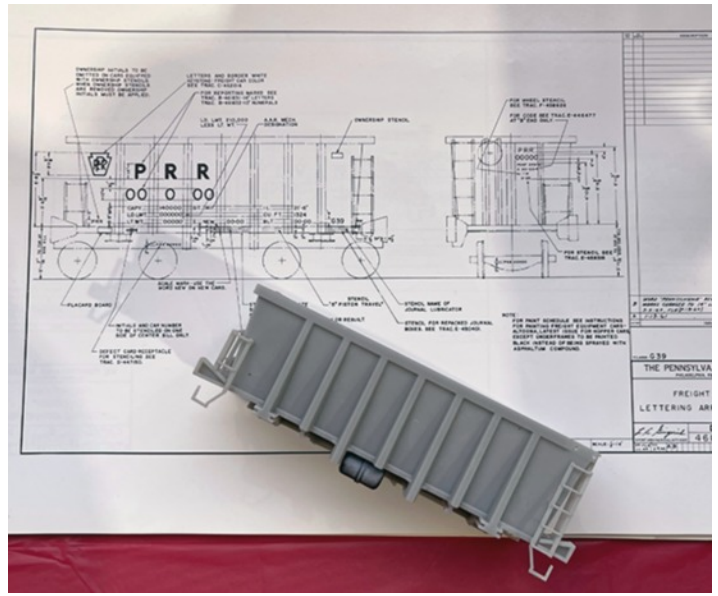
- Sarah Griesenboeck has been designing and printing parts with remarkable fidelity and detail. Both parts design and manufacture are progressing rapidly. Sarah leads by example and shares her accomplishments for use by and inspiration to other modelers. It will be only a short period of time until detail parts – model-specific and railroad-specific – will be within the modeling capacity of many O Scalars and available from small manufacturers. Sarah titles her efforts the “train kitchen”. The question for detail part availability will no longer be limited whether PSC made it.

<https://trainkitchen.com/>



*Erie RR's unique trucks from Sarah Griesenboeck is an example of the files she has at her Website: <https://trainkitchen.com/> The files are there for anyone to download and print. Dan Dawdy printed these for a customer.*



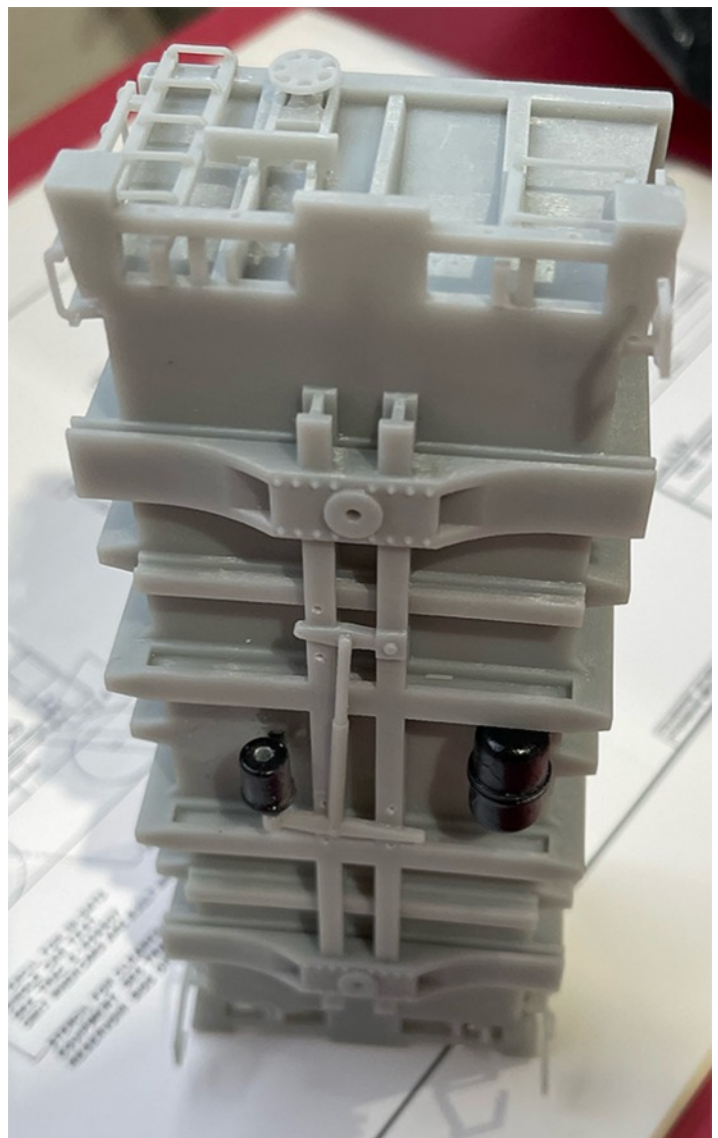


Above: Rick Trinkle at his clinic at the Denver National O scale show. Left: One of Dylan Lambert's many test shots of cars he is working on.

David Vaughn photo

- Rick Trinkle is on his way to turnkey rolling stock projects in O Scale: he needed a roster of coal gons, set up the programs and printers, went off to work and, when he returned each day, he would take out finished cars. Translated to the rest of us, the purchaser (a railroad historical society, small company or individual modeler) will be able to pull together a plan package for a favorite (but unavailable) piece of rolling stock, provide it to a vendor, who prepares the cad files and masters, prints and/or casts the parts and delivers them packaged and ready to go.

- Dylan Lambert (Lambert Locomotive Works) is producing 3D printed models of a PRR G39 ore car and of a 60' center beam car. Why is this important? Because these are nicely done models of interesting prototypes not previously available and will be available in quantity for affordable prices: the G39 will be \$40/each (assembled, but less trucks and couplers) plus shipping. OS2R, with its potential for great detail and the necessity of short runs, was made for 3D printing. Or the reverse: 3D printing was made for OS2R. Got a favorite car but have given up on brass, injection molding or scratch building to get it? 3D printing of cars is just around the corner. [lambertlocomotiveworks.com](http://lambertlocomotiveworks.com).



- Dylan also designed and 3D-printed a B&A commuter coach and sold and shipped the models. Why is this important? Because it is a step in disproving the "O Scale models are too big to fit in the printer" bias. He had



growing pains in both printing and shipping, but the good news is that the problems got solved and the models went out as advertised.

- Scaling up 3D files is getting more straight-forward. As print quality improves, the ability to simply take a file written in one scale and scale it up to print in 1:48 is happening. So things available in HO can be adapted for our use. By way of example, sometime prior to the end of 2023, there will be an additional line of trucks of types never seen in OS2R before. 3D printed, nylon axle inserts.

- 3D printing is not just for techies anymore. Eric Peterson reports that he can go to his public library and get parts printed on the library's printer. Services to print 3D parts for modelers are becoming more available. And 3D printers themselves are advancing with processing-speed rapidity: more capabilities, lower prices, easier operation.



- There are some designers out there who are making masters for resin kits which have incredible fidelity and detail. There are limitations on casting skills and resources. Count on supply and demand to open this up.

- Oh, yeah: figures have always been an easy way to look at a photo and conclude it is a model, even in O Scale. That too is changing, as new, holographic type scanning techniques allow poses and attire taken from life and improved painting techniques make it tougher to spot figures as models.

- None of this takes away from the recent efforts of companies like Kadee to bring new, more conventionally developed, models to market. It's not either/or. Smaller also companies – like All Nation - are using new techniques along with more traditional efforts, to bring to market more conventionally developed models.

*The hood of Glenn Guerra's brass SW-1 along with the air tank ends and headlights were just some of the parts Dan Dawdy printed for Glenn, who then did the wax molds that were cast in brass for the locomotive.*

- So where do these elements point? I submit that OS2R is just around the corner from having models of rolling stock of any prototype, however specialized and obscure, at extremely high levels of fidelity and detail, almost on demand, at reasonable prices and with good returns for the creators. The pieces will come on line on different schedules and to different degrees of being finished. Some elements are here now. Others are 6 months, a year or two years out. But they are coming.



- Proto-48 continues to offer additional levels of fidelity with closer-to-scale trackwork and trucks, but the advantages of better techniques and higher-fidelity models will be available to all of us.

- Improvements in DCC and sound and in power-on-board are ongoing. They will make our locomotives more prototypical in sound and better in operation. And don't underestimate the longer-term role of 3D printing in engine parts – and maybe whole engines.
- Advantages to the designer/manufacturer of these changes are that the designs are scalable (within limits) and there will be essentially no minimum runs and no inventory. This should lower barriers to bringing new models to market and benefit all of us.
- With increased efforts to familiarize detail-oriented modelers in other scales with what is going on in O Scale, we should be making converts from other scales. Railroad Prototype Modeler meets are full of folks for whom the O Scale revolution, with its high levels of fidelity, is a natural environment.
- Nothing in the revolution that is coming cancels our existing models or the satisfaction we get from them. However, what is coming won't be limited to your grandfather's hobby. The potential for our scale and modelers in it will be game changing. Get ready for a very exciting future for OS2R.

Happy Reading & Happy Modeling,

*David Vaughn*



## 41st Annual Cleveland Two Rail O Scale Meet

**November 4<sup>th</sup> - 9am - 2pm**

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**November, 2023 Sets**

**12002: Passenger Car Set, #1**

- TCP-054 Pullman Green
- TCP-225 Interior Cream
- TCP-226 Interior Pink
- TCP-227 Interior Light Tan
- TCP-228 Interior Light Green
- TCP-229 Interior Light Blue



**10406: Metals Set, #1**

- TCP-354 Galvanized Steel
- TCP-355 Dark Aluminum
- TCP-356 Semi-Matte Aluminum
- TCP-396 Metallic Burnt Iron
- TCP-412 Matte Steel
- TCP-774 Metallic Tin

**December, 2023 Set**

**11012: CSX Set**

- TCP-265 Y2k Scheme- Blue
- TCP-266 Y2k Scheme- Gray
- TCP-267 Y2k Scheme- Yellow
- TCP-289 Yn3 Scheme- Blue
- TCP-290 Yn3 Scheme- Yellow
- TCP-332 Beige (Cream)



**NEW SERIES- Paints for 3D-Printed  
& Cast Resin Objects**

**November, 2023**

- TCP-5014 Glossy, Sprayable Medium Purple
- TCP-5018 Glossy, Sprayable Light Gray
- TCP-5029 Glossy, Sprayable Red- Orange

**December, 2023**

- TCP-5010 Glossy, Sprayable Light Blue
- TCP-5028 Glossy, Sprayable Off-White
- TCP-5036 Glossy, Sprayable Cherry Red

**Current, 2023**

- TCP-5001 Glossy, Sprayable Medium Red
- TCP-5008 Glossy, Sprayable Medium Green
- TCP-5011 Glossy, Sprayable Medium Blue
- TCP-5026 Glossy, Sprayable Medium Brown

*These paints are specifically designed with enhanced adhesion promoters for 3D-printed and cast resin models and parts.*

Come Visit Us at Our [YouTube Channel](#) at: Tru-Color Paint. We are also on [Facebook](#) & [Instagram](#).

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**References gladly furnished.**

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Visit my website at [americanscalemodels.com](http://americanscalemodels.com) for models, detail parts, trucks, and more!

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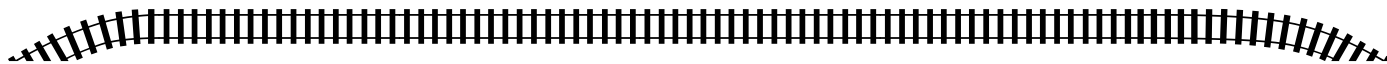
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# NEWS YOU CAN USE



The Rockford O Scalpers 2023 Fall Open House will be held Saturday November 4th from 12:00 noon until 6:00pm. Details below:



Location: Rockford O Scalpers  
3886 Lookout Drive  
Rockford, IL 61109  
Time: 12:00 noon to 6:00pm  
Hosts: John Handlogten, Frank McCabe,  
Tom McCabe

All are welcome to attend and we look forward to seeing everyone.



## Atlas Model Railroad Company, Inc. and MTH Electric Trains Announce Shared Asset Deal

Hillside, NJ-- Atlas Model Railroad Company, Inc. and MTH Electric Trains today announced a shared asset deal that will allow each company to use select models from the other company's O scale tools and molds to complement products being produced by each company.

The first of these shared assets will be passenger car tooling that can complement locomotives being produced by the other company. The program expands upon the sale of select MTH O scale tooling and molds in 2021 to Atlas and the incorporation of MTH's

onboard locomotive Proto-Sound 3 sound system into select Atlas O scale locomotive releases that began in 2022.

Atlas' first product announcement utilizing the shared asset program will be unveiled in the October 2023 York Catalog later this month.

Products produced from the shared assets will be branded in the company packaging of whichever company is using the tooling and molds to complement that company's products. As an example, the production of streamlined passenger cars using the Atlas tooling to complement an MTH steam or diesel locomotive by MTH will be marketed, warrantied, and sold by MTH and packaged in MTH Premier Line boxes. The production of MTH passenger cars using tooling still owned by MTH to complement Atlas locomotives will be marketed, warrantied, and sold by Atlas and packaged in Atlas O Premier boxes.

"This is a great opportunity for both MTH and Atlas to offer our customers even more variety and choice," said Rich Foster, vice-president of sales for MTH Electric Trains. "We're excited to see what we can create together."

"This partnership is a win-win for both companies," said Jarrett Haedrich, COO of Atlas Model Railroad Company. "It allows us to offer our customers more of the O scale products they love, while also providing both firms better economies of scale throughout our production processes."

Founded in 1924 as Atlas Tool Company, Atlas Model Railroad Co., Inc. was incorporated in 1949 and produces locomotives, rolling stock, track, and accessories in N, HO, Z and O scales for its Atlas Master, Atlas Classic, Trainman, and Atlas O and Atlas O Premier lines.



David Vaughn of [Wit and Wisdom Models](#) has announced a very limited run of the B&O Deshler, OH Signal Tower.



Guarding the Crossroads of the B&O, DR Tower in Deshler, OH is one of the most recognizable and famous signal towers left from the Best and Only. Deshler was the site where the B&O's Detroit Line crossed the Main Line to Chicago. The tower was responsible for guiding trains from the Toledo Division (the North-South line) over the Akron Division controlled Main Line (the East-West line). At peak traffic, upwards of 60 trains per day, including the Capitol Limited and Cincinnati, would pass through Deshler.

Today, CSX operates both lines. The train station was demolished in 2022, but Deshler is still home to DR Tower and some of the last remaining functional B&O CPL signals. The crossover is also home to a railfan organized park called Crossroads Park and a Virtual Railfan camera.

The Kit includes:

- Base for signal tower
- Parts for two window configurations per timeframe modeled
  - 1906 to 1975 (1930's Black and White photo for reference)
  - 1976 to present (2000's colored photo for reference)
- Exterior details per timeframe modeled

[See the tower page on their Website for more here.](#)

New from [Model Tech Studios LLC](#).

Working on the Railroad 2 Pack Special. The Train Conductor and Station Agent O scale Finished Figure set to detail your Railroad scenes the right way with these essential RR men. NOTE: Minor color scheme variations can occur as these are Hand Finished. Made in the USA 1/48 figure.

### Working on the Railroad

Station Agent

Train Conductor



O Scale train dispatcher is busy at work at his desk. Ensure safe operations of your rails with this essential figure. Highly detailed down to his microphone in hand, speaker, train sheet and logs. A truly unique O scale set comes all painted. Decals come with the set to be applied as you wish.

[See their Website for more great products.](#)

[John Wubbel of All Nation Line](#) says they have you covered from beginning to end from engines to cabooses!

In addition to the F3/F7 & Switchers, All Nation continues to be excited to offer 3 rail to 2 Rail Conversion kits as well as Ready to Run Engines. We are sure modelers will enjoy the ready to run 3 Rail to 2 Rail Conversions:

- MTH GP35 All Nation Single Drive Helical Gear Tower Transmission Blomberg Trucks Ready to Run (with Pilots)
- MTH GP60M All Nation Single Drive Helical Gear Tower Transmission Blomberg Trucks Ready to Run (with Pilots, Snowplow & Couplers)
- MTH GP40-2 using the All Nation Single Drive Transmission Ready to Run
- Lionel GP-35 All Nation Dual Transmission Drive

Check out all our Ready to Run Engines on our site at:

[https://allnationline.com/WP/?product\\_cat=ready-to-run-diesel](https://allnationline.com/WP/?product_cat=ready-to-run-diesel)



What a feast of new offerings in our 3D printed caboose line! Our caboose kits range from beginner kits to advanced where modelers can decide when to add detail kits and trucks. The caboose kits are a mix of 3D Printing & other detail parts in either brass, metal or plastic. Our newest caboose kits include:

- Western Pacific / Sacramento Northern Caboose
- Atlantic Coast Line Class M3 Caboose
- New York Central Riders Car
- Chicago & North Western Drovers Car
- Chicago & North Western Way Car

Check out all our Caboose Kits at:

[https://allnationline.com/WP/?product\\_cat=caboose-kits](https://allnationline.com/WP/?product_cat=caboose-kits)



All Nation's custom transmissions are hand crafted to power your trains and keep the freight rolling. We continue to work 1 on 1 with modelers to ensure that we can get their engines moving. We strive to provide ready to hook up and as well as supply parts that you can't find anywhere else. If you haven't had a chance to recently review our transmissions, you can do so at the following link:

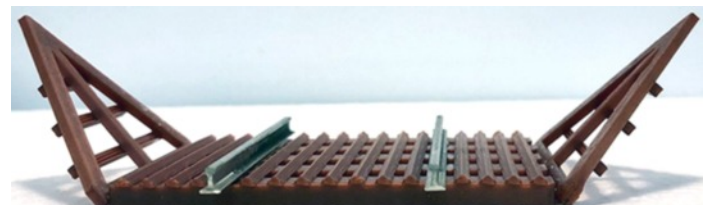
[https://allnationline.com/WP/?product\\_cat=power-transmissions](https://allnationline.com/WP/?product_cat=power-transmissions)



New Product: Having trouble keeping cows and people off your track? The All Nation Cattle Guard is a unique kit and within your holiday budget.

Check out additional pictures of this kit at:

<https://allnationline.com/WP/?product=all-nation-cattle-guard-plank-crossing-kit-3d-printed-pn407ank>



Realistic 3D Printed grills 2 styles for F7 engines.

The original (#500) All Nation side grills for ventilation were polished aluminum and silk screened black ink to simulate the prototype. We set out to improve on this feature of the locomotive for our F7 engines using 3D Printing technology. The thickness is now approximately 0.6mm and the silver PLA plastic simulates the chrome with a transparency such that in the right light, one can see the engine detail cast into the engine body behind the grill work.



The grills are also printed with silver on a black background. In this pack, the lengths are for both A & B Units. You can check out these products at the following link:

<https://allnationline.com/WP/?product=all-nation-grill-f7a-unit-1-pair-and-f7b-unit-1-pair-3d-printed-pn500pla9534615387443290517846>

For more details, see our advertisement in this issue and enjoy our website.



Our friend Scott Cohen of Tru-Color sent us the following: Here is the new product information for Tru-Color Paint and Phoenix Precision Models for November-December, 2023. The sets comprise six (6) 1-ounce paint bottles of either all sprayable or all brushable paints (for now, anyway.)

TCP is releasing a set of 3D printer paints that are specifically designed with enhanced adhesion promoters for the resins that comprise most 3D printed and cast resin objects. We have tested these paints on (approximately) a dozen different resins (and combinations) to make sure the paints adhere correctly. So, if you guys would like any of the models and paints to review, just let me know and I'll send some out to you in either S- or O-Scale (with paint.)

These paints reflect generic colors; they aren't specific to railroading, ship, plane, armor, or car

modeling. We do have some specific railroading paints that we have put in a combination of adhesion promoters to bind to 3D printed engine shells and rolling stock bodies.

### Current Phoenix Precision Models Objects in O-Scale, 2023

- 33000- 20' Container Construction Office (1)
- 33010- Mid-Dozer (1)
- 33025- Compressor Trailers (2)
- 33510- 12' Ladders (3)
- 33515- Closed Dumpsters (1)
- 33525- Tall Pallet Stack Assortment (4)
- 33536- Crate Stacks- Type 1 Only (1)
- 33539- Single 3' Crate (3)

### November, 2023

#### Railroad

- 4200: San Diego Coaster- Dark Blue
- 4201: San Diego Coaster- Teal Blue
- 4202: San Diego Coaster- Gray

#### 3D-Printed and Cast Resin Paint

- 5004: Glossy, Sprayable Light Yellow
- 5009: Glossy, Sprayable Dark Green
- 5024: Glossy, Sprayable Beige

#### Paint Sets

- 12002: Passenger Cars, Set #1
- 054: Pullman Green
- 225: Interior Cream
- 226: Interior Pink
- 227: Interior Light Tan
- 228: Interior Light Green
- 229: Interior Light Blue

#### 10406: Metals, Set #1

- 354: Galvanized Steel
- 355: Dark Aluminum
- 356: Semi-Matte Aluminum
- 396: Metallic Burnt Iron
- 412: Matte Steel
- 774: Metallic Tin

#### Phoenix Precision Models

- 33527- Single CHEP Pallet (1)
- 33530: Skylight- 0-Windows Out (1)
- 33545: Food Service Counter (1)
- 33560: WW2 Supplies Crate (1)

## December, 2023

### 3D-Printed and Cast Resin Paint

5007: Glossy, Sprayable Light Green

5020: Glossy, Sprayable Dark Gray

5035: Glossy, Sprayable Yellow-Brown

### Paint Sets

11012: CSX Set

265: Y2k Scheme- Blue

266: Y2k Scheme- Gray

267: Y2k Scheme- Yellow

289: Yn3 Scheme- Blue

290: Yn3 Scheme- Yellow

332: Beige (Cream)

10105: 1970 Dodge Challenger Set

502: Midnight Black

503: Sublime Green

504: Light Blue

506: Bright Red

517: Sienna Orange

518: Bright Yellow

### Phoenix Precision Models

33015: Excavator/Backhoe (1)

33040: Stack of 4 Tires (1)

33541: Individual Tires (4)

33560: 6' Rectangular Crate (1)

If you or your readers have any suggestions for new colors or new paint sets, email us at [tru.colorpaint1@yahoo.com](mailto:tru.colorpaint1@yahoo.com). If we can find enough information on the color, we could put it in the next year's product schedule. [See their Website here.](#)



New from Atlas O:

Atlas O Premier SD45 Locomotives.



Like the Pontiac GTO, the SD45 was a mid-60s General Motors muscle car. Stuffed under its brick-like hood was the largest diesel motor made up to that time, a turbo-charged, 20-cylinder, Electro-Motive Division model 645E.

The Great Northern took delivery of the first SD45 in May of 1966 and promptly painted the nickname "Hustle Muscle" on its flanks. The name said it all: the SD45 was heavy freight power, intended for long trains and high speeds. And it had the muscular looks to match, with flared radiators capping a long body that filled every inch of its frame - as opposed to its baby brother, the SD40, which had an open "porch" at each end of the engine.

[Click here for more details.](#)

The Atlas O Premier F40PH Locomotives have some new paint schemes. [Click here for those.](#)

[Click here to download the Fall 2023 catalog.](#)



[Rusty Rail has released some new castings.](#)

Here are a set of vents for your building roofs. It comes with 4 cyclone vents in different sizes and 2 of the square vents. You get to 6 vents in all. Castings come unpainted.



Here we have a set of castings that are dust collectors that were mounted on the roof or the side of a manufacturing building. You add these to existing

building on your layout to add to the detail. Castings come unpainted.



[Check their Website for all their castings.](#)

---

Lambert Locomotive Works is proud to be working to bring more variety to O scale, whether it be narrow gauge or standard. For standard gauge, the question has become "Has it been done before?". In keeping with that question, and by extension the standard gauge mission statement of LLW, there's a number of designs we've been working on. But right now what's most important is our G39 ore car, a car that Pennsy modelers may readily recognize. To that end, the important point. Less trucks and couplers, the G39s are \$40/ea plus shipping and we're preparing to open our order books and start spinning up the first batch. But what do you get for that \$40?

Well, as a start, there were two versions. The G39, which rolled out in 1961 and the G39A. The biggest difference here is the location of brake fittings on either car. The G39 has all components mounted under the car, while the G39A (which started rolling from Sam Rea Shops in 1964) saw the air reservoir and brake valve relocated to the end of the car opposite the hand brake.



The O Scale Resource November/December 2023

With the LLW car, you can configure your model as a G39 or G39A, as the appropriate hardware is included for either version.

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If you want in on these cars, it is my intention to be ready to begin shipping cars by January 1st if not sooner. Interested in getting in on these? Well, we set up a Google form that can be used to leave word as to how many cars you might want and how I can reach out. That link is included here;

<https://forms.gle/LyMdWHoserXWr1hp7>

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In recognition of November being National Model Railroad Month, the Rockford O Scalers have a month long exhibit of 1/48 O Scale caboose models on display at the Cherry Valley, IL Library.



These twenty models are a part of our caboose collection and include both brass and scratch built styrene models from Malcolm Byrd. We included a description of what the railroad caboose is and its purpose, as well as, some samples of reference books on cabooses.



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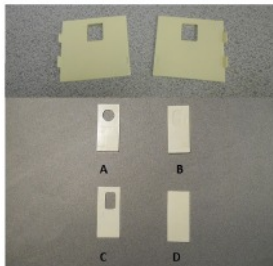
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# Exhibition, Apartment, or Office: Build a Small Portable Layout

By Brian Scace



There has been a lot of discussion about "evangelizing" O Scale; by O Scale in this context, we mean scale modeling (including the number of rails) as O relates to the other scales such as N, HO, and S, and the presentation of O to the masses as a viable choice of scale modeling medium.

I have always held that the practice of taking tables at multi-scale shows and staffing them with people dressed in matching polos handing out literature and banging on figurative tambourines doesn't really get the message across in an inviting manner, and indeed, such fervor might actually put people off.

So, what alternative would be suggested? Importantly, one attracts more voluntary attention by actually having something to show, a lesson we can learn from such organizations as the Gauge 0 Guild. A small exhibition-style layout, several models and pieces of product on display, even a member of staff actually building something, a plastic car kit, hand-laying a turnout, whilst pleasantly answering questions during the process, all are extremely attractive and effective. One sees this approach at the shows in other countries, such as in the UK, and other venues, such as IPMS, and perhaps we'd do better by doing likewise.

A word of caution, though. Nothing shows the scale to disadvantage more than a couple tracks nailed to some bare planks populated by rough-looking stock that doesn't run well, or poorly finished, beat-up, ancient models on display, especially to young potentials who can look across the aisle at the N-track display or an attractive well-lit rack showing off the latest HO from the likes of Tangent or BLI, rather than look at what the stereotype of old guys with anvils are blacksmithing in the gloom. We can do so much better, can't we?

In looking at the pages of *O Scale Resource*, it's obvious we don't need another article about building a gorgeous contest-level boxcar, Geep, or Berkshire to display to the masses, and showing a nicely built turnout built of ROW castings, some current electronics, even a couple sticks of decent flex to show you don't have to hand-lay if you are more interested in modern control systems than pliers, none of this needs further mention other than the suggestion to actually show it live and in person.

A small display layout, though, might be fun to look at in these pages and, yes, easy to build. Not only are such things excellent platforms (when neatly presented and reliable in operation) to exhibit, the portable single-scene layout is an ideal first project to explore the scale and puts paid the notion that "O is too big for my space". Indeed, the specific project we are about to show was not meant for exhibition (though certainly designed for that purpose), but built for a good friend whose situation suddenly found him living in very constrained circumstances.

**The Givens and Druthers** of the design between exhibition and apartment living are very closely related, so this is very applicable to those for whom space is at a premium (why the design follows UK practice more so than the linear design that is the "go-to" in America) for whatever reason; easily transported, and easily assembled. The assembled dimensions are eight feet by 20 inches, easily accommodated on the usual six-foot rented show table with a foot overhang on each end and leaving enough table along the long edge for a small power supply. It breaks down easily into two four-foot-nominal sections for transport in a normal size SUV or station wagon such as a Subaru Outback with room to spare for locos and stock and other stuff you want people to see.

For those who envision this sort of thing for home use, each chunk is easily carried into something as small as an apartment and assembled with a minimum of tools. Even in the construction phase, there is nothing really



Photo 1



Photo 2



Photo 3

sloppy going on here. I built this one in my workshop between a couple old British sports cars without fear of bashing or spattering them, so your security deposit will be pretty safe with a little care.

Photos 1-3 show the basic construction technique I used for the two sections. Rigidity is a good thing so that the two halves assemble and disassemble very repeatably keeping both track alignment and the surfaces in plane with each other even on the crappiest church rental table. I built these using leftover bits from my home layout, hence the mismatched depth of the vertical plates, something that will be tidied up when we skin it all for appearance. Everything is glued and screwed (in my case using the concrete floor of the workshop as a surface plate) with attention paid to squareness in all three axes (x, y and z). Neurosis here pays off in the final product, as no layout large or small is better than its foundation.

That joint between the two sections is one of the keys to reliability, “reliability” being a word I cannot harp on enough. There are many methods to use, but my favorite is the use of finished and mated hardwood plates held together by a coffin latch, a very positive and repeatable mechanism operated with a hex-key. The assembly used here is made by a company called Kam-Konnect. The plates are screwed and glued to the underside of the plywood surfaces, and each come with either alignment pegs or matching holes to mate. A best practice is to lock the two together, then install on the two pieces of ply butted together. Photo 4 shows the installation as well as a couple of wooden runners



Photo 4

I added outboard of the plates for coarse alignment.

Photo 5 shows the initial assembly, in this case on a little four-foot plastic table. Even on that little surface, the assembly is surprisingly rigid, which is a good thing.

**On to the trackplan.** Again, the material came from my own stores, stuff I had accumulated for years combined with leftover material from the layout. I had quite a bit of Atlas/Roco flex to hand and a “mystery box” yielded six new left-handed Atlas/Roco switches. These things are really sharp in frog angle, but are cheap to buy and dead reliable. Because we are



Photo 5



Photo 6



Photo 7

building O in the same tiny area as the smaller scales, an area defined by the back of the car you haul it in, we'll accept the almost ludicrous sharpness of these turnouts and limit the switching trackage to end-cab switchers and forty-foot cars.

Our purpose, as defined in our Givens and Druthers, is well served with a separate eight-foot test/programming track in addition to a classic single-scene switching layout design, a run-around in the center with various spurs on either side.

A chunk of 1/2" Homasote roadbed handles the test track and some leftover 1/2" Homasote sheet does the same for the switching tracks. While the test track didn't take too much thought and was pounded in straightaway, the trackform of the switching section was laid out in situ, making sure none of the turnouts fall across the joint between the two sections. After the turnouts were brought into proper relationship (Photos 6 and 7) using a decent straightedge, the flex was laid in between. A straightedge was used to enhance the reliability of the result (there's that word again). Bear in mind, if this is going to shows, you are competing for attention with the smaller scales, scales admittedly better suited to the space constraints, so neatness counts as much as our natural reliability.

Photos 8 and 9 show the completed track-form. The run-around will easily accommodate a four-motor diesel or a forty-foot freight car and, because it is set on a diagonal, it's very space efficient. The design also avoids the reverse-curves of mating two turnouts in the manner of a crossover, enhancing your chances of reliable operation in this space, especially with these little turnouts. It's a really good track-form to have in your bag of tricks.



Photo 8



Photo 9

**Time to pull some wire, then.** Electrical reliability is the key, so the first thing I like to do is solder all the rail-joints which is not only the first step in a belt-and-braces approach to the wiring, but also enhances the robustness of the track alignments. Only then do I cut across the joint between the sections, paying particular attention to the security of the resulting ends as they match up over the disassembly joint.

Then, it's up on their sides as we string copper bus wires under the sections, stripped Romex I had sitting around, secured at each end of the sections with screw-eyes. Then, feeders were dropped from each section of rail, a section defined as bordered either by the end of a rail or at a frog. Don't rely on any internal current paths in the turnouts, just drop another feeder. I make notes with a marker underneath; it makes troubleshooting much

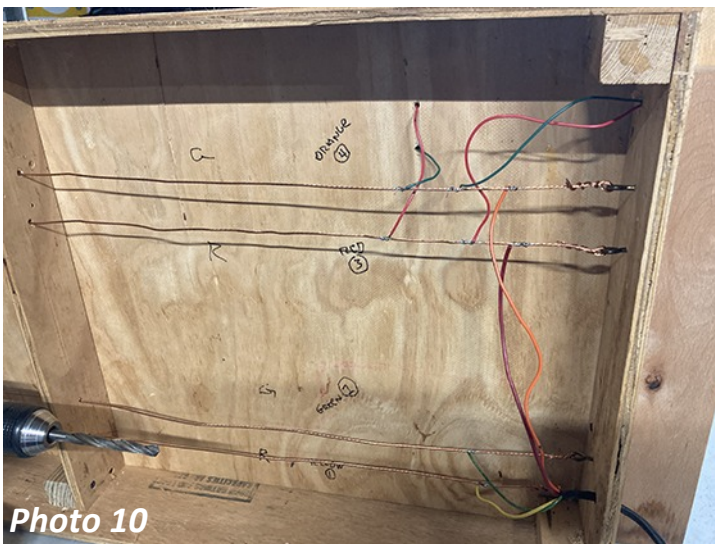


Photo 10

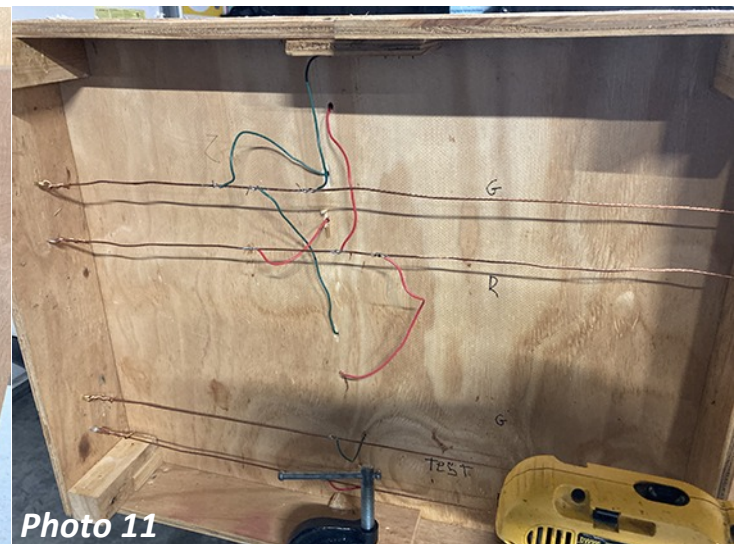


Photo 11

easier. Photos 10 and 11 (previous page) show the underside of one section; pretty simple, not really pretty, and extremely robust. I soldered everything underneath for (you guessed it) reliability.

Photos 12 and 13 show the four-conductor plug used to jumper the two sections; two conductors for the test-track bus and two for the switching trackage bus. Make it simple and robust; some shrink-tube adds neatness to the job. The wire colors as they correlate to the numbers on each plug-pin are noted underneath.



Now is a good time to test everything with a power supply, first the test track then the switching trackage, using clip-leads to the rails. Be merciless in your testing now, for everything you find is easier dealt with before scenery. A piece of 1" stock c-clamped across the ends of the trackage is good insurance rather than watching things fly off into space during a testing failure of some sort. We'll go back and finish off the wiring with power taps and routing switches in a few more steps.

**As I said earlier, neatness counts.** It's time to skin the exterior with a smooth and painted fascia. Again, we dip into the layout leftovers for material, this time a very thick and stiff cardstock used in theater scenery. Photo 14 shows some of it being applied after having been cut to size and shape with a fresh box-cutter. Masonite or the like also works well for this, but remember the goal is neatness.



There is a practical purpose to this as well. The edges down the sides protrude above the top by a half-inch and are sealed with a carefully applied bead of hot-glue. That will serve to protect the outside edge of the ground-cover after it is applied, keep it from flowing over the side in a great mess while being applied, and give the whole transition from structure to scenery a nice crisp edge. The fascia stands proud by three inches all the way across the ends to provide a stop, keeping expensive things from rocketing off the ends in case of a loss of control during tests or operation, especially with DCC. You'll also see the outline of a single track engine house, one of those ubiquitous kits

(marketed by Lionel, amongst others) pulled from the kitbashing stores and sawn in half to better fit the paltry space provided. The last feature of note is the “tunnel” at the edge of the joint between the two sections. It’s purpose is to let a hand in to draw up the coffin clamp after the sections are positioned. Again, the seams between the top and the fascia are carefully sealed to provide a dam, keeping all the glue and water up top where it belongs in the upcoming scenery phase.

**Now is a good time** to paint the sections, throw a sheet over the table, and crack out the brushes and rollers. I used a semi-gloss grey applied to the fascia with one of those little foam “hotdog” rollers. I brush-painted as much as reachable up top with the same paint, sealing up the lion’s share of the surface area of the Homasote. Photos 15 and 16 show the two sections after the grey was applied and before the track itself was painted with some Scalecoat I had on hand. The area covered by the engine house was painted black so the eye isn’t attracted to the inside after installation. You, of course, will detail that area in a most neurotic fashion so as to invite attention, I’m sure.

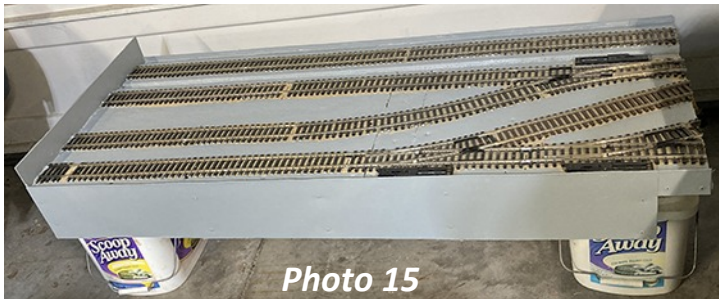


Photo 15



Photo 16

After painting the rail, I cleaned the tops (best done when the paint is dry, but before being fully cured). Power leads from both bus pairs were pulled through the fascia to the outside taps of a double-pole double-throw switch. The power supply leads were then brought through from the center taps of the switch out through the fascia and finished off with solder-lug spade connectors to screw to whatever power supply is to be used. The switch is a great huge knife-switch on a ceramic base I had lying around. Just looks very cool and ludicrous; I had to use it. The result is the ability to power either the test track or the switching trackage, but never both at the same time to avoid unwanted surprises.

Lastly, the structures were set and sealed with hot-glove. They become very solidly placed with this method, a nod to the fact this railroad is intended to be portable. The half-an-engine-house is also sealed to the fascia in the same manner, giving stiffness to both. Now is a good time to walk away for a week and let the paint cure.

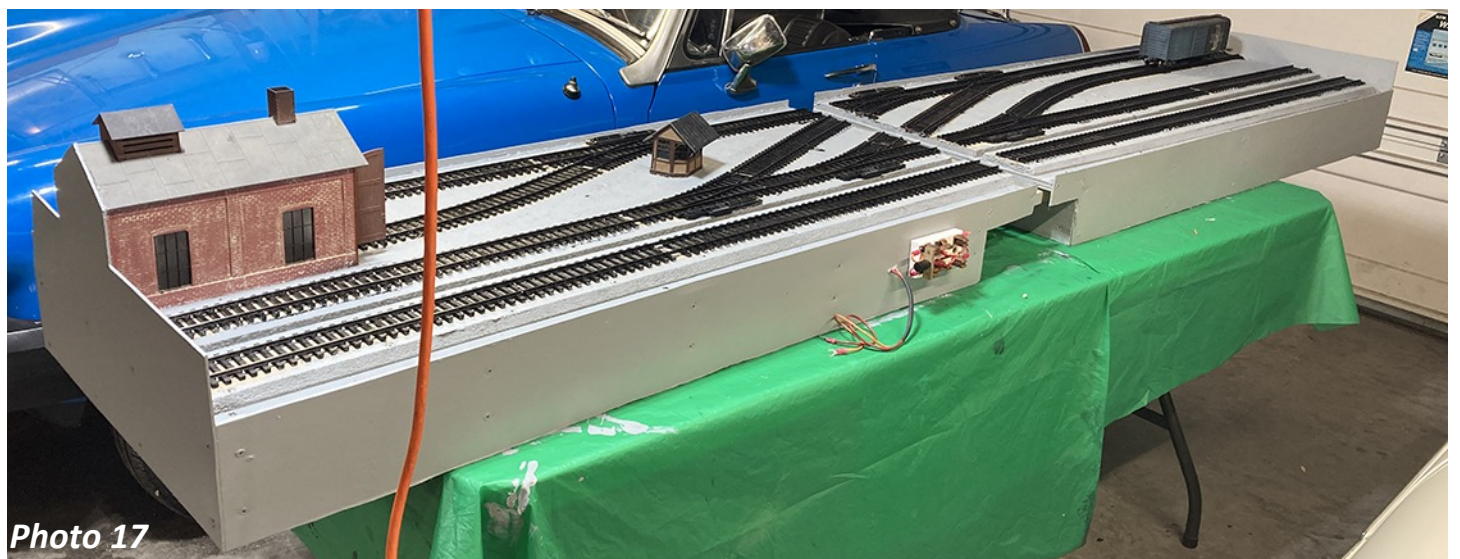


Photo 17





Photo 18

You'll need that mental re-adjustment period because the annoying task of troubleshooting the turnout mechanisms is at hand, mechanisms that really don't tolerate that rail-painting exercise with any feelings of mercy. It's your last opportunity to test track without ballast, so be thorough. Fastidiousness now is reliability later (Oh, that word again!) Photo 17 (previous page) shows where we are, ready for ground cover and ballast.

**It's the home stretch** as we give some thought to that pesky joint between sections again. Photo 18 shows the result after the application of basic ground covers, foams from Woodland and Scenic Express, and ballast. It's all applied in the usual manner, spread to suit, then wetted with a plant sprayer filled with water and a drop of washing-up liquid to break the surface tension, then well-soaked with 50/50 water and white glue/PVA. The issue is that joint and how we protect the innards of the latching mechanism, the mating faces, in short keep from gluing the sections together and keeping the scenic slop up top where it belongs. I sandwiched a length of styrene strip in the joint and ran the clamp tight against it. As you can see in Photo 19, it worked quite well. The sections parted quite easily and cleanly.



Photo 19

In the same photo, you get a good view of that obnoxious knife-switch. I make no apologies; O Scale overkill at its best!

That brings us to the state of affairs in Photo 20. At this point, the basics are done, some coarser ground covers added especially around the structures, and all the trackage is cleaned and tested yet again. Time to add some stock, which you will have to ensure are tolerant of those frog angles with good wheels in gauge and enough truck- and coupler-swing clearances.

**A word about weight and handle-ability is warranted.** I really made no efforts to reduce weight to a minimum as a design requirement, but used leftover materials anyone might have to hand. The deck is 1/2" five-ply, 1/2" Homasote for the track surface, the structure underneath 3/8" three- and four-ply as was lying around. I also erred on the side of stiffness, neatness, and robustness rather than lightness. The resulting sections in the state you see them are light enough for one person to handle. If anything, they might be a bit clumsy though, so I added crate-handles, staggering the opposing ones on the sides so each section is easy to balance by the person handling it. The handles on the end are convenient for pushing the sections together and taking them apart. You naturally gravitate to them, rather than reaching underneath only to catch fingers between section and table or snag a fingertip on a protruding screw. Those handles are time and treasure well-spent.

Here is where I shall leave you to detail, add trees, figures, and all those little gubbins and gubitzes we love. Perhaps you'll clamp a back scene to it, everything that makes this a personal expression and certainly not what I can tell you to do at this point. Meanwhile, should you be one of those guys in matching polos at the next multi-scale show, you'll find such a thing a real magnet. People will be back if only to test prospective purchases on the test-track you and your organization are so thoughtfully providing and staring again at all the other models you have on display.

Hand 'em a throttle because, not only it presents well, it's (All together, now!) reliable.





*Here is the layout loaded up to go to its new home.*

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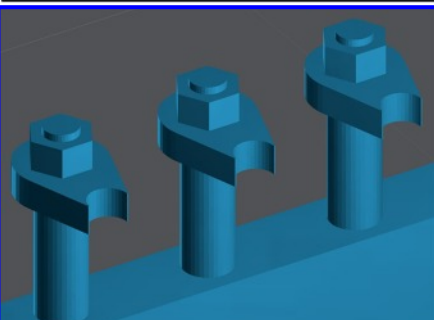
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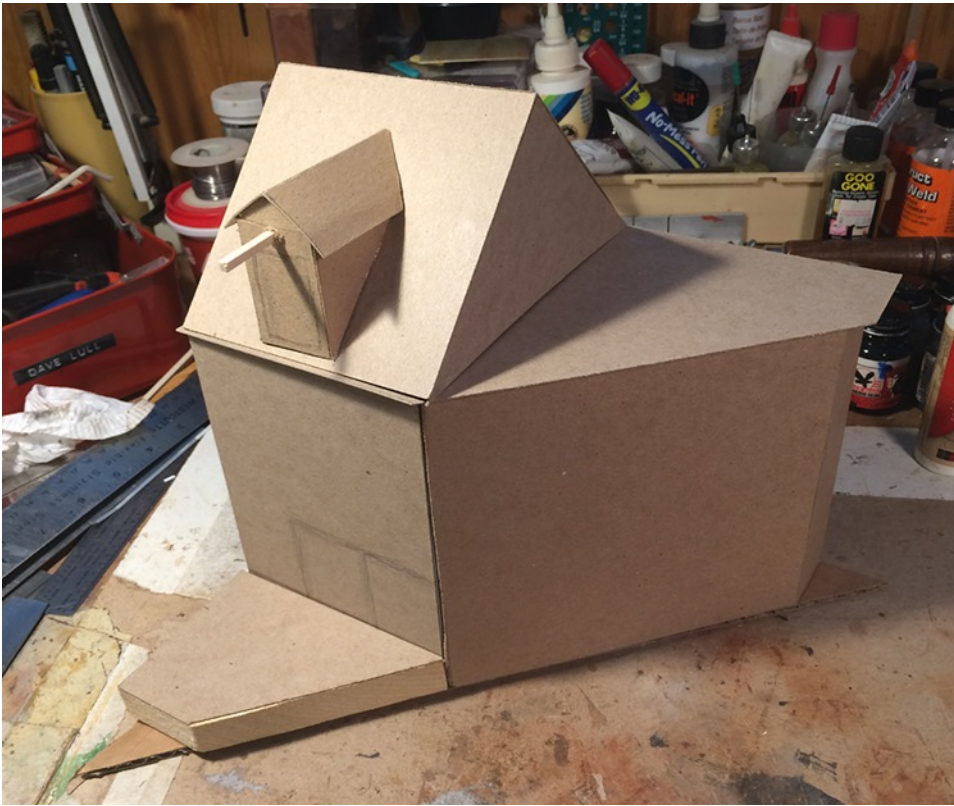
AN O SCALE SCRATCHBUILT STRUCTURE FOR A TRIANGULAR LOT

By Dave Lull



A client who models in On30 in the early 1900s era gave me a triangular piece of cardboard with some markings and said he would like a two story warehouse made to fit the space. After some questions, I decided to make some sketches and mockups in card to get a better idea of what he wanted. He agreed on one which was basically a square building with a triangular addition. The addition had a flat roof and the main part had a simple peaked roof with a dormer for a block and tackle lift. The remaining triangular area was to be a loading dock. For a foundation it was to sit on timber posts.

I began by marking out the overall space and making a grid of 1/4" square basswood strips for the main joists. I cut a subfloor of 1/32" basswood for the building itself. For the loading dock I cut floor joists (roughly scale 2x8s) to go on top of the main joists. I built the actual floor with 1/8" x 1/32" basswood strips prestained various colors and glued these across the subfloor and joists as the actual floor planks. The owner wanted to light the building and possibly add some interior detail, so I opted for the planks rather than scribed sheet to allow for the varied coloring.



The walls were made with 1/8" sized (1/16" thick) clapboard basswood sheet. After locating and cutting the window openings, I laminated the wall to 1/16" thick plain basswood and cut the openings in that piece. The windows were 3D printed. I then stained the walls using a Minwax oak stain first and a second coat using a driftwood stain. I used a natural colored Danish oil stain on the inside wall surfaces. I painted the sign using an adhesive vinyl mask I made for the lettering and spraying through with a black primer. After installing the windows, I assembled the walls using 1/4" square basswood strip for the inside corner supports and 1/8" square basswood strip for the outer corners. I added a number of vertical and horizontal supports to avoid warpage of the walls, and especially the cross members between the long walls to keep them the right distance apart for the roof, which was to be made removable.



The main roof is made using 1/32" thick basswood planks 1/4" wide (although later I realized I could have just used wood sheet or cardboard) covered with strips of black construction paper. The flat roof was made the same way. I built a cornice around the flat roof with wood strips and drilled a drain hole. I then added a scupper near it and a downspout. I added an access hatch to the flat roof and a simple pipe stack to the main roof. The dormer is for the block and tackle arrangement; I made a simple platform with railings for handling it.

The second story door just has a platform. I added the second story extension for interest, and used the same wall and roof techniques, with scale 4x4 supports.



The block and tackle, wall fan, door hangers and details were 3D printed. I weathered the roof and dock floor with gray pastels. I lightly ran white pastel stick over the roof to highlight the texture. A light brushing of pan pastels over the walls helped age them and make them a little dirty and dusty. I gave the model a light overspray of clear matte, which is recommended if a structure that is pastel covered is handled, brushed or vacuumed.



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# A New Container Business for the Mountain Electric

By George Paxon

While surfing the net one day I had a look at photos of some old rail container operations. A while back we had scratch built a model of a Cincinnati & Lake Erie traction flat car and its unique containers (See **Photo 1** for my model and **Photo 2** for a shot of the prototype.) The prototype photo is from the Indiana Historical Society, who have the Cincinnati Car Company archives, and used with permission.



An idea began to form in my head for a small container terminal. Not too long ago we wrote an article for *The O Scale Resource Magazine* about a package express car for my Mountain Electric Ry.

Well, the story goes that the package and less than carload lot (LCL) business has been so good that containerization has become imperative to further reduce labor costs on the ME Ry when handling some of this LCL freight.

My initial thinking was that the C&LE container car could be spotted at the container terminal for loading and unloading. And we should also build another traction flat or gon which could bring steam railroad containers to the terminal as well. Our Mountain Electric interchanges with the Baltimore & Ohio at Somerset, Pennsylvania, and the steam road containers could be transferred from steam road cars to ME Ry cars there and then moved over the ME Ry to the terminal at Jacobs Creek.

From these initial thoughts, a new industry was spawned for my town of Jacobs Creek, and the ME Ry was in for some more revenue.



In the early days prototype LCL freight could be rather casually handled by both steam and traction lines. See the unloading operation on the Leigh Valley Transit near Philadelphia, Pennsylvania in **Photo 3**. This view is from the Rockhill Trolley Museum and used with permission. Containers quickly came into favour and were used to package and consolidate LCL freight as much as possible. The first containers were crates, sacks and barrels. Considerable labor was still needed to move these containers from shipper to train station, station to freight car, often car to car, car to station, and then, destination station to receiving

customer. By the 1920s, rising labour costs required better ways of handling LCL freight. Several steam railroads experimented with larger containers.

Early containers were wooden and later steel. The New York Central developed a bottom dump container for hauling bricks. Previously, bricks were tossed into box cars, loaded and unloaded by hand and wheel barrow. The brick containers reduced the unloading time for a car of bricks from several days to several hours. And a 50 foot long gon with containers actually carried almost four times the number of bricks as a 40 foot box car had due to the haphazard and partial loading!

Cement was a big product from eastern Pennsylvania, and it needed protection from the weather in transit. There were no covered hoppers then. Containers were developed for that commodity. Before bulk goods containers came along, cement was shipped mostly in bags and barrels in box cars and loaded and unloaded by hand.

Containers were also developed for coke, merchandise, and even refrigerated products. Some railroads jumped at the opportunity to reduce costs of labor in handling and transferring LCL freight and reduce the cost associated with loss and damage of loose freight. They developed special cars to transport the containers. Most were rebuilt flats and gons.



One enterprising firm, the LCL Corporation, provided several thousand containers for this traffic. **Photo 4** shows a B&O gon loaded with the quasi-standard LCL Corporation merchandise containers.



But there was little planning and even less standardization across the rail industry. Each type of container required a unique car type to move it. The Pennsy, in typical Pennsy fashion, rather than settle for the somewhat standardized rectangular container furnished by the LCL Corporation, and used by many eastern railroads, developed a merchandise container of their own that could not be accommodated by cars of any other line. See **Photo 5** from the Altoona Association of Model Railroaders, courtesy of William Burket, and used with permission.

**PHOTO 6**



- Above is one of our
- modern cars specially adapted
- to transport freight in twelve
- removable protective containers.

The New York Central, always a rival, and not to be outdone by the Pennsy, also developed their own unique merchandise container, it would seem. And, to just keep things interesting, the NYC container cars were built from ex-baggage cars or express reefers. To make matters even more complex, both the Pennsy and NYC also used some of the quasi-standard LCL Corporation containers as well. And, as we saw above, some interurban lines jumped on the band wagon as well and developed their own peculiar containers and container transporting cars as the C&LE car illustrates. Some smaller steam roads even got into the container business as you can see in **Photo 6** where my favorite steam line, the Pittsburgh & West Virginia, had containers and container cars! As far as I can tell, this early U.S. container business was concentrated in the northeast. And, it was probably best considered a failure due to the lack of standardization.

Down here in Australia, they also had early rail LCL containers. And, as you can see from **Photo 7**, even though the rail network was small and consisted of only a very few different rail systems, they also had a problem with standardization. Photo is by Brad Hinton and used with his permission.

**PHOTO 7**



So, before the rail industry could sort itself out, along came the fork lift, the pallet, larger and more powerful trucks, etc, all of which evolved into the 20, 40 and 53-foot intermodal container business we know about today. This technology was developed by the ocean shipping industry while the railroads had their collective and parochial heads in the sand. Later, the rail industry just worked out methods and cars to load and move the

**PHOTO 8**



Photograph by J. Baylor Roberts

ocean containers on rail. And the LCL aspect really disappeared as the ocean containers could hold car load quantities. Also, any LCL freight was consolidated into the larger containers by freight forwarders long before the railroads got involved. But we model the 1930s and are interested in the early rail containers.

And, since space on my layout is always a premium, industries need to be small enough to model and/or they need to use layout space effectively. This objective is extremely critical for us O gaugers as everything is big! I also lean toward quaint structures that remind me of those I saw in my early years. I also like to keep my models plausible as to period, product, geographics, etc. Before charging off and building or buying cars, locos, buildings, I try to ponder how it would fit into the scheme of things for my layout.

With these objectives and constraints in mind, we set off to design our container industry. Further research into early containers, and the container handling business, helped develop the background for our new industry and flesh out the idea further.

My next thoughts turned to the terminal itself. My initial idea was to keep it to an area approximately 10 inches square. This would suit as it will take a standard 40-foot car. The plan was to have an overhead crane inside a roofed but wall-less structure. Many such prototype cranes would have run along ground mounted rails to access several to many rail cars. See **Photo 8** by J Baylor Roberts, of a very large Pennsy container terminal.

But in the interest of economical use of space, we opted for a crane with relatively limited travel that could service only one car. We justify that for our country traction line where traffic will be limited to receiving only one car at a time. On the ME Ry, the containers would be craned from the ME Ry car to a truck for delivery to local customers. To support this scenario, we can have some containers stored on the ground awaiting a truck or a rail car to take them further. We would also need a roadway through the terminal and under the crane to enable loading the containers onto trucks.

Some containers could be unloaded while remaining on the ME Ry car. This was often the case with the cement containers in the prototype world. An air compressor was used to pneumatically convey the powdered cement from the container into a waiting vehicle via a large diameter hose.

Another location consideration was that the terminal must be located at the end of a siding. The overhead wire needed for traction would interfere with crane operations and the overhead needed to be terminated at the point where the siding reached the terminal. We try to provide a few industries on each siding to make better use of trackage. Having this at the end of a siding would not present a problem.

With the above ideas rattling around in my mind, it was off to the drawing board to sketch out the container terminal ....

### **Building the Jacobs Creek Terminal**

The basic structure planned was conjured up as a combination of structural steel and wood. The uprights and crane rails would be of structural steel construction and the roof trusses would be of wood. No real reason for this – just what could have been.

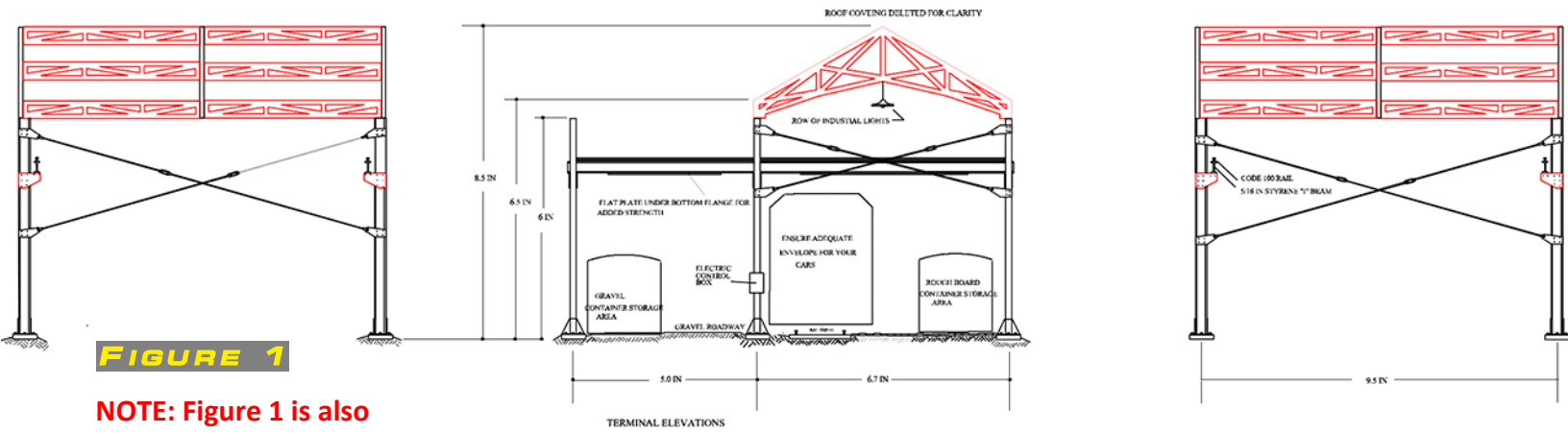
When working out your basic dimensions, be sure to take into account your clearance gauge, the envelope for the car, on the rail siding. My dimensions suit my traction cars and could be smaller than yours if you are into larger steam road or more modern cars. The track approaching my terminal will be straight. If the approaching track was curved, additional clearance might be required.

The sketch of the terminal we conjured up is shown in **Figure 1 on Next Page**.

In the prototype world steel uprights are columns and horizontal members are beams. Since Evergreen Styrene did not differentiate at one time neither did we. Now they make both “I” beams and “H” columns. The uprights we made of 5/16-inch styrene “I” beam material. It would be more correct to use the new “H” material for the uprights, but we did not have that luxury. At a scale 15 inches this is quite realistic. As you can see from the drawings, we made craneways and brackets to attach to the uprights. Diagonal rods stiffen and keep the structure square and are located where they will not interfere with rail cars, trucks and the crane. The uprights sit on concrete pads with a bottom plate and gussets as shown in the drawings.

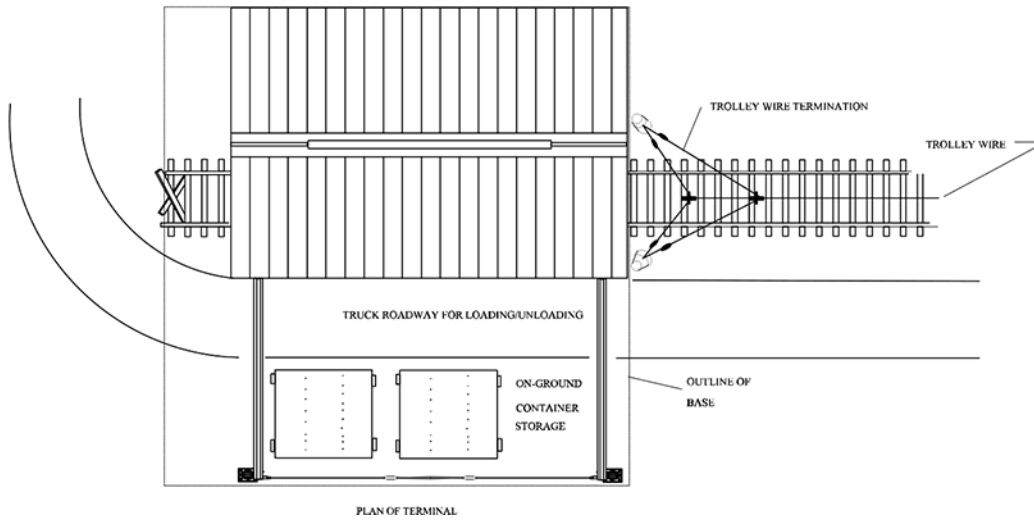
As the shed is open and can be easily viewed, I suggest you detail the trusses. We cut ours from 1/8<sup>th</sup> inch thick material. You could build the trusses up from styrene also. To model the steel splice plates used to join together the wood beams, cut them from thin styrene. Stain or paint the wood trusses and paint the steel plates a black. Then glue the painted plates over the joints. You can simulate bolt detail on these joining plates. One way is to drill through the plates and add nut-bolt-washers and touch up the paint with weathered black. As an alternative, you can press rivet-like impression into the plates.

Once the trusses are detailed, assemble the roof structure. See the exploded drawing, **Figure 2 (on next page)**, for our trusses. Install the roof purlins of .060 x .188 styrene or stripwood. Deep purlins are needed due to the long spans between the trusses. Pre paint or stain the purlins before adding them.

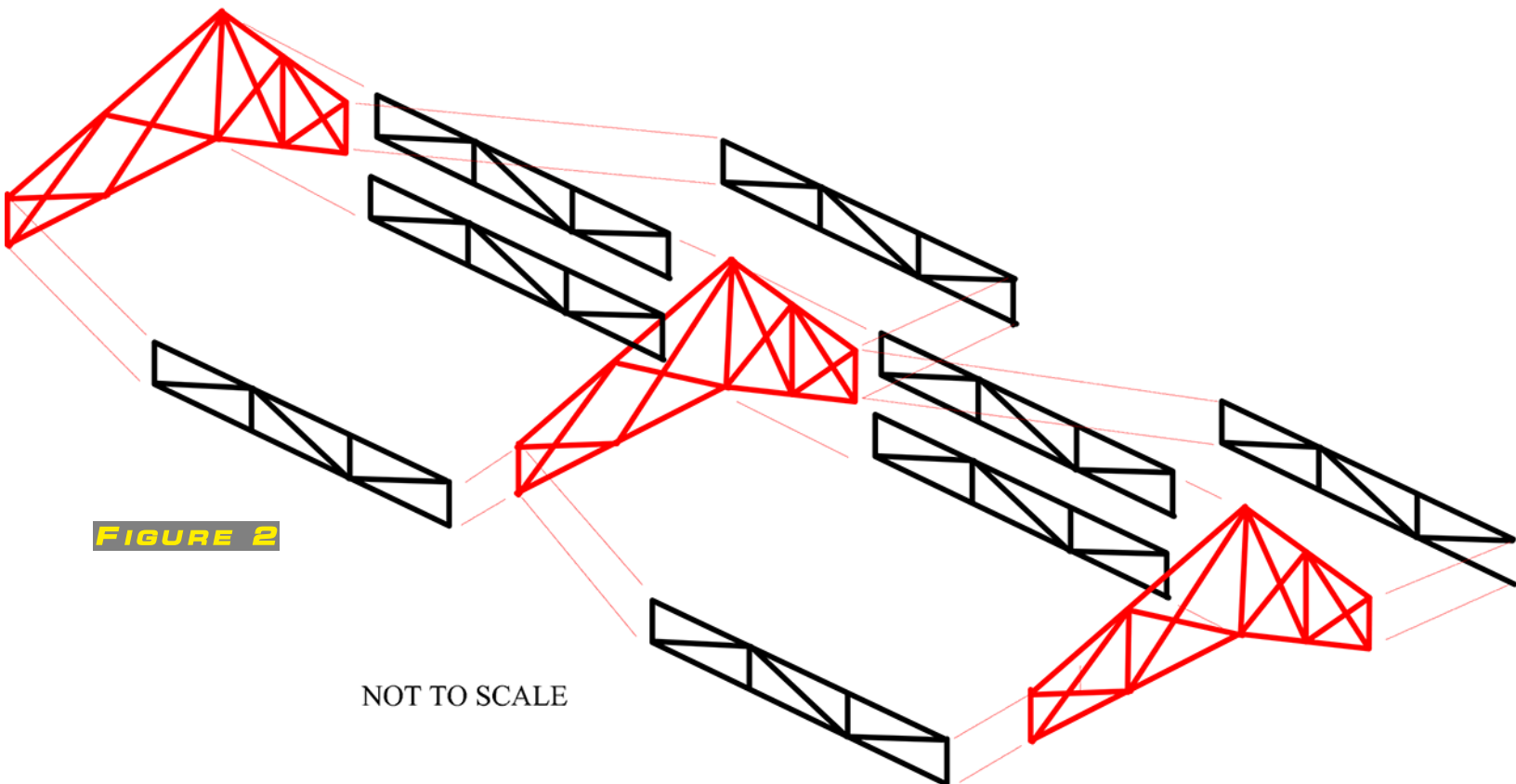


**FIGURE 1**

**NOTE:** Figure 1 is also reprinted at the end of the article in a larger format.



**Figure 2 TRUSS CONSTRUCTION**



**FIGURE 2**

NOT TO SCALE

**PHOTO 9**



**Photo 9** shows the roof structure at this stage with trusses assembled and purlins in place and ready for roof covering.

Now is a good time to install lights under your roof if you are planning to have them.

We use LEDs after growing weary changing burned out bulbs. A 20 thou diameter, brass wire was installed down each side of the center truss as a 3 volt and ground bus, and connected to the LED in each industrial style lamp to the wires. I bent the ends of the LED leads into a small hook, hung them over the wires and then touched them with the soldering iron. Nice neat way to route your light power. Be careful to make sure your lights are high enough to clear the crane. With LEDs remember to keep the polarity correct. I made shades/reflectors for the lights from disks of styrene drilling two holes in each to pass the LED leads through.

Two supply wires were added by soldering them to one end of each of the two brass bus wires allowing about 24 inches of length for routing the wires to the corners of the roof and down the back of two columns to the ground. A varnished copper wire was used for this as it has a much smaller diameter than plastic insulated wire. This wire is often called magnet wire. This will provide a less conspicuous installation. Run the wires separately to avoid any shorts should the varnish insulation wear off or become damaged.

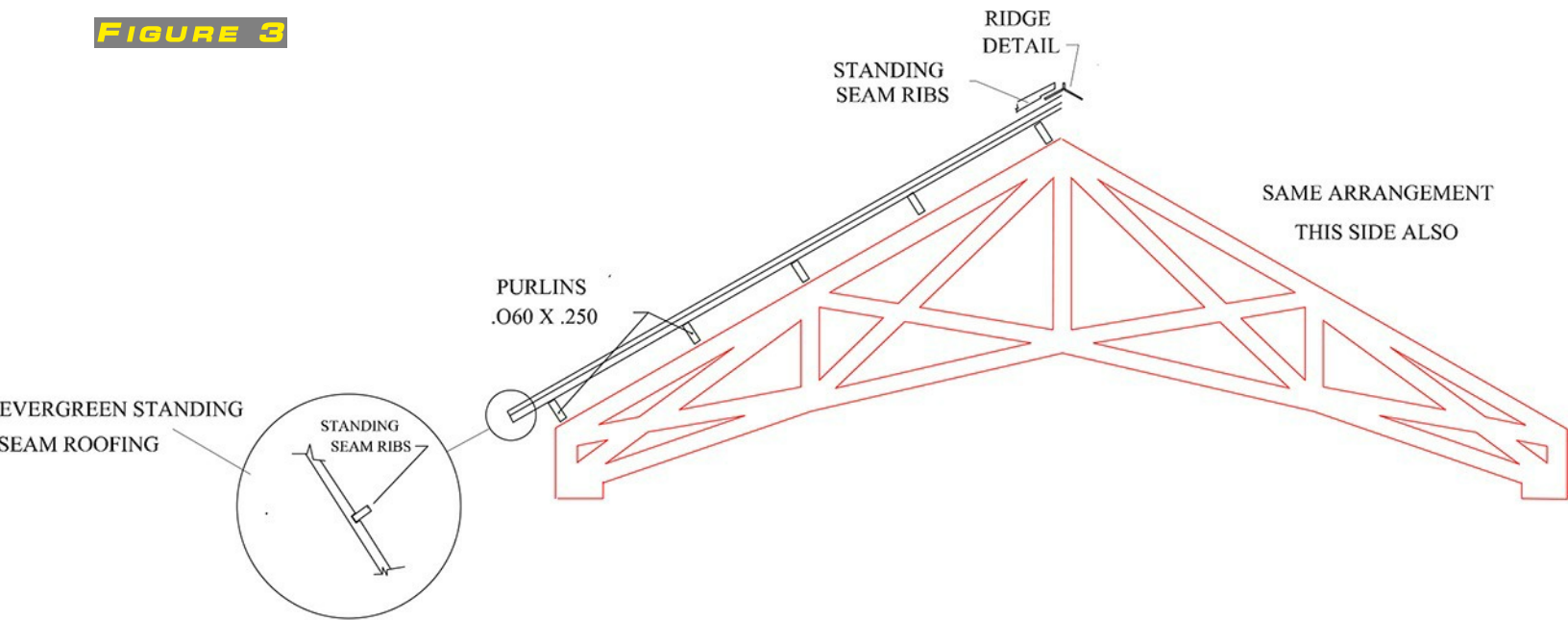
When installing the structure on the layout the fine wires will be joined to heavier wire just as soon as they go under the layout to reduce voltage drop.

Do run an electrical test at this stage to ensure all LEDs light and that none were installed with the polarity backwards. It will be a frustrating and unrewarding experience if you need to change an LED after the roof is installed. Paint over the brass wire where it might be seen with a suitable shade of grey after testing to disguise it.

Now for the roof covering. We used Evergreen styrene sheet to simulate standing seam steel roofing. You could use corrugated sheeting or whatever roof surface you prefer. If you are working with styrene sheet, it will probably be best to paint the underside of the roof a dull grey of some form as the white plastic would be too bright and unrealistic. The underside of the roof will be quite visible. Since our roof was standing seam we had to install all the seams from 0.010 X 0.030 styrene strips. A ridge cap was formed from some 0.010 x 0.250 styrene strip and round rod. **Figure 3 on next page** shows our roof construction.



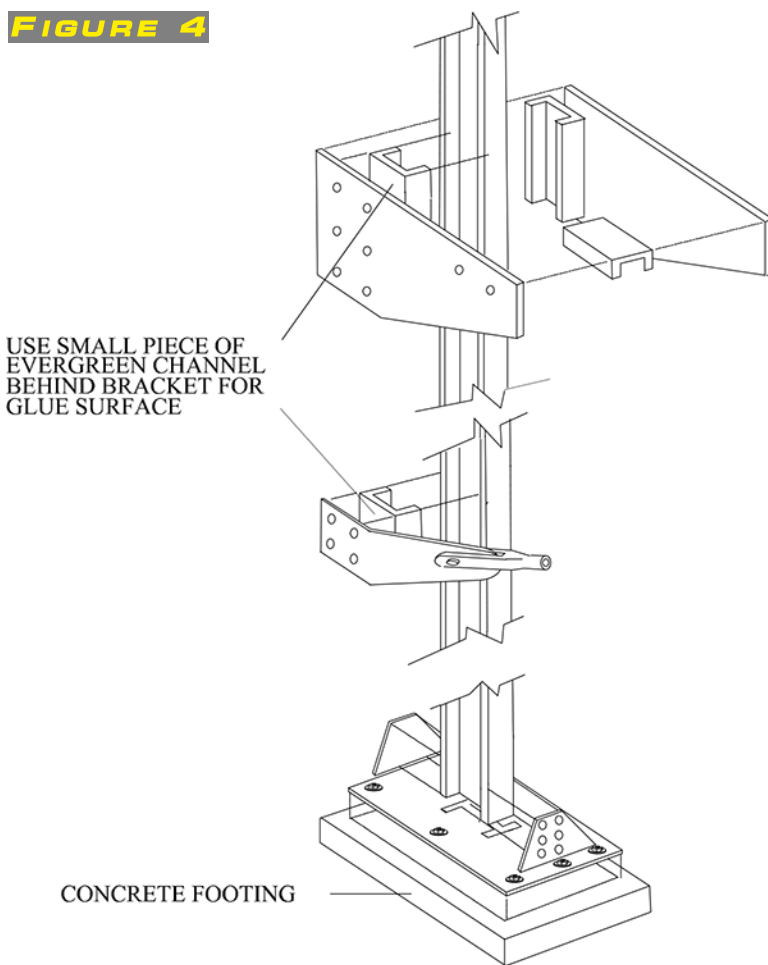
**FIGURE 3**



When all the steel columns are assembled, paint them. Structural steel was often painted either a black or a red, like freight car red, in the good ol' days. Yours can be whatever color you like. I painted mine a weathered black – basically black with a bit of white added. I rarely use straight black even for locomotives. Black is too dark and hides detail. By adding a little white and making it a very dark grey, your models will stand out better. Ever notice that even when prototype locomotives were photographed at the Baldwin or Alco factory they were painted grey so the details of the loco would show in the photo. Cut the concrete footings for under the steel columns from some 1/8<sup>th</sup> inch thick MDF, wood, or styrene and pre-paint them concrete color. When dry, glue them under the columns.

**Figure 4 Steel Column Construction**

**FIGURE 4**



NOT TO SCALE

Make an electrical control box. This would operate the crane and also house the switches for the lights. In modern times a pendant hangs from the crane and an operator can walk along controlling all the crane functions. I suspect in the 1930s crane operation was not so flash, however. Paint the box a drab green or medium grey so you have some contrast with the black column. These were common colors for electrical boxes. After the building is assembled you can install the electrical control box on one of the columns making sure to keep it clear of the rail car envelope. The top of the box should be about head high on a scale figure.

We build almost all our structures on a base of MDF, chipboard, or plywood, whichever is on hand and about the right size. Some of our structures have been on many layouts on two continents and some have survived for fifty years

now. The strong base provides a good footing for the structure and protects it to some degree. And, if the base needs to be set lower on the layout, the plan has always been to just cut a hole for it. From the sketch in **Figure 1**, we worked out our base for the container terminal needed to be 12 X 12-1/2 inches. This was bigger than the original objective, but this sort of thing happens. We cut the base from some chipboard I had on hand.

## Assembly

With the roof and steel columns done you can begin assembly of the building. Lay the ties and rail for the siding on the base. We extended our siding about two inches or so beyond the end of the terminal to allow some room for over-running the car spot location. Later we can add a bumper made of old ties when the model is worked into the layout.

At this point, we worked out exactly where the six steel columns will be and marked them out. Sit the four main building columns in their approximate positions and place the roof on their tops. Shift the steel columns as needed to get them correctly under the roof corners and test with a small square to ensure they are plumb. When satisfied, mark around each of the four corner columns. Run lines out from the four main columns and mark out where the 5<sup>th</sup> and 6<sup>th</sup> columns will sit.

Add the rough boards for the container storage area next to the tracks. We stained some stripwood from our scrap box and some strips selected from the wood hoard. Use several widths and several thicknesses to get a nice rough look. If you can't find some wood of various thicknesses, sand down the thickness of some of your stock. Run the side of a razor saw along the top surface of the strips to rough it up and add wood grain. Cut some bits off edges here and there. Touch up the stain. Cut to board lengths and cut some short to make some joints. Glue down the boards and then add nail holes to indicate that these boards were fastened to supporting timbers below. Such a deck would be rough, so try to make yours that way, too. We use a straight edge and a machinist's scribe to add nail holes. Rub some black and/or brown chalk into the boards after and it will help accentuate the nail holes.

Install some cardboard where the roadway will run to help better define the road. We put a ridge down the road center made from a few layers of thin card cut into narrow strips. Also, we built up the area adjacent to the roadway a little. The plan was to make the crude roadway appears as two worn tire tracks in the dirt and gravel.

Paint the entire base, except where the columns will sit, with your basic scenery earth color. We usually do this in a two-coat approach. The first coat of earth paint soaks into wood and card very quickly. Then we go back later and apply the second coat and immediately sprinkle on the fine sand, grout, gravel, sawdust or whatever is appropriate for road and ground surface. We do small areas at a time. The second coat will not dry as quickly and will bond the material better. When dry, use more sand, grout or gravel to better cover the road and storage area and spray with wetted water and use white glue or matt medium to bond this material. Add other scenic ground covers of dirt and grass, etc., to the edge of the base outside the boundary of the rough board storage area to fully cover these areas. And, as before, use the wet water spray and glue to bond the scenery in the areas.

Work ballast, dirt, gravel, or your choice of ground cover to the track. My Mountain Electric uses a lot of power plant cinders for ballast, but we used dirt and gravel here for contrast. Some weeds and grass were planted here and there and we will add some more around the bases of the columns once they are in place. Plant some weeds and grass on your railway track, too. This is good particularly toward the end of the siding where they would be less disturbed by the passing of rail cars and work boots.

A material that makes good "earth" is tile grout. I have ground and/or sifted clay from our yard and used it, but if applied too thick it will crack when dry. Grout comes in many shades and does not crack. One caution – grout contains cement which presents a minor problem if used around track. Cement can get into gears and

wheel bearings and wear them out. Grout, like any cement or plaster product, should be well glued and sealed so it cannot come loose and migrate into gear trains or wheel bearings of passing locomotives and cars.

I find it best to do scenery in a few stages. I am not very artsy-fartsy and need to work harder on the more artistic modelling tasks. I put on some scenic material then let it sit a while and go back for another look. Sometimes I just add more, and sometimes I need to remove and rework the area. Although I don't consider myself good at scenery, I do find it a nice relief from the accuracy needed for track, and car construction. Sometimes I get my domestic manager to come look and give an opinion as she has a better eye for color and placement of things than I do. Doing much of the scenicing now will be easier than working under the roof and truss rods later.

With the preliminary scenery on your base dry, you can set the four main columns in place. Do this one-by-one, gluing them down to the base and testing as you go to ensure they align correctly under the corners of the roof structure and are plumb. Drill a very fine hole behind the marked-out space at two of the back columns that will carry the thin wires from the lights.

We did not attach the roof permanently to the four steel columns at this time. It will be easier to install the crane first.

Add each of the other two orphan columns that will support the extension of the crane rails making sure they are in a straight line with the other columns.

## Craneways

Cut two lengths of 5/16 inch high Evergreen "I" beams as the supports for the crane rails. We will call these the craneways. Well, that was my plan anyhow. When checking my Evergreen styrene hoard we found it was short of 5/16 beam material. We had to make one beam by gluing together two 5/16 channels back-to-back. This beam was a bit wider than the other, but by putting it in the far side of the structure, the difference will be hard to spot. Running out of materials is one of the hazards of living 10,000 miles away from sources of supply. It is not the first time it has happened, believe me. You learn to improvise down here.

For a bit of extra detail, you can add stiffeners to the bottom flange of the "I" beam craneways. Often a steel flat bar section was welded, or riveted, under the bottom flange to increase its ability to carry the crane load and decrease deflections. Not essential, but an extra bit of prototype detail is always nice. Cut two lengths of code 100 rail the same length as the craneways. Paint the "I" beams with the weathered black and the code 100 rail with a rail brown. Glue the rail to the top of the "I" beams. You can now glue the crane rails to the six brackets, one on each column, provided for this.

**Photos 10 and 11** are of work in process on our terminal. We placed the containers in one photo just to see how it was going to look. It is coming along at this stage and starting to look more like the plan.

With the craneways in place, the structure has sufficient rigidity to install the four sets of truss rods and turnbuckles. I purchased some large-scale clevises and turnbuckles from Ozark Miniatures, a supplier to our garden railway brothers, with the intention of using them. The turnbuckles actually looked too large. After some desperate searching in our parts hoard, we found some Simpson plastic turnbuckles made for 1/2 inch scale which were smaller than the Ozark Miniatures ones. These were used instead. We did use the Ozark Miniatures clevises though. These larger scale parts are nice for bridges and structures where larger clevises and turnbuckles would be more appropriate than the smaller ones we use under a freight car. Some 0.040 inch diameter wire between the clevises and turnbuckles will be about right.

When fitting the truss rods, we suggest you sit your roof in place on the post tops. The posts are still quite flexible and it would be easy to push or pull the tops out of plumb when installing a truss rod that was slightly

too long or too short. If this happens, when you go to install the roof later, the roof corners may not line up with the posts. And, don't ask me why I know about this issue...



PHOTO 10

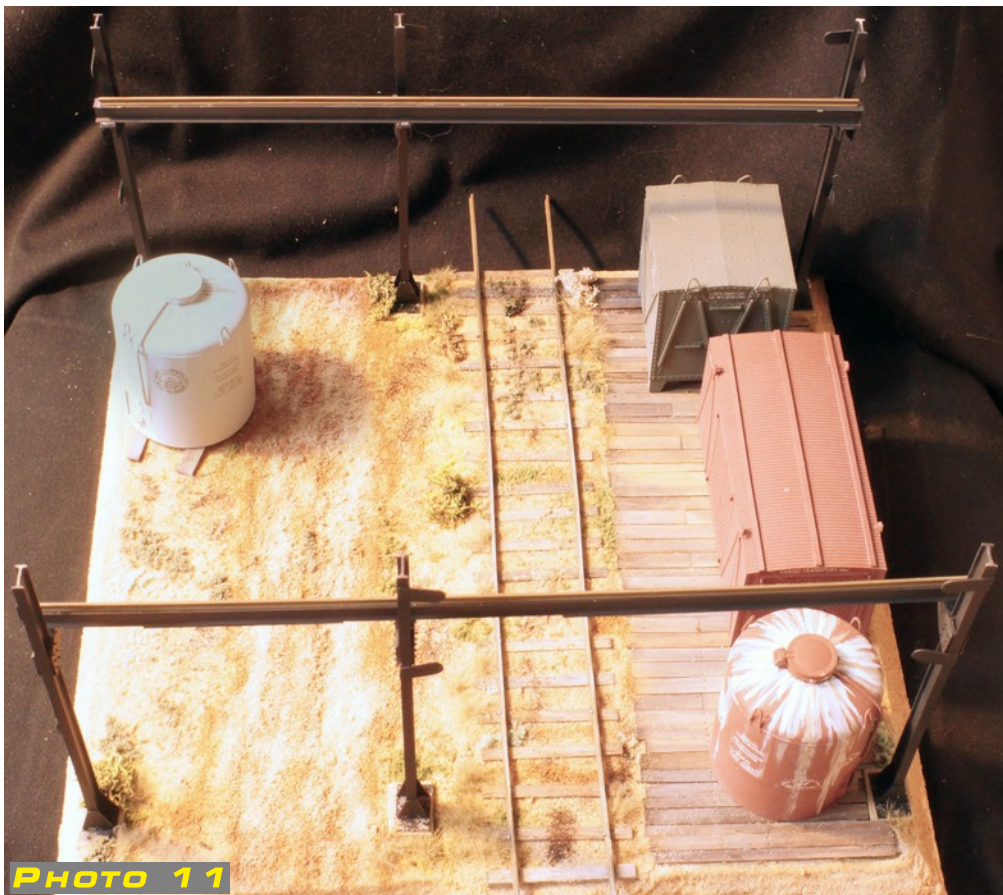


PHOTO 11

With the roof temporarily in place, we measured the length of each truss rod, made it up from two clevises, a turnbuckle and two lengths of wire. Don't put your turnbuckles in the center of the truss rods as they will interfere with one another where they cross. Offset them from the center. After a test fit, we pre-painted the assembled truss rods with the same weathered black used previously on the steel work. When dry, each truss rod was glued in place. Do this for all eight of the truss rods. When the truss rods have dried, you will find the structure has considerably more rigidity.

## The Crane

A word about our crane – it is a caricature of a crane at best. We cobbled ours together from some styrene, Crow River Products O-44 and O-56 winch kits, and some On3 wheels. We used wire for the four hauling ropes so they would remain straight. The hooks will be castings by Precision Scale Company and I'll install them if they ever show up in the mail!

By the way, if you have not worked with any Crow River Products items, you should give them a serious look as they are very nice.

We decided to make our basic crane color yellow for contrast to the drabness of the dark structure. Down here most such travelling cranes seem to be painted yellow. I am not sure what the prevailing color was in the US in the 1930s, but yellow will do for me. You can have yours pink if you so desire.

With the roof in place you will not be able to see much of the crane unless you stand on your head. What is most important is that the viewer understands the crane is there. We just wanted an assortment of thrashing gears, a motor, etc., to create the look of this sort of machinery would have in the 1930s. You could spend a modelling year doing a perfect scale model of such a crane. But, you really can't see much of it, and as we have other fish to fry, chose not to do so. In the real world such a crane would have three drive systems. One would move the entire crane bridge along the rails on the craneways. Another would move the trolley side to side along the length of the crane bridge. And the third would operate the hoisting ropes. The fellows on my Mountain Electric Ry are so clever they get by without all this non-sense though and use one drive system.

We painted our crane as parts before assembly. The non-machined and non-meshing parts of the gears were dirty yellow, but the electric motor I did in a contrasting color. All the shafts and gear faces I left unpainted to represent clean work-polished steel.

For the wheels that roll along the rails on the craneway, I pulled some 26-inch dia On3 wheels from the axle and used them. I painted them, except the treads, a dirty black-brown and pinned them in place after the crane bridge was painted.

When our crane was together, we gave it some overall weathering. The "SAFETY FIRST" signs were added to both sides of the crane bridge. You see this on lots of cranes and it makes an extra a bit of detail. Not sure when they started using such a slogan, but on my railroad it started in the 1930s. Modern cranes are required to post a safe working load on the crane bridge as well-such as "Maximum Load 10 Tons", but I think this all started long after the 1930s. **Photo 12** shows our crane bridge assembly before installation in the container terminal. You can see that the hoisting hooks are still missing. I'll close the loops in the wire and paint them rust once the hooks are in place.

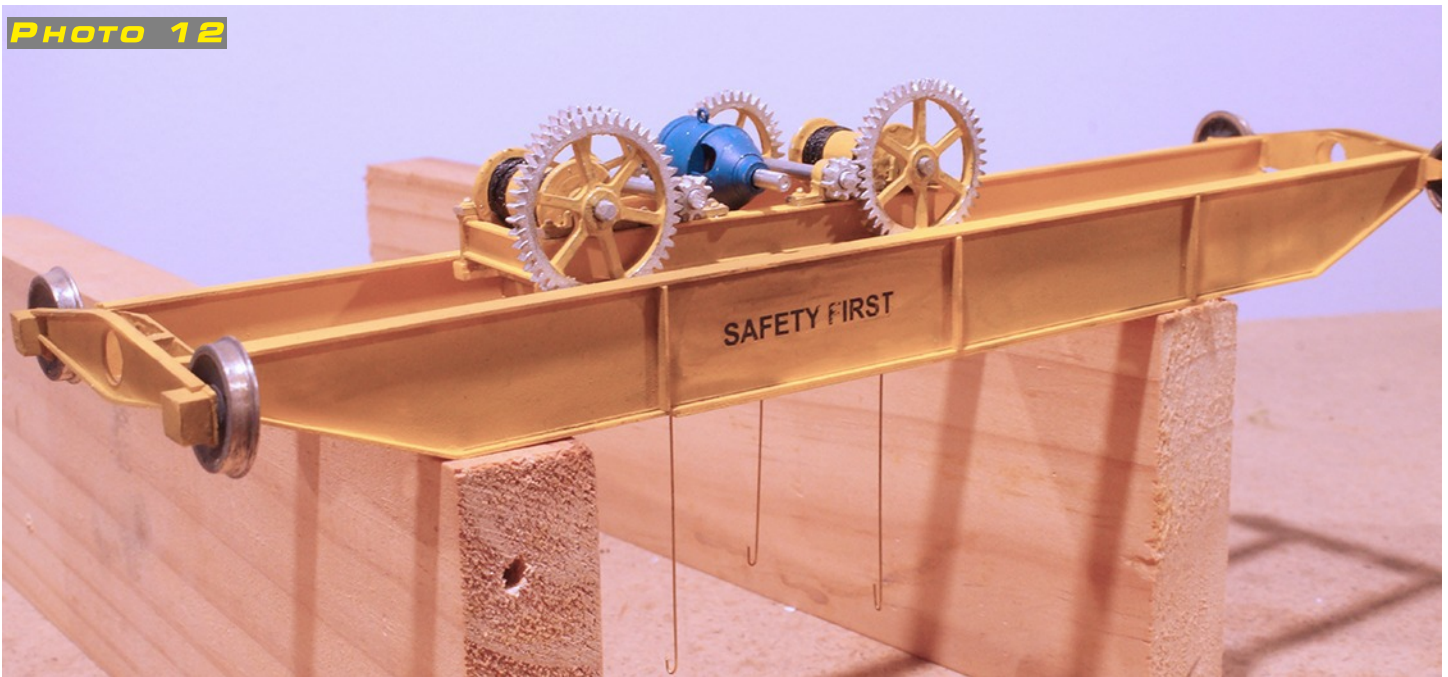
We took care to place our crane away from the lights under the roof for two reasons. One was to ensure the light beams were not blocked by the crane as, if they are, the light will not fall on the terminal floor. LEDs are very directional and narrow beamed devices. And, second, we did not want to light the crane that was cobbled up as a caricature only. If you spent the modelling year building a perfect scale model of your travelling crane, you may feel differently about lighting it.

Also, you need to make sure hanging container hoist ropes and hooks do not interfere with rail cars.

Our crane is glued down to the rails and is not a working model as a result.

Now you can permanently glue the roof to the four posts. I made a few brackets and painted them the weathered black and glued them over the joints between roof trusses and posts.

Place a few containers in the two storage areas. They can sit loose or be glued down as you see fit. We have a flatbed trailer, and, when we find it, will place it on the roadway. With that, if you have been building along with me, your terminal should be about ready for a container car. We now need to get busy and get our container terminal installed on the layout. **Photo 13 (next page)** is of my finished terminal ready for layout installation.



When eventually installing the structure on the layout, I wished I had used a thinner base. The container terminal was to be located on top a section of 3/4 inch thick plywood directly over a hidden return loop and I could not easily cut a hole to drop the base into the plywood here. Since the base was also 3/4 inch thick, we would need to build up the track, and road, approaches in a ramp like fashion to get to the right height. So much for forward thinking. But a strong stiff base is more than justified for what would be a fragile and unstable structure if a strong base was not used. We finally, after much effort and quite a few nasty words, managed to cut a squareish hole in the 3/4 inch thick ply so we could imbed the model in the table top. I glued blocks at each corner to hold the square model's base packing them up a bit so the terminal would be higher than the surrounding ground just a little.

Some heavy wire was soldered to the thin bare copper wires at the base of two or the steel piers and extended to the power bus under the layout. Scenery was worked into the crack where the base of the terminal was dropped into the hole just cut.

We used cardboard to build up the roadway that brought truck to and from the terminal to collect containers. The road crosses the track in the yard in several places and crossing were built for these. The road when exiting the terminal makes a sharp right turn to avoid another industry behind the terminal. There was only 4 inches between the back of the terminal and that industry and the layout edge, so the tight turn was needed. A minor problem was that the short overrun track almost reached the layout edge. A concern, of course, was that a serious overrun of a car when being spotted at the terminal could push the car off the edge of the layout. We knocked up a block to avoid the unpleasant trip to the floor by extending the adjacent industry just a little. Now an overrun would have the car smack into the wall of the extended structure which, although undesirable, will result in much less heartache.

When the cardboard was dry, some newspaper was glued over it to form edges along the road and then the area was painted using our standard earth color. Fine sand and ground cover were worked into the wet paint as usual to provide some scenery. The roads were modelled as minimal gravel over dirt, so some fine grey material

**PHOTO 13**



was used to model these. Coarser gravel was used at the center of the road and along the sides. Weeds and shrubs were worked into the scene particularly to disguise the edge of the terminal base.

With this done, I only have one remaining structure to complete the Jacobs Creek industrial area. Slowly, but surely, we are getting there.

## Containers

While researching on the net we ran across containers made by MTH for the tinsplate market. A few were purchased via EBay to have a look. We found them to be very nicely made and quite true to prototype in size and detail. (See **Photo 14.**) After some study, we decided a new paint job, lettering, and weathering was all that would be needed to turn these toys into useful models. **Photo 15** shows these containers after a bit of work. New decals were drawn, printed, and the containers were re-lettered and weathered after painting.

The two containers on my C&LE flat car were scratch-built, and one more was wanted to place on the ground at the container terminal. Rather than make another single set of laser cut parts and build another from scratch, we purchased one from Berkshire Car Shop. It is a very nicely done 3D printed model. It is much finer and has somewhat greater fidelity than my scratch-built ones. For example, the doors include the actual number of indentations as on the prototype. We had to reduce the number of indentations on the scratch-built ones to accommodate the laser cutter technology used. The Berkshire Car Shop's C&LE container came fully

assembled except for the three wood battens that hold down the corrugated steel roofing. Otherwise, only painting, a little lettering, and weathering was required to turn this in a complete and very nicely detailed model.

I think Berkshire Car Shop also now has the C&LE container flat car available for purchase if you want one. I know Ed was working on it after the containers were done.

As a side, we had not been able to find decent photos of, or other information on, the prototype coke containers. We certainly wanted to know more about them as well. Since our layout is based in the Coke Region of southwestern Pennsylvania, we certainly could use some coke containers. We assumed they had bottom drop doors to discharge the coke similar to the brick containers. From what I have been able to learn, they were also open at the top. We continued to research these coke containers as we saw them as another container commodity for the Mountain Electric. Finally, we ran across a photo of coke containers in a gon. We built some as a part of our removable load project a while back. Loads of these containers will also use the new container terminal.



While checking out the vendors at a local train meet one day, I ran across some kits for the rail containers used down here some years ago. Some of these are pictured in **Photo 7**. I bought a bag of two, assembled them, painted them orange and lettered them for the Mountain Electric. This fits in well with the historic trend in that they are totally different than the other “quasi-standard” containers





in use by some of the northeastern US roads. My finished, and repatriated, Aussie containers are shown in **Photo 16**.

These new ME Ry containers will be used to move LCL lots of merchandise from the joint traction freight terminal in downtown Pittsburgh via the Pittsburgh Railways and Mountain Electric to the end of the line at Somerset. They might even be interchanged with the West Penn Railway at Scottdale and work their way to Brownville or Connellsville on that line if the West Penn gets busy and builds container handling facilities as has the Mountain Electric. Of course, the new container terminal at Jacobs Creek will see delivery of these containers as well.

### Building Container Cars

To get the most use we could out of the new container terminal, we decided to knock up a pair of container cars to supplement the traffic that could be generated by the single C&LE container flat already on hand. And, of course, our first thought was we needed cars that would accommodate both the LCL Corporation and oddball Mountain electric containers. We could just include the Mountain Electric containers as loads for my interchangeable load gons we built a while back and dodge building new cars. But, it's more fun to complicate your modelling!

In pondering what to build in the way of LCL container cars, we came up with a few criteria. They should be traction trailers capable of street running with radial couplers. And, they should be “new” cars. Our ME Ry is famous for its antiquated equipment – almost a rolling museum. So, we thought it best to shock the traction community by building something “new and modern” for the container service. Well, not really all that modern as we are modelling the 1930s. We could have just added container restraints to some old sagging wood frame gons, but the ME Ry faces enough criticism for this all-to-common practice already.



What ended up on the sketch pad after a doodling and drinking session (besides some spilled red) were two approximately 40-foot-long low side gons with steel frames and bodies. The initial intention was for the inside length and width of the cars to accommodate either of the MTH containers. But the round and square containers could not both be easily carried in the same car length. Recall what I mentioned earlier about the lack of standardization for rail containers. We finally arrived at a compromise. The car for the round containers had sides and ends while the car for the rectangular containers had only sides. The basic idea is shown in the sketches at **Figure 5 on the next page**. If any of you out there are draftsman, you will quickly note that there is no danger of me putting you out of work. Our drawings are a bit rough, but hopefully you can get the idea of what we have done.

Finally we decided to limit the new cars to moving the rectangular and round quasi-standard containers and deal with the oddball ME Ry containers using the flats and gons previously built for the interchangeable loads. We already have a removable coke container load for these cars, so adding the oddball merchandise contains to their workload fits the scenario.

We usually build freight cars with a thin wood, styrene, or MDF floor. The underbody detail as a result is not as deep as the prototype would be to help disguise the presence of the 1/8 inch thick sub-floor sandwiched between the underbody and the floor. The sub-floor construction gives some strength to the model as well as provides a platform on which to build up the detail.

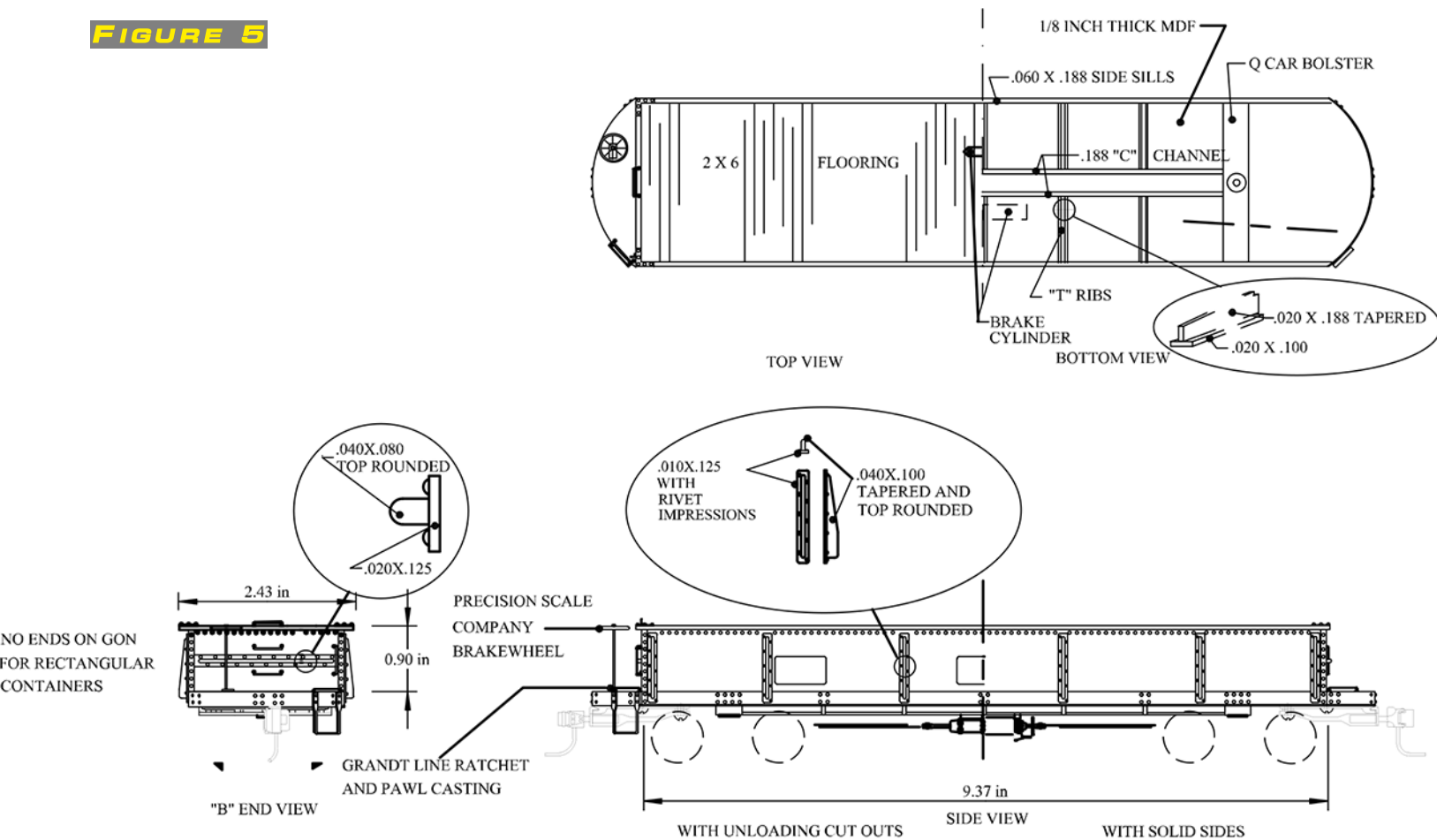
Also, our usual approach to open freight car construction is to make a center sill filler of 1/4 x 1/4 inch key steel drilling and tapping holes for the truck screws. This provides a bit of weight low in the car. We do like to build cars a little on the heavy side to ensure they will operate through spring turnouts as traction lines usually have quite a few of these. For traction, a little extra weight is not a major operational hindrance since traction freight trains are usually short. Car weight is a matter of compromise and requires caution though. If cars are too heavy, they cause unnecessary wear on truck bearings as well as increased friction. But for these gons, the key steel in the center sill was deleted since we will be able to add any needed weight in the bottom of the containers.

One car, intended for the round bulk material containers, has the side cut outs, as per prototype steam road gons. This allows access for unloading without the need to remove the container from the car. The other car has solid sides and is for the rectangular containers. **Photo 17** is of the finished cars loaded with their respective containers and ready for service on the ME Ry.

The design of the gons assumed the ME Ry bought in some off-the-shelf parts to hasten construction in the Company shop. Sides and ends are braced with stakes and stiffeners made from standard pressed steel parts.



**FIGURE 5**



The sketches included on **Figure 5** above show how these parts are made from scratch. Otherwise, the cars are modelled as constructed of standard angle and C channel shapes and steel sheet.

My cars are mostly styrene with the MDF sub-floor, brass for grabs and steps, and a strip wood floor. There are a few commercial details such as brake wheel, brake cylinder and white metal Q Car bolsters.

The cars are modelled as riveted construction. The rivets on the “pressed steel parts” were made using a Northwest Short Line rivet press, a very handy tool. But all other rivets are done with decals. If you have not tried the new rivet decals, you should. They are a very welcome addition to the scratch builder’s arsenal. If we had a dollar for every piece of brass or styrene tossed out over the years after making a mistake while pressing rivets, we would be rich. Decal rivets are much more forgiving, speed up the building process, and greatly reduce waste. If installed properly they are quite durable, too.

My cars are equipped with type K brakes and arch bar trucks both of which were common on traction line freight cars to the very end. The radial couplers use brass investment castings made from our own patterns and take the new Kadée 700 couplers. The cars have all common brake rigging modelled which is particularly important for traction freight cars as they ride so high with much of the underbody detail quite visible.

Some with keen-eyes, the nit-pickers, and/or the rivet counters might notice the odd arrangement of grab irons, steps, etc. Traction lines did not interchange such cars with the steam railroads and were not bound by Association of American Railroads (AAR) rules and Master Car Builder (MCB) conventions. They often did as they pleased with many appliances and fittings. Grabs and ladders on the center line of car ends was common on traction lines such as the Indiana Railroad. Unfortunately, the ME Ry is equally oblivious to modern train crew safety appliance trends and similarly inclined. If you were going to build such a car for steam road



interchange it would not have rounded ends and radial couplers, and would follow AAR and MCB design requirements for safety appliances and fittings.

### Restraining the Containers

The traction clearance gauge was generally tighter than that on steam roads, traction cars were often a bit narrower than steam road cars, and that is the case here. There were many different restraint systems used due to the different container configurations. We had to cobble up an approach to restraining the containers for my narrow cars that made some sense and hopefully looked like it could have been. But, I am sure it is

far from prototypical. **Photo 18** of a very fine model O scale car by RY Models, used with Rich Yoder's permission, shows the restraint detail in a standard rail gon for round LCL Company bulk containers.

For our round container car, we made the restraints from some angle and strip styrene. A four-sided bracket made of scale 6-inch angle stock sits on the car floor. A similar three-sided bracket is also at the top, but this one has longer legs and sits on and is attached to the top angle of the car side. Guide plates made of .020 x .250 styrene strip were glued between the two brackets. The tops of these guide plates were curved toward the car sides by rolling them over a jeweler's screwdriver shaft. These plates were to help guide the containers into position when lowered by crane into the car. The sketch at **Figure 6 on the next page**, might help you understand what we did here. These were somewhat fiddly to make since we needed six of them and they could not be built in place in the car. The restraints needed to be installed after the car interior, and the restraints, were painted, and the wooden floor was installed. We drew a template on a bit of scrap card and used it for cutting and assembling the styrene angle material. This approach helped us to get them close to the same size and shape.

I assume the rectangular containers were restrained by pins on the car floor that engage sockets or holes in the four feet at the corners of each such container. Therefore, we did not add any special fittings to that car for them. Had we only partially loaded the car, i.e., left one container out, it would have been necessary for me to add such fittings to the car floor. Taking the easiest course of action as usual meant that we totally filled our car with rectangular containers to avoid the need to research and model the restraint feature for these containers. As you can see in the shot of the Pennsy flatcar, they used a different system of restraint for their unique rectangular containers.

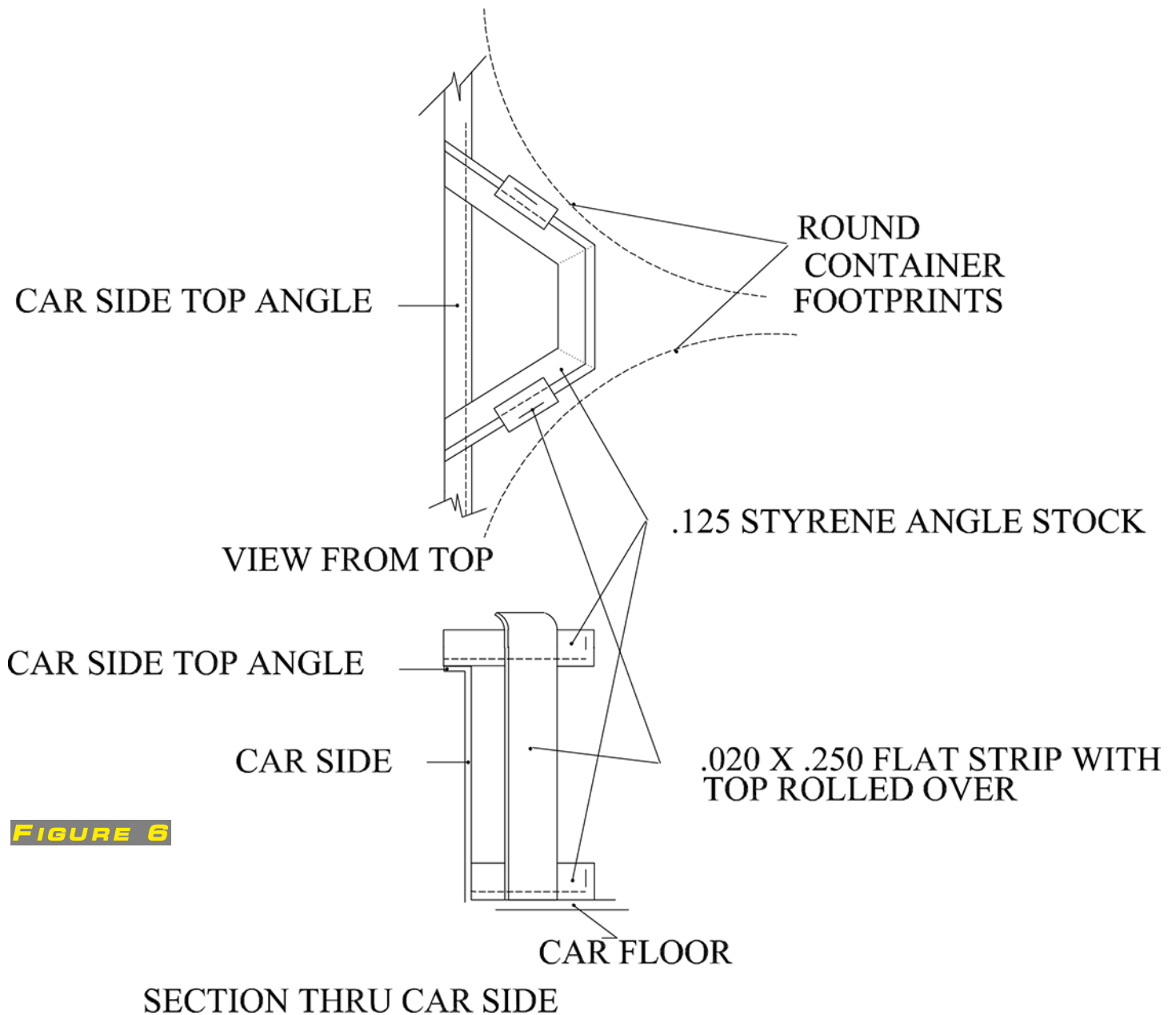
When we built our coke load for one of our interchangeable load gons, we left one container out and modelled the locator pins using the pointed ends of truck axles.

### Finishing the Cars

The cars were sprayed with primer before the decal rivets were applied. With rivets and all other details finally complete, the cars could be finished.

The ME Ry has been experimenting with new paint schemes in a probably futile attempt to modernize its image. Like many traction roads, the early ME Ry standard color was a drab dark green. The newest scheme is a bright orange body with a dark red roof – when there is a roof. And, given these gons are modelled as having been in service for a while, they have some weathering of course. The insides of the cars particularly take a beating from crane men who are in a hurry to load the containers.

# ROUND CONTAINER RESTRAINTS



**FIGURE 6**

The cars are lettered with our own decals. Well, we do the art work in Corel Draw and a train friend prints them. The car lettering even advertises the “*EXPRESS CONTAINER SERVICE*” and reflects the hopes of ME Ry’s management for this relatively new business line. One reason the cars have radial couplers is to permit them to be added to express passenger trains to expedite their movement when necessary.

## Container Operations

So far the container service is well patronized by our ME Ry freight customers, is earning reasonable revenue, and is justifying the investment in container terminal and special cars.

Many different commodities can be accommodated in the rectangular merchandise containers. We have been thinking about what these are in anticipation of working out a waybill system add-on for LCL shipments. So far we have identified hardware, furniture, non-perishable groceries, general store merchandise, school books, etc.

The small but somewhat capable ME Ry Traffic Department has been directed by management to look around for bulk commodities, in addition to the commonly handled cement, that can be moved in the round containers. Rock dust has been identified as a relevant bulk commodity. Rock dust is a powdered limestone used in coal mines. It is blown on the walls, roof, and floor of coal mines to cover coal dust and lessen the chance of an explosion due to airborne coal dust. Traditionally rock dust has been shipped to the mines along the ME Ry in sacks by box car. Mines were required to order large quantities of the sacked rock dust, commonly 20-30 tons or so, to make delivery by box car economical. Such large quantities were costly to store for long periods until used. The ME Ry has convinced several of its mine customers to receive their rock dust in the bulk containers where as little as 6 tons can be ordered. These mines are very satisfied that the ME Ry is looking after their interests.

The Traffic Department also needs to come up with some other bulk materials for our new service. In pondering this further, we do have an off-line automobile tire manufacturer in the town of Jacobs Creek that ships and receives goods via the local team track. We began wondering if this customer could get some carbon black needed for his tires in bulk containers. We decided to do a bit of research and see how carbon black was shipped in the real world before covered hoppers came along, and/or how it was transported when smaller lots, less than a covered hopper load, were needed. This could be another commodity for the container terminal.

After a consultation with Dr. Google, it appears carbon black is nasty stuff to transport. It is explosive; difficult to unload by gravity; is carcinogenic; contaminates the container making extra cleaning necessary, difficult and expensive; and is a mess to clean up when spilled. At one time it was shipped in box cars in big bags that were put in a frame of some sort so the contents could be vacuumed out and pneumatically conveyed to mixers where it was added to the rubber in making tires. Sounds like just the sort of product we really don't need on the ME Ry. But then any revenue will do in a pinch particularly during the Great Depression. Occasionally carbon black arrives by box car in the vacuum bags. But the tire firm has been using trucks to get smaller quantities. Business during the depression is, well, depressed. Orders for tires are not as frequent and smaller quantities are being made to keep finished goods inventories from getting too large. Using the round containers might just maintain this business for the ME Ry. The ME Ry management will look into this further I am sure.

Other bulk commodities might include small lots of palletized fertilizer, and dry feeds, etc. We will research this further.

We are also pondering a procedure for adding waybills for container commodities and using them to control movement of the containers on the layout much like car loads of freight. We shall give this more thought. Having such a system that could also work for non-containerized LCL freight would be a good operational add. But getting this all done and working is another story for another time.

**Photo 19** shows a container car at the new terminal on the layout. We have installed the line poles for terminating the overhead wire, but the wire is not yet up.

In summary, this new LCL container service has been so successful, management is now looking into a possible piggyback service such as that introduced on the Chicago North Shore & Milwaukee and the South Shore, traction lines in the Chicago area, in the 1920s. **See Photo 20** of a nice model of the flat car and road trailers used in the Chicago operation. We just might need to build a model of these for our operations on the Mountain Electric. But, that too, is another story for another time.

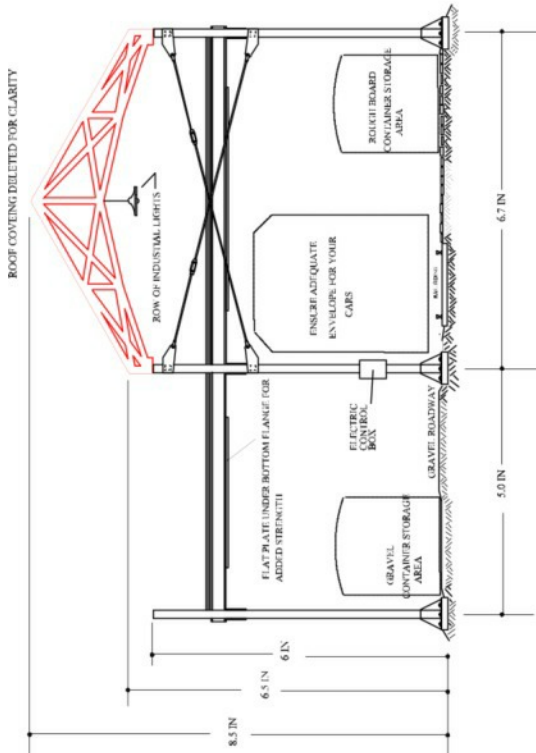
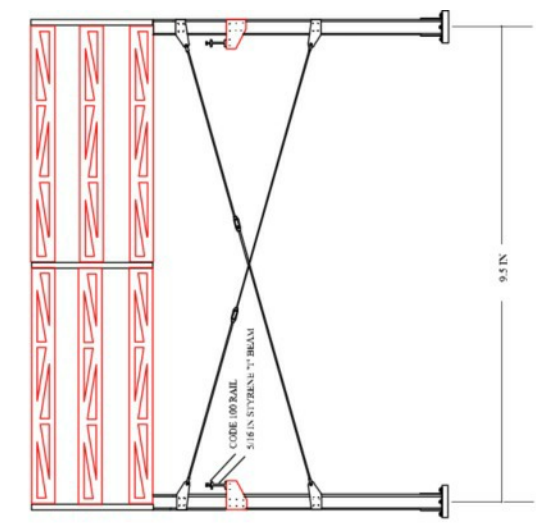
PHOTO 19



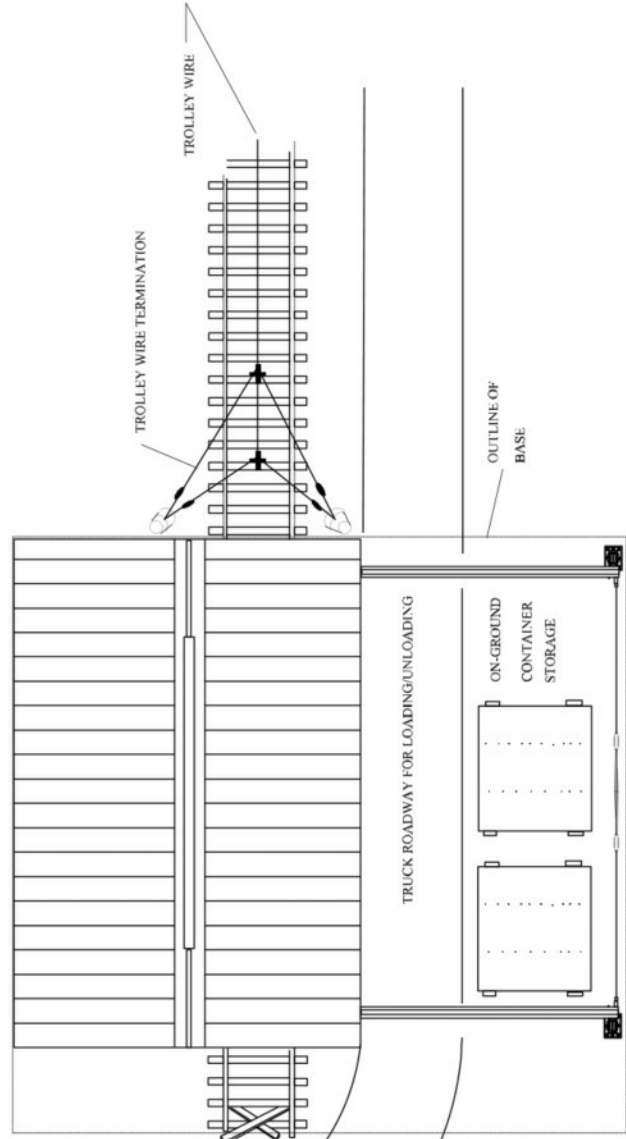
PHOTO 20



FIGURE 1



TERMINAL ELEVATIONS



PLAN OF TERMINAL



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# O Scale... Old School Style Vintage O Scale 1926-54

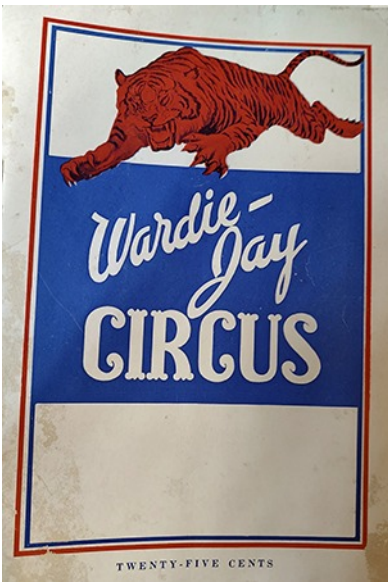
By Carey Williams

Email author by clicking on their name.

## Vintage Wardie Jay Circus Trains

*"The whole world is a circus if you know how to look at it"*

Dr Lao



Circus trains have delighted many a child... Marklin 1908 +-, Ives 1928-30, Lionel Mickey Mouse 1934. Tony Koveleski of Scranton, PA wanted to produce a scale model Circus for adults. In March 1946, an ad for Koveleski's Wardie Jay circus was published in *Model Railroader*. Wardie Jay sold through Scranton Hobby Center offered kits for the talented builder to create an entire circus in O scale with basswood and cast metal kits.

In February 1947 Koveleski bought out Walter Bathgate's HO scale circus line... to corner the model train circus world in both gauges now made by Hobby Lines Mfg of Clarks Summit, PA. Koveleski quickly added tin plate length (shorty) cars in addition to the long scale 70' foot cars to broaden his audience. By 1951, Wardie Jay Mfg Co. was located in Cape May Court House, NJ. The Wardie Jay line remained in production for 20+ years and was last located in Vermont. Circus Craft of California seems to have picked up the circus lineage by 1969 and continued for many years.

Early wagons have cast metal wheels which do add some weight, later switching to plastic wheels unknown to shave weight or cost of kits. The Wardie Jay catalog went into great detail on building a 1/4" scale tent... only to occupy 36x64" on your layout.



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R.F.D. NO. 1  
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The "Circus Model Builders" club formed in 1936 is dedicated to the art of recreating the circus in miniature and still going strong. Many of their member's handiwork can be seen at Train Fest in Milwaukee, WI.



When I think "circus", the Great Milwaukee circus parade comes to mind as I rode in it many years a top of an antique high bicycle. A natural selection then to pull the train was a Milwaukee Road S2... scratch built from the late 30's early 40's... running with outside 3rd rail.

Cars and wagons are all old... built with varying degrees of talent. All the wagons have cast metal wheels... the engine is aware it is pulling something with the 8 cars consist. The personnel/clown car at the end is a molded copper sided Exacta car from the 1946-1948 window.

[Cheers Carey Williams](#)  
(click to email Carey)





To see the Circus Train in action please see link

<https://youtu.be/CSQ5M4ALLfU?si=qIYyzZLxoJ6WHOPe>

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**CONSTRUCTION**  
 THIS PLAN IS DESIGNED AND SHOULD BE STUDIED CAREFULLY BEFORE BEGINNING ASSEMBLY OF THE WOOD PORTION OF THE MODEL. IDENTIFY PARTS IN KIT BY COMPARING THEM WITH PLAN. THE FIRST STEP IS TO CEMENT THE STAMPED BRASS WIRE SPACERS TO THE MODEL. TRIM THE SPACERS TO 3/32" STRIPS AND INTO THE RECES IN THE FLOOR SIDES. TRIM THE SPACERS TO 3/16" IN LENGTH CUTTING 3/16" FROM EACH END OF EACH SPACER. NEXT GLUE ENDS AND PARTITION TO THE FLOOR. BE SURE TO SET THE ENDS BACK 3/32" FROM BOTH FRONT AND BACK ENDS OF THE FLOOR THEN GLUE THE 1/16"x3/32" STRIPS WITH SPACERS MOUNTED IN THE NOTCHES IN THE ENDS AND THE PARTITION IN MOUNTING THE STRIPS. BE SURE TO KEEP THE SPACERS TO THE BOTTOM OF THE STRIP. THE WIRES REPRESENTING THE BARS CAN NOW BE CUT AND SET IN PLACE AND CEMENTED TO THE WAGON BODY. FALLING AND CEMENTED TO THE WAGON BODY. FALLING AND CEMENTED TO THE WAGON BODY. FALLING AND CEMENTED TO THE WAGON BODY.

SEAT RAIL SEAT  
 GRAB IRON WINDOW  
 STEP  
 DOOR DRAWN ON WITH INK  
 ORANGE  
 1/8" SQ.  
 CAST METAL REAR SPRING

REAR VIEW  
 FRONT VIEW

ROOF MAY BE HELD IN PLACE WITH PINS TO PERMIT PLACING OF ANIMALS IN CAGE

ELEVATION  
 BRAKE SHOE  
 CAST METAL CROSS GEAR  
 PIN  
 BRAKE BEAM 1/16"x3/32"  
 FASTEN IN PLACE WITH PINS  
 PIN  
 BEND PIN & INSERT THRU FRONT OF REAR SIDE SPRING  
 FOOTBOARD

REAR SIDE SPRING  
 NEAR WHEELS NOT SHOWN TO SHOW RUNNING GEAR DETAIL

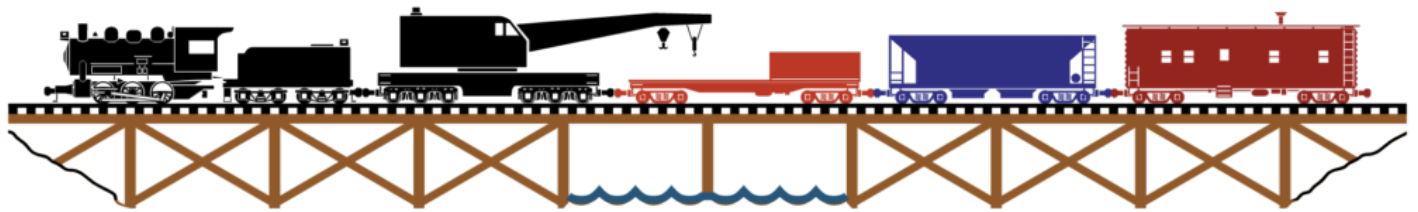
BOTTOM VIEW  
 SIDE SPRING  
 1/16"x1/8"x1-3/16"  
 1/16"x1/8" FILTER 1/4" LONG  
 3/32" SQ.  
 1/16"x1/8"x7/8"  
 FRONT RUNNING GEAR  
 REAR VIEW  
 TOP VIEW  
 FRONT VIEW  
 TONGUE DETAIL

1/16"x3/32"  
 1/16" SQ.  
 DUMMY SLIDING DOOR IN CENTER PARTITION  
 SPACER  
 1/16"x3/32"  
 BRASS WIRE SPACERS IN PLACE TO HOLD FLOOR

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# NEW TRACKS MODELING



By Contributing Editor Jim Kellow MMR

**“Modeler’s Path to Success.”**

**Use your Mentor’s past modeling memories to help create your own and achieve your modeling successes.**

**“Try it. It works”**



Jamie Stengle - Associated Press DALLAS - published an article about Greg Jein in the September 9, 2023 *Citrus County Florida Chronicle Newspaper*. One part really hit a nerve.

“Jein, who had an Oscar and Emmy nominated career making miniature models, was also a collector of costumes, props, scripts, artwork, photographs and models from the shows he loved. "He spent his entire lifetime in a movie industry at a time when practical effects and models were the way that magic happened," said Joshua Benesh, Heritage's chief strategy officer. "They (models) were the way that spaceships traveled through outer space. They were the way that aliens came and visited Earth. They were the way that catastrophes and disasters were depicted.".

I immediately thought that models are also the way our imagination and creativity allows us to visualize real railroading. I believe the comments made in the article express how many model railroaders fondly remember railroad modeling from their youth. It is why many of us continue to build models today, and want to see railroad modeling continue into the future.

Younger and new modelers can definitely benefit from seeing the modeling of older modelers and learning how they were built. Older talented modelers owe it to less skilled and new modelers to teach them their skills. Please help pass on to future modelers your knowledge, skills, techniques, and memories whenever you can. I know our hobby needs it.

## **New Tracks Modeling New Show Segments**

### **1. A Series focusing on specific Scales: N Scale, HO scale, S Scale, O scale, O Gauge Hi-Rail, and G scale Model Railroading**

I have had many discussions with modelers who say things like: “what is Scale (X) all about? Does anyone really model in (X) Scale? Can I scratchbuild a model of ?? In (X) Scale?” Question mark (?) Scale can be G, O, O Hi-Rail, S, HO or N. So we decided to start a monthly segment on G, O, O Hi-Rail, S, HO, and N Scale hosted by knowledgeable talented modelers in each scale to try and have your questions answered and information given about what is possible and things a new modeler entering a scale might need to consider. One scale will be discussed each week, each month, starting in our November 2023 shows. Here are the dates and hosts for the November and December 2023 shows. If you have specific questions you want addressed, or a

**MENTOR DEFINITION: A TRUSTED COUNSELOR OR GUIDE**

specific person you would like to see interviewed on a scale segment please contact the appropriate host. Their email addresses are shown below.

**G Scale Modeling hosted by [Steve Bittinger](#) (11/29, 12/27)  
Sponsored by [New Creations Victorian Railroad Buildings LLC](#)**



**O Scale Modeling hosted by [David Schultz](#) (11/15, 12/6).  
Sponsored by [O Scale Central](#)**



**O Gauge Hi-Rail Modeling hosted by [Dennis Brennan](#) (11/15, 12/6)  
Looking for a Sponsor for this Segment. Interested? Email me at [Jimkellow@newtracksmodeling.com](mailto:Jimkellow@newtracksmodeling.com)**

**S Scale Modeling hosted by [Timothy Huebner](#) (11/1, 12/20)  
Sponsored by [NASG](#)**



**HO scale Modeling hosted by [Ed O'Rourke](#) (11/8, 12/13)  
Sponsored by [Mainline Hobby Supply](#)**

**N Scale Modeling hosted by [Clem Harris](#) (11/8, 12/13)  
Sponsored by [National Capital Trains](#)**

After these 2023 shows we will evaluate our content based on viewers, reactions and comments. So, if you are interested in discussing something specific about one of these scales, please let the host know and tell him what you want to know and who you would like to hear from. It's up to you to help the host decide what these scale segments discuss and who are guests on the segments. Please subscribe to our website: [newtracksmodeling.com](http://newtracksmodeling.com) so you don't miss any of these discussions on our Zoom shows.



## 2. A Segment about “The Economics of the Model Railroading Market”

This segment will be hosted by Bernie Kriger owner of National Capital Trains. The first monthly segment will be on our January 10, 2024 Zoom show. There are three pieces of the model railroading market. The modeler, the retailer, and the manufacturer/distributor. The economic issues facing each of these groups determines the health and viability of our hobby. Bernie has a long history of consulting with private companies on economic issues and as a retailer and modeler in our hobby. He is extremely well suited to host this monthly segment and try to keep us up to date about the issues facing our hobby.

Please contact Bernie with your questions, comments, and opinions about model railroading and specific areas you would like him to discuss. We know that Bernie will improve our knowledge of the hobby's economic situation and what we can expect in new products, pricing, distribution channels, and manufacturing technology. Bernie can be reached at [Bernie.Kriger@newtracksmodeling.com](mailto:Bernie.Kriger@newtracksmodeling.com)

## 3. A Segment: “Setting the Stage with Scenery”

Starting in January 2024, Bob Geldmacher, Chief Scenery Clinician at Scenic Express, will be presenting a 15 minute segment twice a month about using scenery to set the stage and enhance our model railroads. As we all know, scenery plays a vital roll in creating the scenes that help to make our model railroading an art form. Bob will discuss a variety of products and demonstrate techniques that can help your scenery come alive. Please contact Bob with any suggestions about subjects you would like him to cover. You can reach him at: [geldy@aol.com](mailto:geldy@aol.com) or call at 410-926-4514.



#### 4. What other new segments do you want on our Zoom shows?

Well before we know it, 2024 will arrive. I am in the process of developing new segments for our Zoom shows and need your advice and ideas. What do you want to see? Please let me know. In the past, I have tried to provide every show segment suggested by a viewer and want to continue to do so. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com). I look forward to hearing from you.

#### **New Agreement of Association signed between New Tracks Modeling LLC and the Big Bend Model Railroad club in Webster Groves, Missouri.**

On August 18, 2023 New Tracks Modeling LLC entered into its second Agreement of Association with a model railroading organization. We appreciate the [Big Bend Model Railroad Club's](#) interest and are looking forward to working with them on mutual projects. I will be providing more information about the club and its activities in future articles.

New Tracks Modeling is interested in entering into such agreements with other organizations for our mutual benefits. For additional information about this program please contact me at: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com)

#### **Reader's Views on Creating Enthusiasm in Dioramas and Scenes**

To refresh your memory, in the last issue I wrote: "Yesterday I talked to a professional modeler who makes dioramas for museums. He caused me to think about our dioramas or individual scenes, as being truly inspirational and opportunities to convey our "enthusiasm" for model railroading to viewers through our modeling. But how do we convey "enthusiasm"? To me, this will require taking our modeling to a different level. We can't just strive for good enough, or even worry if our model will get a NMRA merit award or win a contest. It seems to me showing "enthusiasm" requires more of us. Phil Edholm talks about our models displaying "emotions" to viewers. Maybe this plays a large part in what "enthusiasm" is all about.

I got a lot of great replies from readers which have helped me further clarify this issue in my mind. Read on, and again, send me your emails with your comments to [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).

The first comment to really strike home for me was because I have made all the mistakes Dan talks about below in building a layout. His last comment I know is true because I have experienced the feelings about "loving" the scene I am creating. Thanks Dan for putting these modeling tips together.

#### **Daniel Bigda**

Jim, I wrote the below to help modelers with their layout or module builds. Hope this helps with your talking points from the post on-line:

Having built a few layouts... I would consider these valuable lessons.

- While measuring the room, I use masking tape where the table edges are going to be. I tape right to the floor. This way, I know how it will be and all. Once I have the table made, I pull the tape. You can also lay the track on the floor to make sure everything fits and works well... this helps with visioning the layout.
- No duck unders... having those suck. Especially for those with back, leg, hip issues. If you feel you need a duck under, then build the walkway area with a lift out bridge or something similar, because ducking under sucks!
- Reach... only make the benchwork as far as you can reach. If it's out of reach, that is where your issues will be. If you can get to the tables from both sides, then your reach to center from each side is your width.

Height... I would have made mine chest height. It isn't, and I contemplate raising the layout every day. But those things have no relation at all to the model being realistic or not. You can be very enthusiastic about a

building in Lego bricks, still they never fool the viewer in doubting their eyes if the subject is real or a model, Legos are clearly 'the model'.

So where enters this enthusiasm? It enters if the build becomes believable. It enters when the viewer no longer sees the materials used, but is pulled into the build and experiences to 'be there' in that model, diorama or whatever you build. Realism, or quality of the build, has little to do with that. Our brains are very capable to experience human thoughts from a stick-man drawing by a toddler, yet struggle to see realism in a highly detailed model with a logical error in it.

In the end it all comes down to the experience, both for the builder while creating, and the viewer while observing the item made.

René Magritte had it right: Ceci n'est pas une pipe.

What we make is not real, it is in our imagination, but do we believe it?

Well what do you think?

There were many other comments that I should include because I found all of them interesting, but these particularly struck a nerve in me and added a new dimension to my thinking about my modeling and its impact, or lack there of, on the people seeing it.

As you know, I am convinced model railroaders produce models that are an art form and have a definite impact on viewers. If we are building something we love to build, loaded with realism, verisimilitude, and craftsmanship, and we have viewed it with our 3rd eye and mind's eye, I believe we will have created models with emotion and enthusiasm that are true pieces of art. Do you agree? Do you care? I bet you do care. Let me know either way. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).

- Electrical... do that before scenery, at least the bus wires – get them run under the layout.
- Height of scenery works better to the eye than depth of scenery.
- Make all benchwork modular. So if you don't like something or want a change, taking out the old and inserting a new piece is easier. Plus, you can work on the new piece while the old one is still in place and you can still run trains.
- For O-Scale, use 3/4" plywood sub road bed. For the areas where sometimes risers get a little wide, the plywood being a little thicker, won't sag (1/2" plywood will). Don't use flake board – that stuff sucks for model RR. If you get flake board and use water against it for scenery, the flake board seems to expand and never contract.
- Wide isles are a must, you can do it with the room you have. Trust me, it is worth the effort to have these. I have two spots where the isles get close, but they open right up to wide areas. I also didn't make those areas points of interest. Make your points of interest where the isles are wide so people can gather easier without hitting benchwork.
- If you're thinking of having a turn table, one where the tracks are coming out all around it, that becomes an area of interest and people gather around it. I did mine where the mainline went behind the house and people can see into the roundhouse and also get right up to the table to see the locos turn. This turntable idea came to me this past spring, and I changed the whole yard and turn table area into another part of the layout in another area of the room to accommodate this. It works out great now.
- With your yard, well, before I forget, have a staging yard. One hidden under mountains (or there of) then your yard itself won't be cramped with cars as mine was until I did this. (I am also ridding cars of later dates because my model RR is set around 1977. So this purge is setting up nicely as I now have room for the cars

that fit the timeline.) Also, keep switches within easy arm length as repair is easier, etc. I did redesign the yard as stated above, and am going to again because of the railcar purge, etc. Plus, the yard is smaller and more manageable. Well, the whole layout is.

Also, keep this in mind when you design: 1) model the places as scenes you loved as a kid or adult. Don't just put track down for the sake of putting track down... you will get bored with it. I learned this on the first layout. I thought I had all the room, and when the layout was up, it sucked. So a friend came over and told me it sucked and gave me this advice... number 1) again, model scenes you love, figure out the top five scenes you love and design them into the layout, then the next five and so on. You find that the layout fills out nicely and you will want to work on it all the time. So grab the 1<sup>st</sup> thing you want to model, then the 2<sup>nd</sup>, then the 3<sup>rd</sup>, then the 4<sup>th</sup>, then the 5<sup>th</sup>, and see if you can 'fit' those into the available model RR area. If not, you might have to leave out some areas you love. Try another place you love, etc. I have also made scale "paper modules", and you can move them around on a scaled drawing you have. That way you can see if the scene fits, etc. Anyhow, designing and building is fun! Good luck! I hope these points help. ~ Dan

These next several comments I feel are getting very close to explaining what enthusiasm in our modeling means. Do you agree? Does your modeling seek to achieve these goals?

In this next comment, Alan Kleipass used the word, verisimilitude, I had to look it up. Here is the definition I found. "Verisimilitude is just how real a narrative seems, how true-to-life it is. Movies, TV shows, songs, stories, and poems with verisimilitude draw in the audience and convince us to pretend they are real." I believe Alan is correct in using this word as it does apply to our modeling.

### **Alan Kleipass**

"Modeling enthusiasm makes me think of layouts like The Utah Belt, Franklin & South Manchester, and Yosemite Valley, as well as the amazing modeling of Bob Boudreau and his Fundy Northern. Four very different approaches to the hobby, but loaded with realism, verisimilitude, and craftsmanship.

A perfect scene need not be recognizable, but merely believably realistic.

A simple but realistic scene does more to attract new modelers than a full-scale model of, say, Grand Central Terminal in NYC, because the former is within grasp of a new modeler, while the latter is intimidating to even experienced modelers, never mind the wannabe or newbie.

If sharing our modeling enthusiasm is meant to attract new people to the hobby, and/or encourage the "toy train" hobbyists to up their game to the next level, then we need to strip the ego from our enthusiasm. The message should NOT be "look at what I built - heap accolades on me!" Instead, we should enthusiastically proclaim, "you, too, can build like this, and I'm here to help you bring your vision to life."

Art Carlton talks about our "mind's eye". I never thought about this before. Didn't know I might even have one! How about you?

### **Art Carlton**

As someone who scratch builds and also builds museum displays, you have basically described enthusiasm in your second paragraph of this post. However, I would add this. I just hope it make sense.

An individual has two eyes, the eyes you actually see with and "your mind's eye." Your mind's eye will see things your eyeballs will not see. The mind's eye will see the smallest details that the eyeball will not see, and trick your mind that the diorama or model is real. If a viewer gets all excited (enthusiasm) about a diorama/model without knowing why, you have activated their "mind's eye", hence causing that model enthusiasm. I can personally tell you, it's a total rush when you activate an individual's "mind's eye." You will know because the viewer will say, "this looks so real" and then start describing the story the diorama is telling them. I believe it's at this point you have captured model enthusiasm.

Try building a diorama using your mind's eye. To do this you have dig down deep and study every little detail of the prototype. I mean go deep, all the way down to the dust on the surface of things. It's an experience you will never forget when you can build a model using your mind's eye. To me, it's like a bad addiction to a drug.

Next, Dave Allen talks about his 3rd eye. I guess I may need new glasses if I have one of these!

### **Dave Allen**

I have the 3rd eye trick in the mind, it's like seeing it on a screen in your head before you lay the first piece. I often find the hardest bit is getting out into the real world.

Dave Allen followed up his comment, after I said “I don’t understand”.

Jim Kellow to be honest, it's just something my dad used to be able to do to. Imagineering he used to call it. I've been building dioramas now since about 2017, my dad was a keen railway modeller himself before passing in 2016. Just means when I build a kit it's already a 3D model building in my head I can work to, almost like a jigsaw picture reference. My diorama scenes are the same. I have the finished completed scene in my head.

And then, Henk Meerhof, a modeler in Denmark who is a member of the New Tracks Modeling management team, said something that stopped me in my tracks. He quoted: “René Magritte had it right: Ceci n'est pas une pipe.” Read his comments to understand what he was saying. My high school Latin and college Spanish were no help here!

### **Henk C Meerhof**

Modeling enthusiasm for me is a separate thing to model making. If the enthusiasm isn't there, you often can tell by the result being okay instead of magnificent.

But those things have no relation at all to the model being realistic or not. You can be very enthusiastic about a building in Lego bricks, still they never fool the viewer in doubting their eyes if the subject is real or a model, Legos are clearly 'the model'.

So where enters this enthusiasm? It enters if the build becomes believable. It enters when the viewer no longer sees the materials used, but is pulled into the build and experiences to 'be there' in that model, diorama or whatever you build. Realism, or quality of the build, has little to do with that. Our brains are very capable to experience human thoughts from a stick-man drawing by a toddler, yet struggle to see realism in a highly detailed model with a logical error in it.

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with emotion and enthusiasm that are true pieces of art. Do you agree? Do you care? I bet you do care. Let me know either way. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).

Now, let's meet some talented modelers:



**James and Anna Cleveland**



This first modeler and his wife own a manufacturing company producing HO and O Scale kits. Their philosophy is to provide kits that are, "Quick Assembly! Great Results!" Tom Farrell recently built their Lobster Boat and Lobster Shack kits on our New Tracks Zoom show. A video of the build is available on our YouTube channel, New Tracks Modeling.

James told me: "Like many families in the 1960's, I had the ubiquitous Lionel train set, but it wasn't until on a family vacation in the early 70's that I stumbled across a *Model Railroader* magazine which I feverishly devoured.





After seeing the realistic trains and scenery depicted in the magazine I knew that I wanted to have my own train layout. When I was 12, I began my first

HO scale train layout in my bedroom on an old ping pong table. I scratch built several buildings using sheet balsa wood, scribing lines to represent planking. They were crude by today's standards, but it was exciting to create something and see the possibilities. By 14 or 15, I became interested in R/C boats and ships which was a catalyst to later creating model boat kits for the model railroad industry.

In high school history class we were given the option of writing a report about gold mining or creating a diorama depicting some of what we had learned in class. I was happy to pick the project option and scratch built an HO scale gold dredge which I still have today and is sadly in pieces in a box due to a rough move. Also in high school shop class I was introduced to machining and had opportunities to use the lathe and milling machine. I machined a rough version of a vertical steam boiler out of steel and decided it was too heavy to be practical, and wanting to experiment with reproducing it, I made a crude mold using clear silicone caulk and actually was able to reproduce the boiler in resin using the mold.

I went on to trade school after high school and trained to be a machinist. I still enjoy using these skills to machine various masters for the model kits that we produce. In my mid 20's I decided to develop a business starting with HO scale tree stumps, logs, cactus castings and sidewalks that I reproduced in resin, packaged and went door to door selling to the various hobby shops that dotted the Denver area in the mid 80's. These were produced under the name Cleveland's Custom Products. I found that there was interest in products like these, so I continued to create these as well as my first resin kit which was an HO scale fishing boat that I produced. We still produce the sidewalks, stumps and the fishing boat kit. From these small beginnings, my model railroad manufacturing business has grown into hundreds of kits and detail parts."



After their marriage, Anna got involved in model railroading. She said: “I joined Jim in Frenchman River in 2004 when we got married. I didn’t have previous modeling experience although I have a long list of artistic hobbies I enjoy. I help with designing our products, doing the graphic design and creating

laser cut accessory pieces as well as marketing and web design. I also paint and weather about 90% of our displays. Over the years I’ve made rubber molds, cast pewter parts and do all of the packaging and shipping of orders. I don’t usually build the models; however, the HO Scale red tug and the lobster boat are ones that I built and painted.”

James continued: “Over the years, my wife and I have designed and produced quite a few boat kits and nautical detail parts under the name Frenchman River Model Works (name change in 2004) for the model railroad industry and dabbled in a few building kits including an HO and O scale cannery complex.

In 2021, we had the opportunity to purchase a line of building kits designed by Thomas Yorke and previously produced by Dennis Brandt of Branline Sales. We were excited to reintroduce these kits and put them back into production. I had always admired Tom's building kits and the amazing detail he has been able to build into his beautiful models.

We have enjoyed building each display model as we reintroduce each kit and each of them has become our new favorite build. Through this experience not only did we greatly expand our product line, but we have learned a tremendous amount about creating structure kits. We are currently putting these lessons to use in our new product line called StinkwaterCrik. As a side note, we lived for many years in Nebraska near the Frenchman River and one of its tributaries was actually called the Stinkwater Creek (the locals refer to it as a crik) hence the business names.

My wife and I enjoy running our business on a full time basis and enjoy having a creative outlet. We are both artistic people, but I decided early on that although I enjoy creating the original master or patterns for a kit, my true enjoyment comes from being able to create it in a way that can be reproduced and shared with other modelers.

We have tried to make each kit and detail part reproducible for the foreseeable future. This includes creating production molds that will reproduce good, accurate parts for many years. Silicone rubber molds do wear out eventually so we create production masters (patterns) that allow us to pour new molds whenever the original molds get tired and worn. Many kit manufacturers produce a limited quantity of certain kits, while our philosophy has always been to create kits that will be available for the modeler whenever they decide to purchase one. Some of our kits currently in production have been in constant production for up to 30 years, and as technology improves, we try to upgrade these kits to reflect that.

While all of our resin kits are being developed, our philosophy of "Quick Assembly! Great Results!" is at the forefront of our efforts. We consider our finished displays to be pieces of art as they are fully capable of being the center point of any scene. We have always tried to create model kits that have the greatest amount of character and details already cast in place to make assembly and painting a relatively quick process. Looking forward, we are excited about all the possibilities of creating new products for modelers to enjoy.

Thanks to both of you for your help and interest. You can reach David and Anna at [frenchmanriver@newtracksmodeling.com](mailto:frenchmanriver@newtracksmodeling.com).

This next modeler lives in Germany and recently sent me information about his model Railroad collection and a photo of the Building where he plans to locate his Model Railroad Museum. Please meet:

### **Christof Henseler**



I thought about your friendly words and some aspects came to my mind which I'd like to write to you.

Many German model railway associations offer activities for children, for example they can always glue together model houses from plastic kits sponsored by the manufacturers at the big model railway trade fairs (photo from the website of the Leipzig trade fair)

There is a Christian Model Railway Team which has groups in many parts of the country. It has the concept to get in conversation with hobby colleagues at model railway events and then speak with them about Jesus when possible: [www.cmt-ev.de](http://www.cmt-ev.de)



The Berlin group has offered a modeling class in a refugee accommodation for many years. I could ask for photos of this if you are interested.

I have collected toy and model trains in all scales from 1:220 to 1:22.5. In 35 years I have always exhibited different parts of my collection, most of it is off the mainstream.

The Lehnhardt O scale tinplate trams were discontinued as the next generations of the owner family are not interested in them.

I once discovered the O scale streetcar rails for models with current pickup from a center rail called SuperStreets from K-Line by Lionel. I got a test oval and found they look nice. When I wanted more of them, bigger radii and switches, they were no more available.

I liked the G scale trolley and interurban models from Hartland Locomotive Works, seen here: <https://kleintischspielbahn.forumieren.net/t4124-spur-iim-spielanlage#18132>, but this enterprise ended production, too. Most of the German tram models further up and down on this forum website were made from people I know in very small series.

I think the toy train system that offers the most possibilities for creativity to children is the Lego railway: <https://kleintischspielbahn.forumieren.net/t4340-lego-eisenbahn>

I wished to have it when I was a boy, but could buy it only as an adult. There was a time when metal rails were offered so you can run it like any model railway. Running the trains by the power of one's own muscles is more fun than just pressing a button of a remote control. At my Hogwarts Express exhibition, the 4 year old visitors were happy to push the train on the outer circle by hand when walking around the tables. I am considering to offer a corner for building with Lego bricks in my future museum.



I was so lucky that I could buy this building some months ago: The history of this small electric railway opened in 1927 and closed for passengers in 1969 and freight in 2002 is here:

<https://de.wikipedia.org/wiki/Extertalbahn>

The museum railway association is here:

<http://www.extertalbahn.de/>

I have worked as a tram driver from 1981, so I have only an average income. My 86 year old mother who was a teacher gave me the money in advance of the heritage. I think it will be the best if you summarize the relevant topics rather than quote my English full of mistakes. Greetings from Berlin.

Thank you Christof for providing this information. Good luck with your Museum. You can reach Christof at [Christof.Henseler@newtracksmodeling.com](mailto:Christof.Henseler@newtracksmodeling.com).

This next modeler lives in South America and builds military models. He writes me in Spanish and I write him in English. While he does not speak English he understands it. Thanks to Google Translator I can translate his Spanish into English so we can communicate. I think you will agree his modeling is excellent. Please meet:

### Eduardo Ovalles



I started modeling 15 years ago. I always built airplanes and helicopters, and starting in 2016, I built a T 72 for the first time and I became familiar with battle tanks and trucks from all eras, although I really like WWII, always 1/35 scale planes and 1/48 helicopters.

The 1/35 scale is ideal since it is neither very small nor very large, it is possible to give realistic life to a plastic model and turn it into a work of art with respect to modeling.

From a very young age, my older brother made models, I am talking about the 80s or so. He and I were left looking at this as great and the best and most perfect toy. Once my brother left it, I got some model Testor paints with which I started in modeling. I didn't have an airbrush or compressor. I had to buy a can of propellant and then a small Testor airbrush to paint. Of course, that model was painted badly, but I decided to do it well, and learned to ask questions. At that time there were good model shops in Caracas, which is where I'm from.

Then, as the years went by, I moved to the city of Valencia, but Long before that, in 1982, the family traveled to South Korea. My father is a ship captain in the Venezuelan Navy. We went to build 4 ships for the Venezuelan Navy of the LST type for the marine infantry. In South Korea, in a town called Massan where the Korea Tacoma shipyard was located for the marine industry. I began to see many models in small stores and they were so cheap that I could go down to the store and buy one after the other. That was when I understood that this would be my hobby for always.





Eduardo says prices of kits are much higher there than here in the USA. This, combined with a lack of material, makes modeling difficult for him. He says “I know I could make better models, but here in Venezuela it is difficult and very expensive to get a model that costs over \$120 when what you earn monthly here is hopefully \$200 – so I buy food or models hahaha”

We think our modeling costs are too high. This shows what passionate modelers in other countries will pay! Eduardo has never had a mentor and says everything he has learned is from tutorials and his

passion for modeling. His passion is obvious in the photos he sent me.

Thanks Eduardo for sharing your modeling and for your help and interest. You can reach Eduardo at: [Eduardo.Ovalles@newtracksmodeling.com](mailto:Eduardo.Ovalles@newtracksmodeling.com).



Now please travel back in time with me. Remember when many of us started in model railroading!

Love Lionel Trains? Who Me?

Anyone ever hear of Lionel? I know I have. I think I got my first one when I was old enough to crawl. My passion for Lionel never left me. Even after I became a scale modeler I still became a Lionel Ambassador in 1997, and helped promote Lionel products.

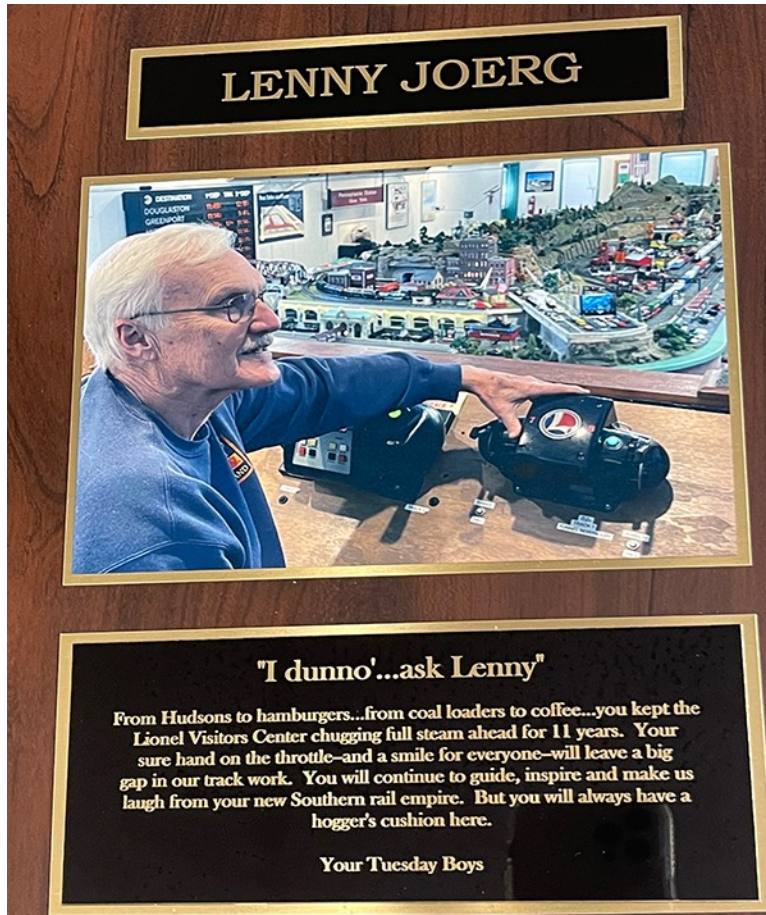
Having lived in Grosse Point Park, Michigan, I had the opportunity many times to visit the Lionel Headquarters and see their displays. What a treat. Also, the then owner of Lionel bought Madison Hardware Hobby Shop in New York City and moved all the stock to a facility in Detroit. Another great treat for a lot of us local modelers who had visited the New York City shop in its heyday and had fond memories of meeting the owners and picking up pieces of original Lionel items we never dreamed of finding.

My last visit I was able to buy every single part and item on my wish list. While I had a great time in the store, my wife was not impressed with the location of the Store and decided to stay in the taxi and go back to the hotel.

I was a long time collector and member of the Train Collector Association (TCA) and attended the wonderland for collectors called The TCA Eastern Division, York Meet, every April and October. All my collection was sold when I retired in 1998, but memories of those days are still as fresh as if they happened yesterday.

With that said, I want to introduce a current Lionel modeler who also does custom designs using old Lionel, which is too far gone to be considered collectible. I always enjoy seeing the various kinds of modeling done in our hobby. Please meet a truly dedicated and talented Lionel railroad modeler.

## Lenny Joerg



Like many of my generation, I grew up with Lionel trains under the Christmas tree. Each Christmas, our train set would grow by one or two new pieces. Once the high school years, and then the college years came, the trains were packed safely away.

After getting married I again put my old trains under the Christmas tree. The Christmas layout started out as a single loop and developed into a multi-level layout running four trains at the same time with a number of operating accessories.

I became aware of local train shows and started to increase my collection. There were shows almost every weekend on Long Island so I got to meet many other collectors and operators. I now realized there was a market for older trains so I started selling on the swap meet pages of the national train clubs. I was able to learn how to repair the trains so I could buy broken items and repair them for profit.

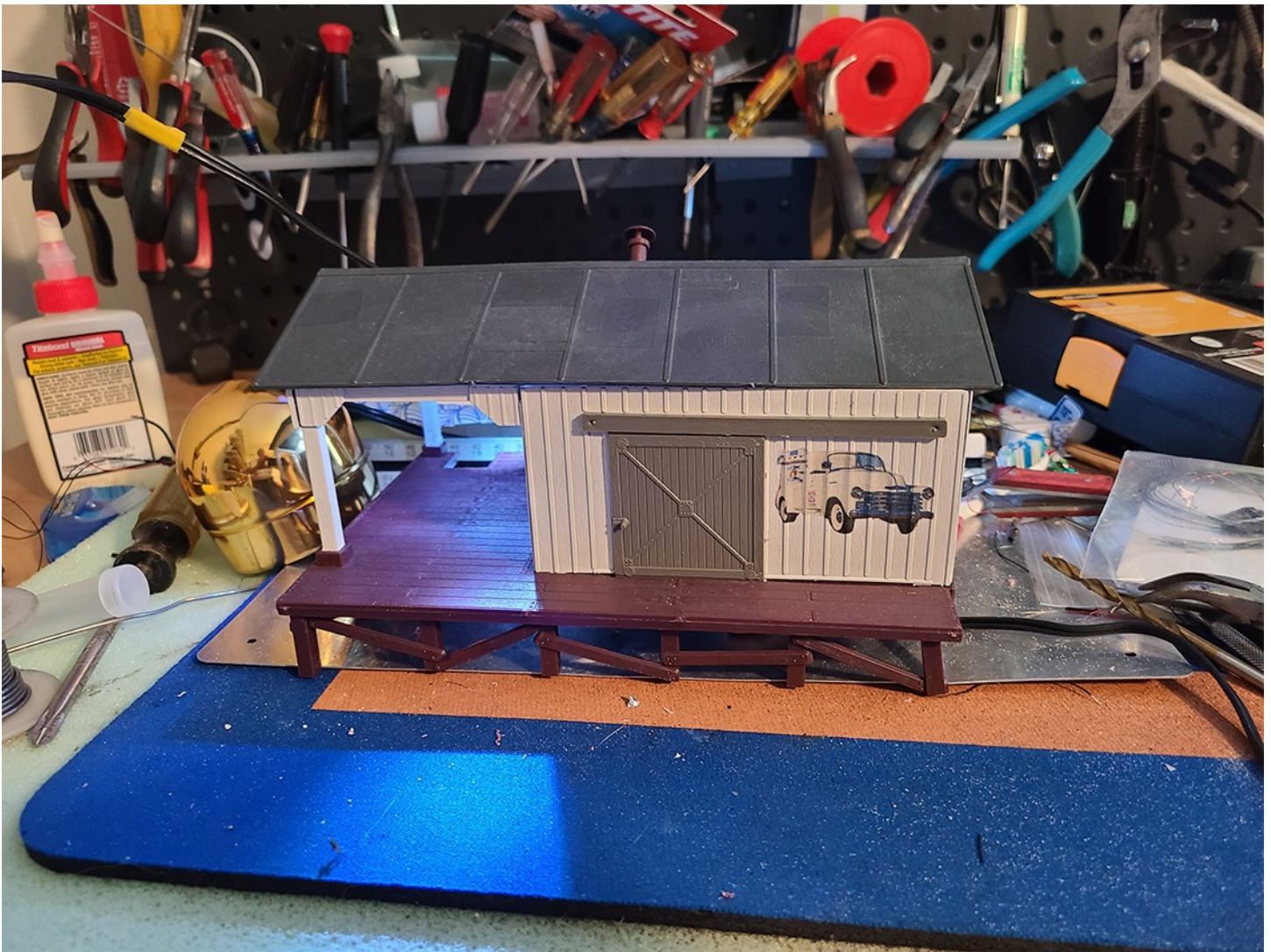
I saw an ad for a 397 coal loader from a local seller and I met him at one of the train shows. This was the beginning of a long-term friendship that

continues today. Les, the seller, organized bus trips to the TCA York train meet and he convinced me to go. I was hooked and went every six months. The trip grew to two charter buses and when Les' partner quit, I became part of the Charter bus crew for 10 years.

In the early 2000's, one of the bus riders suggested I join the Railroad Museum of Long Island (RMLI). The reason this interested me was the museum had just gotten the Lionel Visitors Center Layout from Lionel in Michigan. I helped on weekends since I was still working at the time. I volunteered at RMLI for over 10 years.

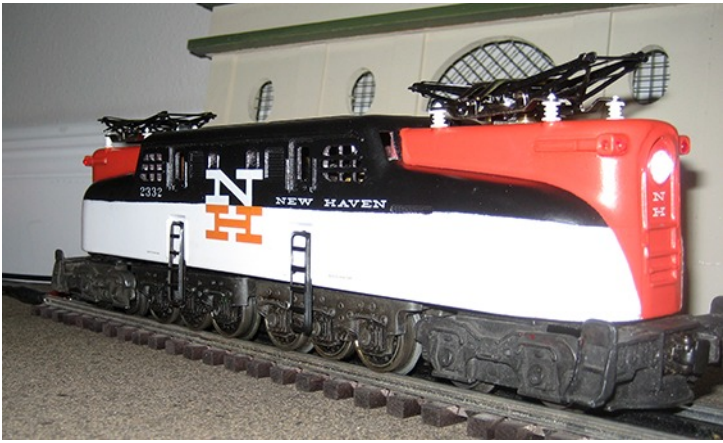
The photo of me at the controls of the Museum layout was taken by the late Ron Hollander. Ron was the author of the book "All Aboard" and it was a detailed story of the Lionel Corp. Ron was a member of RMLI and a good friend

One of the RMLI mentioned to the owner of the local hobby shop that I repaired Lionel trains. We met and I did repairs for him until he closed up his shop. I also did repairs for one of the largest dealers on EBAY. Jeff was located only a mile from where I lived so it worked out well till we moved south.



I've attached some photos of some of my custom work. None of these pieces were collector items as they were damaged or made up from parts.

The Good Humor warehouse above, the GG1, the LIRR snowplow and the lighted NYPD cars are all lit with LEDs purchased from Evans Designs.



I've authored articles for the *Switcher*, which is the Lionel Operating Train Society's magazine and one article in the *TCA Headquarters News* about the buses to York.

Lenny, thank you for sharing your model railroading Lionel experiences. Without Lionel, many of us may never have been exposed at an early age and fallen in love with our great hobby. I think we all need to remember those early days. Do kids today get that kind of exposure to our hobby? Lenny can be reached at: [Lenny.Joerg@newtracksmodeling.com](mailto:Lenny.Joerg@newtracksmodeling.com).



Here is another dedicated railroad modeler and information about his club and their activities for our hobby.

### Frank Deuter

Corddry Company Crossing Model Railroad Club

O Scale 3 Rail Layout

Upstairs of Pocomoke River Canoe Company

2 River Street Snow Hill, MD 21863

[snowhillmodeltrains@gmail.com](mailto:snowhillmodeltrains@gmail.com)

[snowhillmodeltrains.com](http://snowhillmodeltrains.com)

Facebook: [@snowhillmodeltrains](https://www.facebook.com/snowhillmodeltrains)

Instagram: [corddry\\_company\\_crossing](https://www.instagram.com/corddry_company_crossing)

YouTube: [Corddry Company Crossing Model Railroad Club](https://www.youtube.com/CorddryCompanyCrossingModelRailroadClub)

Shore Boyz Facebook: [@shoreboyzrailroadclub](https://www.facebook.com/shoreboyzrailroadclub)

Shore Boyz Instagram: [shoreboyzrailroadclub](https://www.instagram.com/shoreboyzrailroadclub)

I got my first train when I was five years old, found under the Christmas tree of course. My uncle and grandfather started taking me up to the Snow Hill trains at just about that time. Roland Collins, my mentor (passed away in 2016), let me run the trains and eventually showed me little bits of how to model a railroad. He and a few other folks including Barry Laws, proprietor of Pocomoke River Canoe Company, worked hard to create a layout with details and cool ideas such as real running water.

They also included many custom buildings that are located in Snow Hill such as the hardware store and Uncle Charlie's farm. I took over the layout and the maintenance when Mr. Rowland passed away. Since then, I have been hooked on the hobby. I bought my first locomotive, a MTH Railking PRR L1s to play with in my dorm room. I now have multiple locomotives (all MTH), mostly PRR, with a current 57 car, all PRR and shorelines. We are currently working on modeling a passenger station on the club layout along with a model of the new "skyscraper" in Salisbury, MD, which is going to be 46" tall! Most of my skills have been by trial and error and tons of YouTube watching.



*Carnival and farm scene. Carnival added in 2022.*

### The Club

The club started in 1992, with Barry Laws, Rowland Collins, Smitty Smith, and a few others starting a couple laps of tracks around the second floor of Pocomoke River Canoe Company, in a historic structure built in the 1930's.



*Homemade roundhouse updated in 2019.*



*PRR K4 Pacific Smoking Away.*



*Homemade model of Paul Jones Lumber Mill in Snow Hill, MD made by Rowland Collins and Steve Brown.*



*Photo of bridge and scene currently being updated (2023). Bridge crosses water well for water pump. And yes, the mill actually moves with water coming off the mountain.*

Since then, the tracks have expanded to 4 mainlines with multiple yards. The layout features tons of custom buildings (currently working on modernizing them with LEDs, fresh paint, etc), real running river (revamped in 2022 with new pump and added filtration system), chain driven highway scene, and many other neat features.

The layout is completely around the viewer, with the stairs coming through the middle of the layout. The layout consists of 1 large, 4 mainline, 3 rail O scale tracks with multiple yards, a small 3x6 layout for Thomas, and new in 2023, being built by the young guys in the club, a 4x9 HO layout consisting of 1 DC inner loop and 1 DCC outer loop with a small yard on top of the old rope pulled elevator hole. We are open to the public during select First Friday evenings, Blessing of the Combines (town event), Snow Hill Fireworks night, and weekends around the holiday season. We have work sessions on first and third Fridays (except in heat of summer and bitter cold of winter). Membership as of now is to come and help out. We are working towards getting 501c3 Non-Profit certified to help pay some our bills, along with getting grant funding to do programs for kids and adults.

New in 2022 and will be back in 2023, we do kids train races with prizes for ice cream from the sub and shake shack in town. We are working on doing an “adult” league with prizes to other businesses in town, as asked about from many people on social media. We also started “Young Engineer’s Camp” in 2018, bringing it back in 2023 due to Covid-19. This camp brings in kids and their parents to learn how to run model trains and how to model trains at their own houses. This program has recruited 2 kids and their parents to the club, and all 4 have been critical in keeping the club alive.



*Scene of houses along river with real water! Area being updated with passenger and restoration yard inspired by Reading and Northern Railroad in 2023.*

We use MTH's DCS system for the most part, 2 tracks being fixed and 2 variable. We are currently working on expanding the layout with a 15'x28" passenger and restoration yard scene, with the kids in this year's camp hopefully learning how to lay down grass and ballast. There is always something to fix up on the layout, with a section each year to modernize and make fresh for the visitors during the holiday season.

We work closely with our club to the north of us, Shore Boyz Railroad Club, who operate the largest mobile layout on Delmarva. They travel to many events and businesses in the area with their 48' trailer filled with O and G scale trains, sharing the hobby with many communities on the Eastern Shore.

Thank you Frank for all your interest and help. [Frank.Deuter@newtracksm modeling.com](mailto:Frank.Deuter@newtracksm modeling.com).



*Young guys in the club running their 4x9 layout. Currently being rebuilt to run all HO.*



*Photo of Shore Boyz mobile layout. Photo by Danny Burris.*





This next modeler has tried many scales, including On3 and On30. He loves to create scenery. After all, scenery is what sets the stage for our trains to operate. Please meet:

### Scott Robertson

I live in Rocklin, California. My model railroad is named: The Never Done and Always Changing (ND&AC). My interest in model railroading dates back to the late sixties. As a child, my grandfather helped me build a 4x8 HO scale layout with an oval of track that I could play with in our basement during the winter months. During the warmer months, the layout was hoisted to the ceiling. On Christmas 1969, I was given a gift certificate to a local hobby shop where I bought my first *Model Railroader* magazine featuring John Allen's model railroad. I was amazed at the artistic depth of the scenes, and I knew from that moment on, I wanted to dive deep into this hobby. I

have an artistic background, and before retiring in 2022, I practiced Landscape Architecture design. Over the years I have constructed multiple layouts in basements, spare rooms, my son's bedroom, a garage, modules, and currently in a dedicated room in my home.

Although I started out in HO scale, my scale interest moved to 1:48 scale On3, On30 and now 1:24 scale. Throughout all of these scales, my modeling focus is more on scene creation than operating trains. Trains do run, but the primary focus is on landscape scenery and structures. In each situation, I strive to create realism in both, but not to the extent that everything constructed is a contest quality model. My modeling philosophy is more along the lines of set design for theater production. I tend to only model what you can see and I exaggerate texture and color based on a viewing distance of 18-36". Color blending and texture are important but, the composition and viewing angle of the scene is equally important in my mind.

I am generally self taught through research, observation and trial and error. But I have been influenced by modelers like John Allen, Malcom Furlow, John Olson, and Troles Kirk. I am also driven by inexpensive approaches and expediency.





I like to experiment with many different scenery, structure, and weathering techniques, finding that many solutions are better than one and keep things interesting. As previously mentioned, my modeling focus is on structures and scenery. As such, I scratch build most of my structures using many different techniques and building materials. My favorite structure materials are probably balsa adhered to foam core and realistic photo textures adhered to foam core. I like balsa because it is very easy and fast to cut, paint and weather. When using photo textures I like the realism it produces and the minimal time it takes to build the base structure. I constantly notice visitors touch testing my structures using photo textures to determine if it is real, or I am asked how I painted it so realistically.



After many years of modeling in 1:48 scale, I determined I needed a new challenge. I have always been intrigued with large-scale modeling, but only thought of it for outdoor layouts. I first experimented with the bashing of some Bachmann equipment and constructed a couple of simple buildings. I really liked the detailing aspect and the fact that I could see more with my aging eyes, so I decided to convert my 1:48 scale On30 layout to 1:24 scale. I kept the bench work and some landscape scenery in place, plus I followed

the same route, just in a larger scale. My radius track is small, but I compensate by running small locomotives and shorty cars. Since changing scales, it renewed my interest in the hobby and forced me to explore the detail of scenes even more.

I believe my biggest contribution to mentoring others is in structure and scenery building. I also promote the idea to “just do it”. I find too many modelers get analysis paralysis by over researching or worrying everything is not perfect before they start working on a model. I also encourage others to experiment and not expect every technique to work or look right the first time. If not, you will find your self getting discouraged, stalling your modeling, and then missing out on why “Model Railroading Is Fun!”

[www.scottrobertson30.com](http://www.scottrobertson30.com): Website with images of my models, layouts, modules and links to my YouTube channel.

[Scottrobertson56](https://www.youtube.com/channel/UC56): My YouTube channel consisting of various layout videos, how to videos, and Trainz 2019 design videos. Look for my postings on [Facebook](https://www.facebook.com/Scott.Robertson). Thanks Scott for all your help. You can reach Scott at: [Scott.Robertson@newtracksmodeling.com](mailto:Scott.Robertson@newtracksmodeling.com).

## **New Tracks Modeling Announcements**

Please follow my Facebook page, [Jim Kellow MMR](#). for breaking news about New Tracks Modeling. Something new is always happening for modelers like you.

Subscribe to our website, [newtracksmodeling.com](http://newtracksmodeling.com), to get all the current information about our future Zoom and YouTube shows and get a weekly email with the log in links for our Zoom shows. We currently have over 900 website subscribers and encourage you to join us. Or, you can join us live on our YouTube channel, New Tracks Modeling, or watch the videos of our past shows. We have over 1,000 videos available and over 1,500 subscribers. All our shows are live with segments presented by a great variety of modelers, manufacturers local clubs and organizations, and hobby shops. We hope you will join us. You are why we do these shows.

## **CatzPaw Innovations LLC Surprise**

I will be writing about their new system for moving things other than trains in the next issue of this magazine. This is the type of system I think I have been looking for to move my motorcycle club members and my farm equipment. Congratulations to CatzPaw for developing this system. Read more about the company and their invention next month. I think you will be glad you do. Meanwhile here is what I just got from the company.

CatzPaw Innovations LLC designed, 3D printed, and started selling a Big Foot figure in 2014. From the very first sale customers have asked CatzPaw to make Big Foot move through the woods. They have spent the last nine (9) years trying to make that happen.

They are proud to announce the release of the InvisaTrax™ Transport System. A system that makes it possible to move things! Lots of things in the most popular scales.

CatzPaw has launched a Kickstarter campaign to fund the manufacture and distribution of the InvisaTrax™ Transport System. The Kickstarter campaign will run through the month of October 2023. For more information about the Kickstarter campaign and InvisaTrax™, follow this link: <http://kck.st/3PGeB2m>

YouTube show video: [https://youtu.be/xkSbP3je0yo?si=Km\\_i7DiRkD7LqNvD](https://youtu.be/xkSbP3je0yo?si=Km_i7DiRkD7LqNvD)

## **Building a Billboard**

Dan Cohen's contest entry of a scratchbuilt New Tracks Modeling sign at the Thousand Lakes Region NMRA Convention.

Here is a 2 1/2 minute video about Dan Cohen's New Tracks Modeling sign. Learn how he built it, and how he entered in the Thousand Lakes Region Convention Contest in May, 2023. Look at the video to see how his sign did in the contest.

[https://drive.google.com/file/d/1L89IHC1hv\\_OqWuJj56e5HzaHRy9QXoEa/view?usp=drive\\_web](https://drive.google.com/file/d/1L89IHC1hv_OqWuJj56e5HzaHRy9QXoEa/view?usp=drive_web)

Great idea Dan. I want one! Thanks for sharing the video. Anyone else doing anything like this, please let me know so it can be shared.

## **Battery Power?**

Steve Sherrill and his guests will tell you all about it.

Steve Sherrill began a new series on the September 27, 2023 New Tracks Modeling show on the installation and use of battery power for motive power operation.. He and his guests plan to show how to get started with basic battery power. Steve plans to use some very basic equipment to show how easily battery power can be used with DC, and DCC. If you plan to consider battery power in the future, don't miss Steve's presentations.

Based on feedback I received from viewers about Steve’s first battery power segment, they are going to be a huge success. If you miss any of his live presentations, you can see them on our YouTube channel New Tracks Modeling. If you have any questions, contact Steve directly at: [Steve.Sherrill@newtracksmodeling.com](mailto:Steve.Sherrill@newtracksmodeling.com).

**New Organizational Sponsors for New Tracks Modeling. Thank you to these new sponsors for their interest and financial support:**

[Brennan’s Model Railroading](#)

[O Scale Central](#)

[National Association of S Gaugers](#)

[National Capital Trains](#)

[Mainline Hobby Supply](#)

[New Creations Victorian Railroad Buildings LLC](#)



Thank you for joining our New Tracks Modeling Sponsor team. Your financial support is greatly appreciated. We look forward to working with these organizations to promote modeling and mentoring in our great hobby.

Thanks to these organizations for their support of New Tracks Modeling:

Thanks to these organizations for including our Inaugural Scholarship Winner and the Announcement of the start of our Next Year’s Scholarship program in their publications.

1. The [National Retail Hobby Store Association](#) (NRHSA) for inclusion in their Hobby Merchandiser October 2023 magazine.



2. The [National Model Railroad Association](#) (NMRA) for inclusion in their September/October E Bulletin.



We look forward to additional support for New Tracks Modeling and our Scholarship program in these and other organizations publications.

**New Tracks Modeling’s monthly newspaper column is helping reach a potential new model railroading audience.**

In my last article, I told you we are doing everything we can think of to promote railroad modeling, and mentoring. My September 13, 2023 article in the “Live and Learn” section of the *Citrus County Chronicle* newspaper tells about our inaugural scholarship’s winner. Everyone who helped us create our New Tracks Modeling Mentoring Scholarship deserves a very special thank you. “THANK YOU ALL!” Now on to next year’s program.

Here is my Facebook post about this article

“Can you believe my article is recommended reading on the front page of my local newspaper? Well it is! Best of all, the article is about the inaugural winner of the New Tracks Modeling Mentoring Scholarship, Ethan Bernstein from Maryland. Truly a dream realized by New Tracks Modeling. Please help keep the dream alive by donating to our next year’s scholarship program at [newtracksmodeling.com/scholarship](http://newtracksmodeling.com/scholarship)”

Wouldn't it be great to have articles by model railroaders in newspapers all over the country? What a boost this could give our hobby. What great past childhood memories of their model railroading might people remember that would cause them to restart their participation in our great model railroad hobby?

I hope many of you will write articles for your own local newspaper. If your newspaper is like mine, they will be glad to hear from you. I tried to get the Associated Press to offer my articles to other papers, but no luck since I am not a full time employee of a newspaper. So it is up to all of us to spread the word about our fantastic hobby one newspaper at a time. If any of you get an article published, please let me know so I can include your accomplishment in a future article. Got a question or need help getting an idea written? My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com). I'll be glad to help you get the message out.

### **Today's Model Railroading community made the inaugural 2023-24 New Tracks Modeling Mentoring Scholarship successful.**

“Thank You, Thank You All!”

Thanks to members of the model railroad community, we've successfully raised donated funds to pay for the first year of our New Tracks Modeling Mentoring Scholarship program. It is also very exciting for us to have received very impressive applications from young modelers. Thanks again to everyone who contributed to make all this possible, including all the organizations and businesses who supported our program. We had seven modelers submit applications, and while we could only award one scholarship this first year, we have encouraged all the applicants to reapply for next year's program. Next year, with your support, we hope to offer more and larger scholarships.

Two things The New Tracks team and the model railroad community can do to help make our 2024-2025 scholarship a success:

#### **1. Donate to New Tracks Mentoring Inc., our 501(c)(3) Florida Non-Profit Corporation**

New Tracks Mentoring Inc. grants scholarships to qualified HS graduates who will be, or are pursuing, a STEAM (Science, Technology, Engineering, Arts or Math) program at a two-year or four-year college or university or an accredited technical school. The Officers of the Corporation are: Jim Kellow MMR, President; Bob Davidson, Vice President; Phil Edholm, Secretary; Kevin Macomber, Treasurer; and Jeffery Joyner, Attorney.

We have set a goal to grant at least \$4,000.00 in scholarships for the 2024-2025 academic year and are soliciting donations to help reach, and hopefully exceed, that goal. As a registered 501(c)(3) non-profit corporation, your donation to the scholarship is tax deductible as allowed by law.

There are several ways that you can donate to the New Tracks Modeling Mentoring Scholarship:



The simplest way for individuals or organizations to donate is to use the Zeffy platform. We have chosen Zeffy as they insure that 100% of your contribution will go to the scholarship. There are no processing or administrative fees. [To use the Zeffy platform, please click here.](#)

If you wish to make a contribution by check, make the check payable to New Tracks Mentoring, Inc., and mail it to:

New Tracks Modeling Mentoring Scholarship  
c/o Kevin Macomber  
1140 Riverview Road  
Dauphin, PA 17018

**Special Challenge Grant of \$1,000 to match the first \$1,000.00 of individual donations of \$80.00 or less.**

An Anonymous donor to the Scholarship fund has made a \$1,000.00 donation challenge to New Tracks Scholarship donors. The challenger will match up to a maximum of \$1,000.00 of a pool of all public individual donations of \$80.00 or less made to the New Tracks Modeling Mentoring Scholarship between today and December 31, 2023. For your convenience you can use our Zeffy account to make any size donation you want.

The New Tracks individual donor contributions will be recorded and shown each week on our Wednesday evening New Tracks Modeling Zoom show so we all can follow how close we are to achieving our Challenge Goal. All of your help in achieving this goal is greatly appreciated. Remember every \$1.00 donated to meet this specific Challenge is really a \$2.00 donation to the scholarship fund.

## Corporate Donors

There are several tiers of donations available to companies who wish to support the New Tracks Modeling Mentoring Scholarship. All companies who donate at any of these levels will be eligible to have their logo and links on the scholarship page of the New Tracks Modeling website for that annual cycle (one year cycle after the award of the previous year scholarship):

Companies that donate \$250 in any annual scholarship cycle will be identified as Brass Donors; Silver Donors will be recognized for a \$500 contribution, Gold Donors for a \$750 contribution and Platinum Donors for a \$1,000 contribution. Donors who contribute at least \$2,000 will be listed as a specific scholarship sponsor. Please contact us at: [NTMMS@newtracksmodeling.com](mailto:NTMMS@newtracksmodeling.com) to arrange for your corporate donations. Donate early to maximize your exposure. Remember, your contribution will help the young modelers who are the future of our hobby.

I am pleased to announce our first two Corporate BRASS donors are:

1. [New Creations Victorian Railroad Buildings, LLC](#) Owned by Alan Rogers
2. [Brennan's Model Railroading](#) owned by Dennis Brennan



New Tracks Mentoring, Inc. is a private, non-profit 501(c)(3) corporation registered with the Florida Department of Agriculture & Consumer Services. A copy of the official registration and financial information may be obtained from the division of consumer services by calling toll-free 1-800-435-7352 within the state. Registration does not imply endorsement, approval, or recommendation by the state.



## 2. Solicit/Mentor/Educate Applicants about our 2024/2025 Scholarship Program

We will begin accepting applications for the 2024-2025 academic year on January 1, 2024. Our goal is to award at least \$4,000.00 in scholarships for 2024-2025 (number and amount will be announced January 1, 2024). We recommend that interested applicants download a copy of the application for review even if planning to apply on-line. If you have any questions, or need help completing your application please email us at: [NTMMS@newtracksmodeling.com](mailto:NTMMS@newtracksmodeling.com).

Helping applicants apply for our scholarships is one form of mentoring we can offer.

Qualified applicants MUST meet the following criteria:

- Must currently be either a current high school senior or a high school graduate living in the United States.
- Are planning to or currently attending a two-year or four-year university, college or accredited technical school in academic year 2024-25 with an intent to pursue a degree in one of the STEAM fields.
- Have a current GPA of 3.0 or higher.

- Demonstrate participation in either a model railroading or other modeling youth activity program such as Youth in Model Railroading®, Scouts MRR program, 4H model railroading, MRR Youth clinics, active participant in building a home layout, or belong to an organized model railroad club. Participation in general modeling clubs and activities will also be considered.
- Write an essay of at least 500 words describing how their involvement in modeling and model railroading as a hobby has impacted their lives and benefited their education within one or more of the STEAM areas of academic pursuit.
- Demonstration of having designed and/or built models by submitting photos and descriptions of at least two models completed by the applicant.
- Provide two references to affirm applicant's involvement in railroad or other modeling activities. (NTMMS will be contacting any listed references, so please let them know ahead of time.)

Please note: Scholarship funds will be disbursed directly to the Bursars Office of the school the student will be attending. Any NTM staff member or relative is prohibited from applying for this scholarship.

Applications can be submitted on-line or by mail.

Click here to download an application for review.

On-Line application will be available January 1, 2024.

Please note that you MUST have a Google Account to complete this on-line form as you will need to upload files.

If you have questions or need assistance contact us here at: [NTMMS@newtracksmodeling.com](mailto:NTMMS@newtracksmodeling.com)

### **Help Show the Scholarship Banner at Events**

Bob Davidson generously donated three banners through his company, Exhibits And More, which will be shown at train shows and rail expos across the country.

Each banner is 3' wide and about 7' tall and has a QR code on it to link you directly to our website to get more information including the application to apply.

It's a retractable unit that is very easy to ship and set-up. So, if you want one for your event, let Bob Davidson know at: [bobdavidson@newtracksmodeling.com](mailto:bobdavidson@newtracksmodeling.com).

In addition to the banners, we can also provide handouts about the Scholarship program to local events we are not able to attend in person. All you have to do is contact Bob Davidson at: [bobdavidson@newtracksmodeling.com](mailto:bobdavidson@newtracksmodeling.com).

Look for New Tracks Modeling at any show you attend. We hope to be represented at as many shows as possible for our 2024,2025 program. If you want to take one of our Banners, or help man a table, or have us be a part of your table, contact Bob Davidson at: [bobdavidson@newtracksmodeling.com](mailto:bobdavidson@newtracksmodeling.com).

Remember, one of these years, your kids or grandkids may benefit from the New Tracks Modeling Mentoring Scholarship program that you helped start. I truly hope so. Thank you.

Thank you to everyone who helped us display the banners or pass out our handouts at shows around the country last year. We are currently taking reservations for our banners for our second year's (2023-2024) Scholarship program. To let us know if you can help show a banner, please contact: Bob Davidson at [bobdavidson@newtracksmodeling.com](mailto:bobdavidson@newtracksmodeling.com).



New Tracks Mentoring Inc, is the only organization, we are aware of, comprised solely of volunteers from the model railroad community who have created a nonprofit 501(c)(3) company whose sole purpose is to provide scholarships throughout the United States for the benefit of young modelers who are pursuing a STEAM education after High School graduation, in a college, university or accredited technical school.

The contributions and help by the current model railroad community can help the next generation of modelers provide the leadership and continue the traditions of our great hobby. For more scholarship information and to donate, visit our website. [newtracksmodeling.com/scholarship](https://newtracksmodeling.com/scholarship).

## **You, the New Tracks Modeling team of volunteers and donors, help make NEW TRACKS MODELING Zoom shows possible. Please continue your support.**

Thank you so very much for all the financial and volunteer support New Tracks Modeling is receiving for our Wednesday and Thursday shows. It is obvious to me that, you, the supporters who finance and the volunteers who produce the shows, make presentations, and do all the many jobs necessary to make our shows possible, are doing an outstanding job. I know this because of the favorable emails from viewers, the growth in the number of subscribers and viewers, and the interest shown for our programs by the model railroading community. It is all of you who make New Tracks Modeling the show it is today. Again, thank you all.

I truly hope each of you are as proud as I am of your contributions and achievements that are creating New Tracks Modeling as the modelers and mentoring place to be for current and future model railroaders. This is what sets us apart: "Mentors Helping Modelers Build". I believe the great part is we are only getting started in helping the younger modelers develop into the modelers and mentors of tomorrow. With your continued help we have a lot more coming.

## **Everyone can help fund our New Tracks Modeling Zoom shows out of pocket costs**

### **1. Please Donate to our New Tracks Modeling Patreon Account**

[Donations to Patreon](#) are not associated with our Scholarship program or personnel costs, but rather cover the out of pocket costs of just getting our Zoom, and YouTube shows produced, to you, our website developed, etc. [We have a Patreon account, NewTracks Modeling](#), for your donations to help fund these costs. We hope you can make a financial contribution. Please press this link to donate to help pay for our NewTracks Modeling shows.



### **2. Monetize our YouTube New Tracks Modeling Videos**

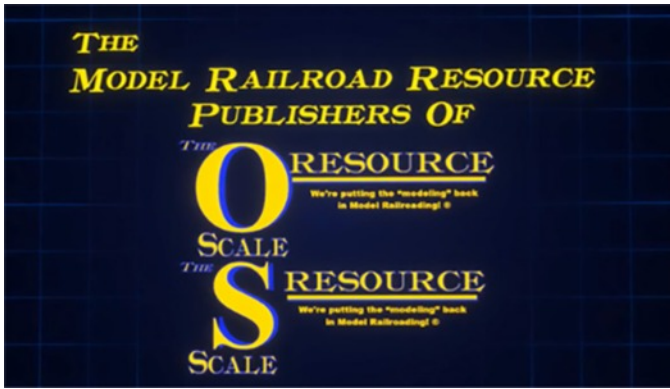
We also have a monetized New Tracks Modeling YouTube account. If you watch the advertising YouTube places on our videos, we earn a little money from YouTube. Thank you in advance for your help in watching these advertisements to help us fund our Zoom shows.

Please visit and subscribe to our website, [newtracksmodeling.com](https://newtracksmodeling.com), to get all the current information about our future Zoom and YouTube shows and weekly emails with the log in links for our Zoom shows. Join us live on Zoom or on our YouTube channel, New Tracks Modeling. By the way, we have almost 1,000 videos of our past shows on our YouTube channel for you to enjoy. They are all free! Don't forget to check out the advertisements on each one. I hope you enjoy and learn something from each one.

## **NewTracks Modeling "MY BUILD": Sponsored by Model Railroad Resource LLC.**

Starting in January 2024, the "MY BUILD" segment within the New Tracks Modeling show on Zoom will take over the show once a month.

The main purpose of MY BUILD is to provide a platform for modelers to showcase their past and ongoing projects, highlight their achievements, and seek advice or assistance if needed. These segments are designed to be supportive and non-judgmental so modelers can help each other learn and develop their modeling skills.



Allowing the MY BUILD segment to take up more time in the monthly show will provide modelers with the opportunity to have more extensive and in-depth discussions about their projects.

Every viewer of New Tracks Modeling is encouraged to send in one or more photos of their modeling to the show's host, Chris

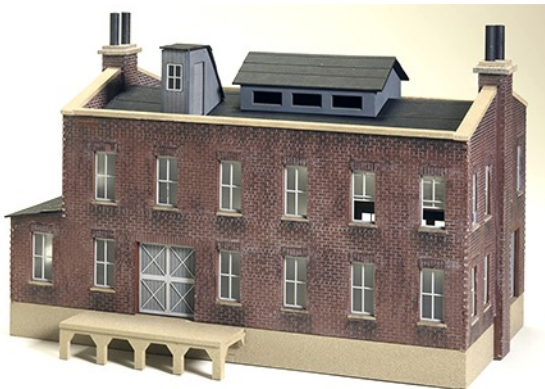
Coarse, in advance of the next MY BUILD show. Chris then organizes these photos into a PowerPoint slide show. During the show, each participant is given the opportunity to discuss their slides. They can share valuable tips, demonstrate techniques, answer questions from the audience, and even pose their own questions. MY BUILD is designed to serve railroad modelers of all scales, gauges, and age groups.

To make things more exciting, some months MY BUILDS include challenges. These challenges typically revolve around seasonal themes and aim to encourage a diverse range of projects within the modeling community. Additionally, these challenge shows are open-ended enough to include a wide range of subjects. If you want to participate in our next monthly MY BUILD, send your model photos with a brief description and your name to Chris Coarse [atrailrunner130@hotmail.com](mailto:atrailrunner130@hotmail.com). Host Chris Coarse is the owner of [Conowingo Models](#).

*See the "MY BUILD" Models Shown on the September, 2023 Show in this issue.*

### **Brennan's Model Railroading**

Starting November 1, 2023, Dennis Brennan, owner of Brennan's Model Railroading, will begin a Build Along of his kit modeled after a Frank Ellison kit of the Sanky Wanky Coffee Company. A 10% discount is available to modelers for who want to build this kit with Dennis for two weeks prior to November 1, 2023. To get the discount please enter the code NEWTRACKS when ordering. For more information please visit his website:



<https://www.brennansmodelrr.com/> See below for another Build Along with Dennis Brennan using his ballast.

### **Dylan Lambert Locomotive Works**

Starting December 6, 2023, Earl Hackett will build Lambert Locomotive Works new Commuter Coach in HO scale as a Build Along on our Zoom shows December 6, 13, 20, and 27, 2023. A 20% discount on the model will be available to New Tracks Modeling viewers between November 10, and December 27, 2023. To get the discount please use the code NTRACK when ordering. [More information about the Lambert Locomotive products is available on their website.](#)



### **2024 Build Along Modeling**

Happy New Year and Very Successful Modeling to Everyone!

I can't believe this is my last article for *The O Scale Resource* magazine for another year. The next issue will be the Seventh year for New Tracks Modeling articles. Time sure has passed so fast, but every article has

been fun for me to write and hopefully helpful to you in improving your railroad modeling. Here is the start of our 2024 adventure.

## Frenchman River Model Works

Starting January 10, 2024, Frenchman River Model Works Build Along with Tom Farrell building two kits. A Lobster Boat and the Lobster Shack. There is a 20% discount on the HO and O Scale Lobster Boat and



Lobster Shack kits, starting September 1, 2023 and ending February 15, 2024. To get the discount use the code: NEWTRX. [For more information about these and other products please visit their website.](#)

## AXM Paper Models

This next Build Along will provide something new for your model railroad as well as teaching you some new modeling skills and techniques using card. Starting January 24, 2024, Alfonso X Moreno, owner of the [AXM Paper Space Scale Models website](https://axmpaperspacescalemodels.com/) and self-taught designer of paper models in the Space Category, will start the Build Along of his SpaceX Dragon in 1:48 scale. You can download the paper model of the SpaceX Dragon in 1:48 from his website: <https://axmpaperspacescalemodels.com/>



The photo shown is from the redesigned Dragon 2 model. The model used for this presentation is my prototype.

Alfonso said: “I design accurate scale replicas of real rockets and spacecrafts made out of paper, covering the Space Shuttle era and current rockets from around the world.

I am from Guayaquil, Ecuador, but lived in New York during my childhood. I still remember watching the Apollo missions in the early 70s, a time which influenced my passion for card modeling.

After all these years, my site has grown into a site that contains designs in scales of 1:144, 1:100 and 1:96. For the most part, the collection consists of Space Shuttle stacks in 1:144 scale for ALL the Shuttle missions. You can now download a Shuttle model for each mission! But my site not only covers Space Shuttle missions but other rocket models.”

Alfonso may be able to provide his card models in S Scale and O Scale if there is sufficient demand for these scales. If you are interested in models in these scales, please contact him directly at: [Alfonso.Moreno@newtracksm modeling.com](mailto:Alfonso.Moreno@newtracksm modeling.com).

“You can also download rockets representing other countries, Commercial rockets from USA and Space Stations including the International Space Station model. You can find models of the Atlas V, Delta IV, Antares, Falcon 9, Ariane 5, Vega, Soyuz and Progress vessels, India’s rockets and SpaceX Starships! It is my intention to also design the American rockets from the Early space age, like the Mercury, Atlas and Saturn V.

I have been very fortunate that my site and paper models have been widely accepted by the card modeling community. Through all these years, I have had many opportunities with Institutions to work in projects involving my paper models. I created the ISS children’s version for the Australian CSIRO’s kids magazine, “Pegaso” and “Krysaor” Cubesats for the Ecuadorian Space Agency (EXA), the SCanTestbed model for NASA’s Glenn Research Center, the Magnetospheric Multiscale (MMS) models for Goddard Space Flight Center, and many more.

In January 2020, which I consider the pinnacle of my paper model website achievements, I was invited to be part of the Boca Chica to Mars Event, which was held at the Brownsville Museum of Fine Arts in Texas, where my SpaceX paper models were exhibited for the public.

Follow me through my Blog and Twitter to get updates of my projects.

Although a few of my creations are not for free download, I try to give most of my models to the card modeling community for free; but in order to cover the expenses of paper, printer ink and other supplies for my designs and the time to design these models, a donation is appreciated.”

Please visit his website: <https://axmpaperspacescalemodels.com/>

## Brennan’s Model Railroading

Dennis Brennan’s company also sells ballast to his customers who say it is the best ballast they have every seen for their O scale/gauge layouts. Starting January 31, 2024, Dennis will begin a Build Along of ballasting track using his ballast. To work on ballasting your track along with Dennis, you can order his ballast and get a 10% discount up to two weeks before the start of the Build Along by using the code word NEWTRACKS when ordering. <https://www.brennansmodelrr.com/>



## Ipswich Hobbies

Jack Dziadul owner of Ipswich Hobbies will build his Section House kit starting April 10, 2024 in a Build Along. The kit is available in N, HO, and O Scale for New Tracks Modelers.



Special Pricing for New Tracks Modeling Viewers:

1. Kit #6 N scale \$15 less 20% discount = \$12
2. Kit #5 HO scale \$25 less 20% discount = \$20
3. Kit #18 O scale \$45 less 20% discount = \$36
4. Shipping \$8.95 – Local pick-up option

Ordering the kit

1. Discount code – NEWTRACK (good for one kit)
2. Discount start date March 6, 2024
3. Discount end date April 15, 2024

Please visit the Ipswich Hobbies for more information about the company and its products: <https://ipswichhobbies.com/>

More Build Alongs to come in 2024...

I hope you want to participate in all the Build Alongs. The modelers and manufacturers, who are making these events possible, do them to try to help you improve your skills and have more enjoyment and confidence in your modeling. They provide a true learning experience that have helped many modelers. So if you have been sitting on the sidelines for awhile, give railroad modeling a try with the help of a mentor.

I am looking for more modelers and manufacturers to be involved in future Build Along segments in 2024. Contact me if you are interested at: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).

## New Tracks Modeling Watch Me Build Railroad Models

These segments provide modelers the opportunity to build a model to demonstrate their modeling skills and techniques. Viewers can ask questions and learn from experienced modelers various facets of our hobby. Upcoming 2023 and 2024 Show Segments:

December 6, 2023 Jeff Jordan will kit-bash a craftsman kit to represent a local Prototype structure.

December 27, 2023 Bob Geldmacher will discuss AI and its potential for model railroading. He will present specific examples for us to consider.

January 11, 2024 Keith Sampler will demonstrate how to use vacuum forming in model railroading. I have never tried this, have any of you?

These segments present modelers who want to share their modeling efforts, and knowledge. They discuss their modeling techniques so others can benefit. These segments can be for one or more shows depending on the details included for the model building presentation.

### Weekly Wednesday Show Segments

#### **Find your Mentor at Local CLUBS and NMRA DIVISIONS: A Bi-Weekly Segment about Local Organizations**

Supported by the NMRA

This Bi-weekly Segment introduces local model railroading organizations where many of us have found mentors, modeling help, and met some really outstanding modelers. I credit these local organizations with providing much of my modeling education and believe such organizations can also help you.



Phil Edholm, a NMRA Division Superintendent of the NMRA PCR/Coast Division, which includes the San Francisco Bay Area down to Monterey in California, hosts this segment.

Every other week, these segments feature an NMRA Division or Local Club leader to discuss what activities they do, how they engage with members, how they recruit new members, and other topics. The focus is a discussion around what they have done that is successful and unique. The goal is to have actionable ideas come out to be used by other organization in their local activities.



Each time we try and identify a few key nuggets that all of us in model railroading leadership positions can use. Each segment is live streamed to our YouTube channel New Tracks Modeling. You can see all of the previous interviews on there.

If you, or someone you know, is actively involved in an NMRA Division or a Local Club and you would like to discuss how your organization is thriving (or surviving) in the new world, please contact Phil Edholm at [coastsup23@gmail.com](mailto:coastsup23@gmail.com) or call 408-832-5618.

#### **Let's Go To Our Local Modeling Hobby Store**

A monthly segment with hobby store owners supported by the National Retail Hobby Stores Association (NRHSA).

“Local hobby shops need modelers and modelers need local hobby shops”. This segment shows just how true this statement is.



Mark Poggendorf, a NMRA member working on his MMR, a hobby manufacturer, and owner of Poggies Trains Hobby Shop, and a member of the National Retail Hobby Stores Association (NRHSA), started a monthly show segment concerning common issues facing hobby shops and modelers. The focus of these segments is on how both groups can work together for their mutual benefit and hopefully introduce the value of your local hobby store to help you become a more knowledgeable and more skilled model builder.

Mark’s involvement in our hobby as a modeler, manufacturer and, hobby shop owner, who not only has a brick and mortar store in California but also attends, as a vendor, many of the train shows throughout the country, makes him well suited to lead these discussions and provide insights into the problems and issues facing both the hobby shop industry and modelers. Who knows, you also may find a new source for your modeling products from this series.

If viewers have a hobby shop to recommend be on our show, or you are a hobby shop owner who is interested in being on our show, please let Mark know at [Mark.Poggendorf@newtracksmodeling.com](mailto:Mark.Poggendorf@newtracksmodeling.com).

It’s that time again for me to return to my workbench and start working on something that I fell in love with and just have to model. It happens all the time.

Believe it or not, I am still working on the 1938 Buick. Based on how many mistakes I am making, it has become a major learning experience. But it is starting to look pretty good, just not finished yet. I recently found this prototype photo someone was riding on and had his wife take the photo. Yep I added it to my do do list. Just need some more information about it.



While I am modeling, you can help New Tracks Modeling by:

1. Volunteering to join our team and help us produce and develop our New Tracks Modeling shows. Email me:

[jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com)

2. Making a contribution to our Patreon account New Tracks Modeling to help pay for our out of pocket cost to run our shows. [Click here to donate](#).

3. Subscribing for free to our [YouTube Channel, New Tracks Modeling](#), and ringing the bell to get advance notices of our YouTube shows. Please watch the advertisements so we can earn a little revenue to produce our shows.

4. Subscribing for free to our website: [NewTracksModeling.com](http://NewTracksModeling.com) which provides login links to our Wednesday Zoom events and also provides information about what upcoming New Tracks Modeling you can travel with us.

5. Donating to our New Tracks Modeling Mentoring Scholarship program. Details for individual and corporate donations are on our website. Check out our Anonymous Donor’s Challenge Grant for the first \$1,000.00 in contributions under \$80.00.

To donate using our Zeffy account, just click [HERE](#).

6. Spreading the word to high school graduates and college students about applying for our New Tracks Modeling Mentoring Scholarship program. Get the word out. Details and an Application are on our website: [newtracksmodeling.com/scholarship](http://newtracksmodeling.com/scholarship).

7. Subscribing for free to *The O Scale Resource* and *The S Scale Resource* online magazines so you don't miss any of my New Tracks Modeling articles and also see some great modeling by various modelers who may become one of your mentors. [Click here to sign up](#).

8. Write to me! I love getting your comments, suggestions, and modeling ideas. I so enjoy hearing from you and having a conversation. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com)

Thank you again for all your help and for reading this far. Till next time with more New Tracks Modeling, Happy Railroad Modeling with whatever you are building!

Wishing you a wonderful holiday season full of fun, family, friends, lots of love and happiness, and of course great modeling!



**Love Modeling?  
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**NEW TRACKS**  
with host Jim Kellow, MMR  
"Where Mentors help Modelers Build"

**NEW TRACKS** is an exciting, **NEW, FREE**, communication concept. Our shows are live and interactive on Youtube every Wednesday at 7PM ET.

**Find a mentor** - Meet and talk with talented modelers and manufacturers from around the world.  
**Get discounts** - Buy kits used in our BUILD ALONG segments.  
**Explore a wide variety of modeling topics** including new technologies. Scratchbuilding. Kit Building. Kit bashing.  
**Share** your own modeling in our MY BUILD sessions.

**Give us a try!**  
Subscribe to **NEW TRACKS** at our website: [newtracksmodeling.com](http://newtracksmodeling.com) for details and Zoom links or subscribe on our YouTube channel: [New Tracks Modeling](#) for show information.

**NEW TRACKS**

The graphic features a green background with various modeling tools like a drill, pencil, and ruler. A laptop in the center displays the YouTube logo. At the bottom, there is a small image of a train model.



**THE S SCALE RESOURCE**  
NEWS, REVIEWS, INFORMATION TO USE  
ONLINE MAGAZINE

The logo features a large, stylized yellow 'S' with a blue outline. To its right, the words 'THE S SCALE RESOURCE' are written in blue, with 'RESOURCE' in a larger font. Below this, 'NEWS, REVIEWS, INFORMATION TO USE' is written in a smaller blue font, followed by 'ONLINE MAGAZINE' in a medium blue font.

Don't forget that we also publish *The S Scale Resource Magazine* on opposite months from *The O Scale Resource Magazine*. More articles that you may find useful along with a different *New Tracks* column.  
**Read free today!**



# NEW TRACKS MODELING

## “MY BUILD” Models Shown on the September 2023 Show

These are some of the photos modelers shared on our recent September 20, 2023 MY BUILD on our weekly Zoom Show. [You can see a video of the entire MY BUILD segment here.](#) Each of the participants has an email address included, and welcomes your contact.



*Ed Cady - jaycerail@gmail.com - HO6 scale (6 foot broad gauge)- Work continues on his bridge scene. Hunterline covered bridge.*



*D.S. Yates - dsyates215@gmail.com - N scale - scratchbuilds.*



*Martin Brechbiel - martinwb@oscalemag.com - O scale - Blacksmith car. Five years to build. Cedar stripwood structure.*

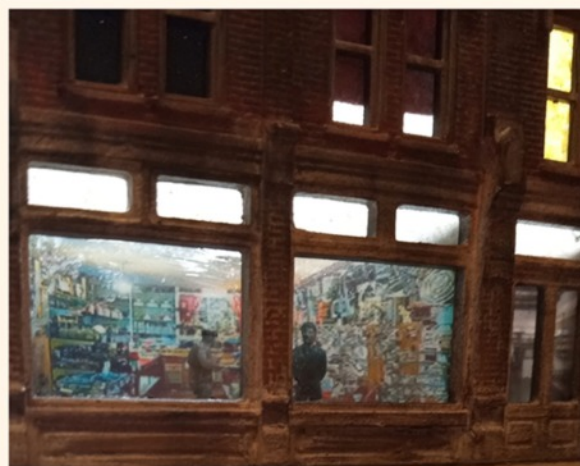




Left: Jeff Jordan - [jordan.jordan54@verizon.net](mailto:jordan.jordan54@verizon.net) - O scale - Vance Junction work continues. NOS Crystal River Coal Tower. Designed from looking at the real structure.



Below: Mitch Lovelace [lovelacemitch@gmail.com](mailto:lovelacemitch@gmail.com) - N scale - Work continues on his city scene.



Ed O'Rourke - edwardorourke1@me.com - HO scale - This is the Batchelder and Sons Furniture factory I built for my friend Jim Heidt's Ogdensburg and Norwood Railroad. It's HO scale. It had to fit between the track and backdrop, so the left end is shortened and the back is not modeled. Don Andrews drew the plan over a decade ago. Built from Northeastern Clapboard and strip wood. The metal roofing is from a company called Paper Creek. They made great stuff, but anyone with a copier could reproduce their stuff, so they didn't stick around long. The windows and doors are from Tichy.

Unfortunately, Don passed away many years ago and never saw it built, and Jim has now passed on as well, so it never really got set into the layout in a finished way. I'm going to see if I can find a place for it on the new layout I am building.



Travis Summit - [travis.summit@gmail.com](mailto:travis.summit@gmail.com) - HO scale - This was a diorama made from a new kit that was released earlier this year by Keep it Rusty, Oddmunn Brothers Mill.



James Taverna - jtaverna1@cox.net - O scale - Pictures of a project that I undertook to correct the lettering on a set of PRR F7s from Key. The lettering was out of scale Champ and I had correct lettering from Clover House. I sanded off the lettering below the belt rail of the engines. The photo of that also shows the sanding sticks I used, which came from a manicure supply vendor. I then repainted the shells with matching Scalecoat paint. Next was adding the correct lettering and striping, plus placing additional lettering to make it complete. I also added 8 wheel pick up to the engines. Last, I show the units pulling a train on my layout.

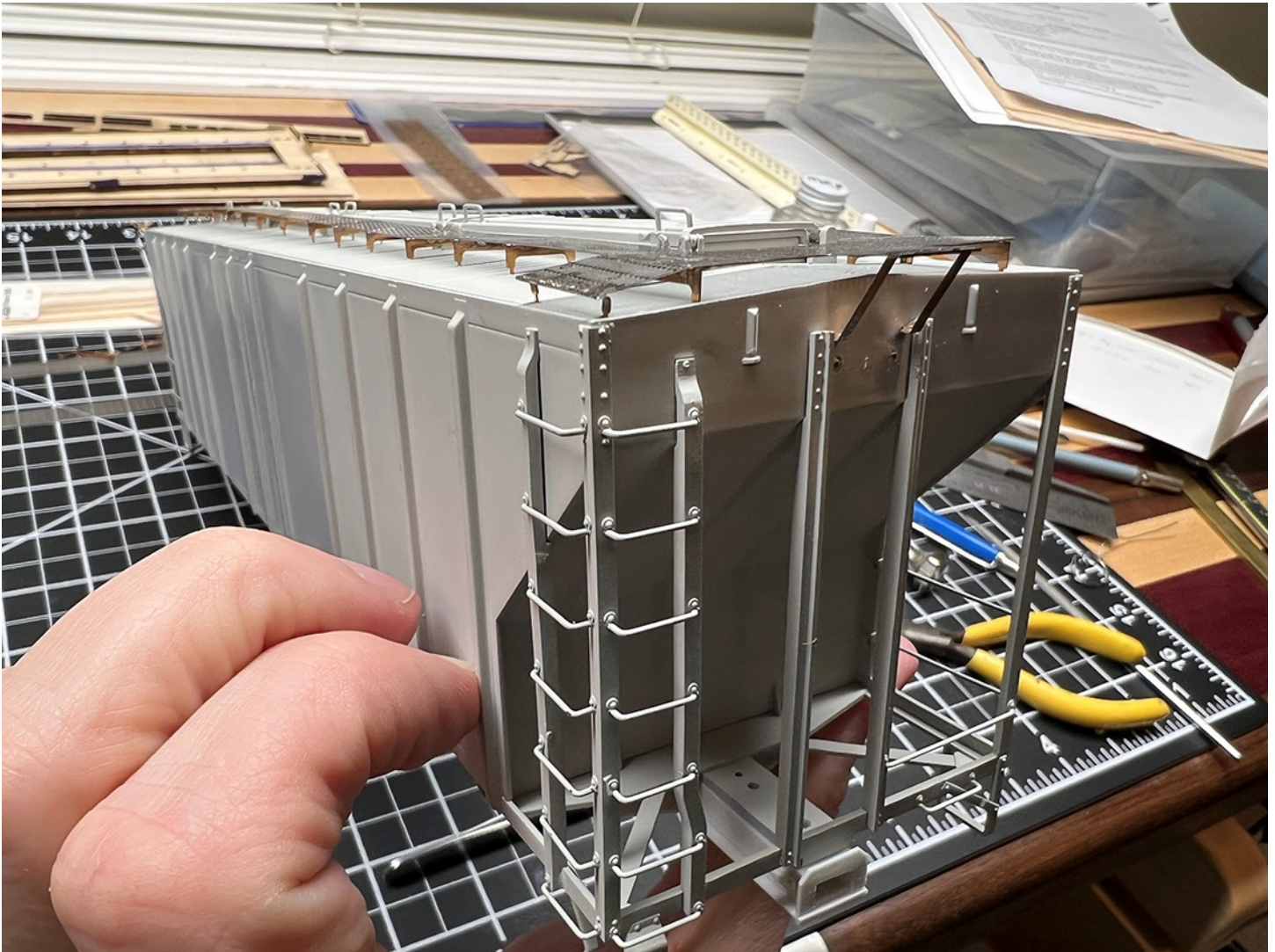


# BACKSHOP SOLUTIONS

By Ross Dando

Have a modeling question for our experts? Please send your description of your modeling problem to [backshopsolutions@oscaleresource.com](mailto:backshopsolutions@oscaleresource.com).

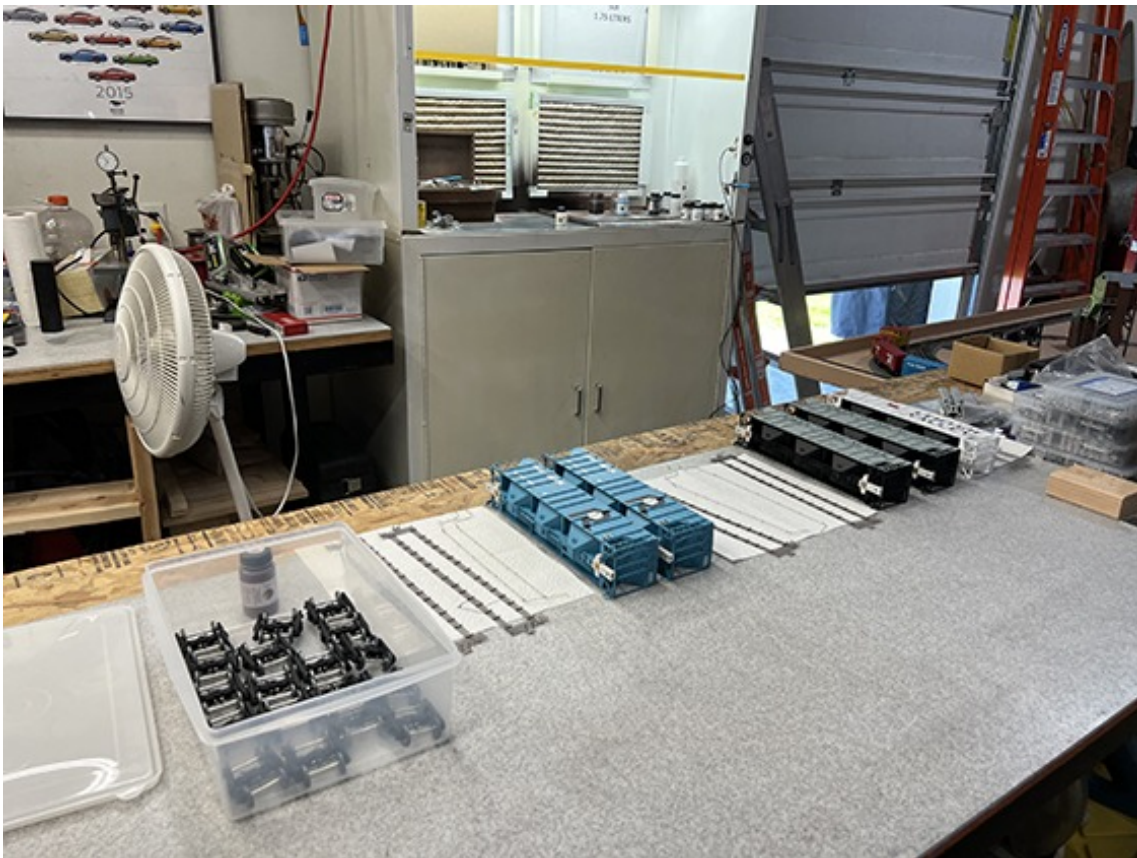
## Where's Rosco Been?



It has been several issues since my last column.

Back Shop Solutions is for sharing knowledge and answering questions. I don't get many questions, and the amount of knowledge I have has been questioned. So here it goes.

Like the squirrel I am, I have been modeling, just not on one project. But I tried to change that and focus on one that has been on the bench for some time. The conversion of an Atlas PS4427 covered hopper to Proto48 as well as adding some details. For a refresher, the conversion is replacing the bolster and coupler box. I made a master and Jon Cagle cast them in resin. The hold up has been my inability to just convert one. They seemed to multiply every time I turned around. When the pile reached ten it was enough.

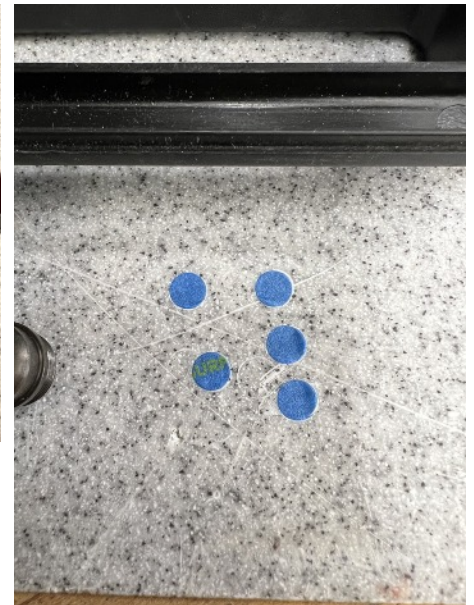


I was now doing one step ten times before going to the next one. The wall I hit was the etched running boards. They don't just put themselves together. I sucked it up and worked through the stack, and it was finally time to touch up paint and fully paint several cars.

First, all the running boards needed media blasting to help the paint adhere.

### Tip number one.

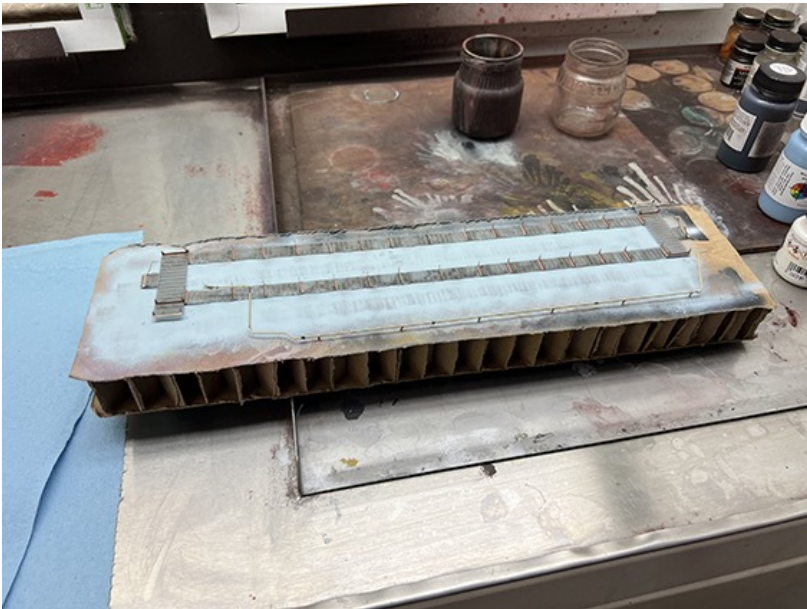
My OCD when it comes to masking is having the tape cover the area. When that is a circle, it is a bit more challenging. I have aged, but not grown up. Yes, scissors still work and I learned in kindergarten how to cut shapes. I try to find the easy button to do things. In this case, a small piece of tape to cover the bolster pad. I have a set of punches for putting holes in gaskets. I pulled out the 1/4" one, punched a piece, and it fit.



### Tip number two.

When painting objects that you can't just hold in a tweezers or hemostat, there are options. Mine is a block of cardboard that came in some shipment and I have found it helpful for testing the spray from the airbrush as well as placing a part that needs to be sprayed. If it works, keep doing it.



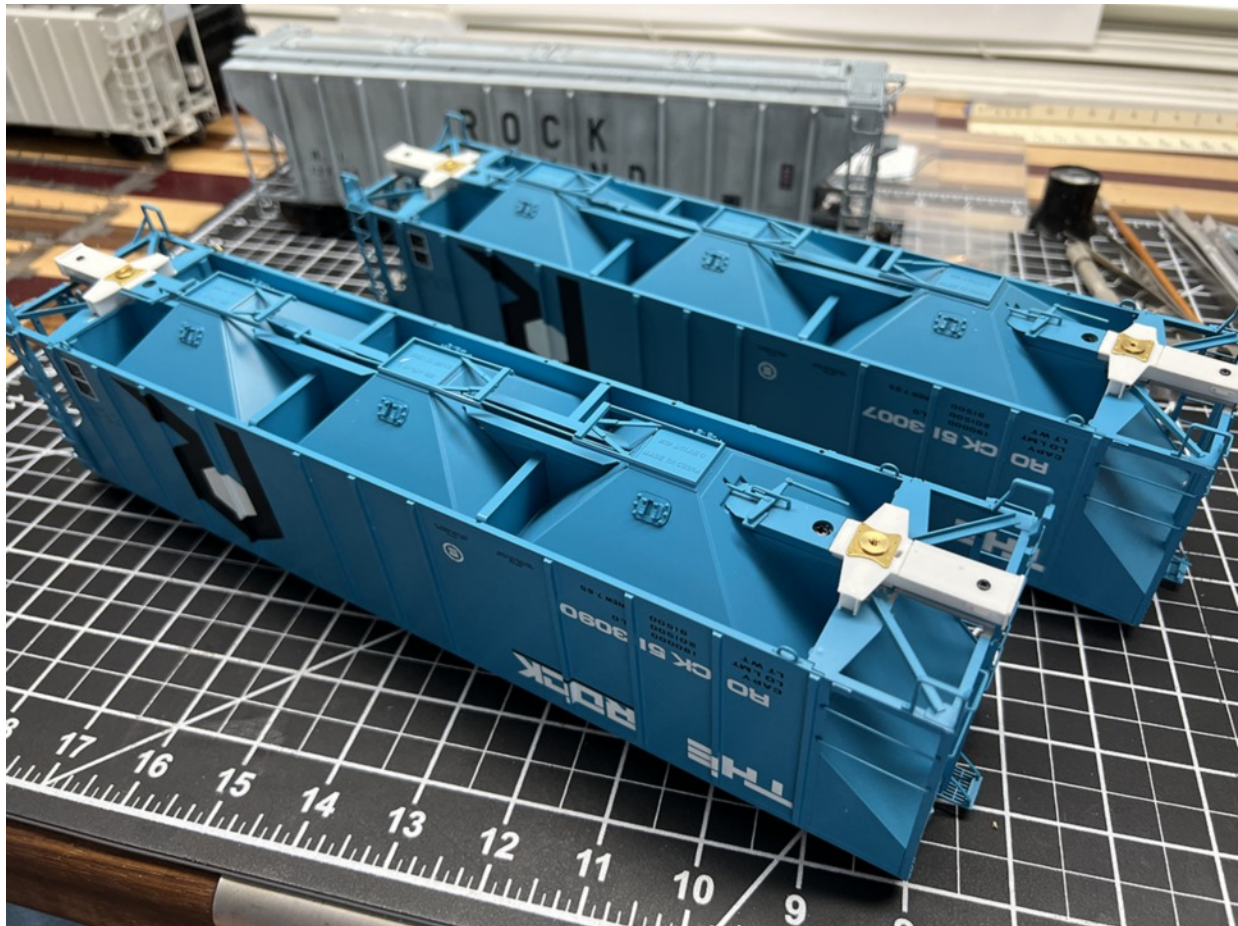


Time to paint. It was an assembly line of loading the airbrush and cleaning the airbrush, rinse and repeat. Bill Yancey came over and kept me company and we had a great afternoon. Went lightest color to darkest just in case I had not cleaned everything completely.



It feels good to get all these cars past this step. Now to get the sub assemblies put together, the painted cars decaled and the other cars sealed for weathering.

Until next time, get to the bench and build something.



# So, What Are You All Up To?

Please Email us your pictures and captions to [photos@modelrailroadresource.com](mailto:photos@modelrailroadresource.com). And remember, a little bragging never hurt anyone!

[Andreas Lüneburg](#) from Germany sent us the following.

*“Last year I was working on two projects, one is my home layout and the other one is a modular arrangement for our 0-Scale Division within our model railroad club FREMO ([www.fremo-net.eu](http://www.fremo-net.eu)).*

*I took up modelling US railroads fairly recently. I got interested in it when a railway fan showed me [this video on YouTube](#). (Switching Utica Club Brewery). I was hooked.*

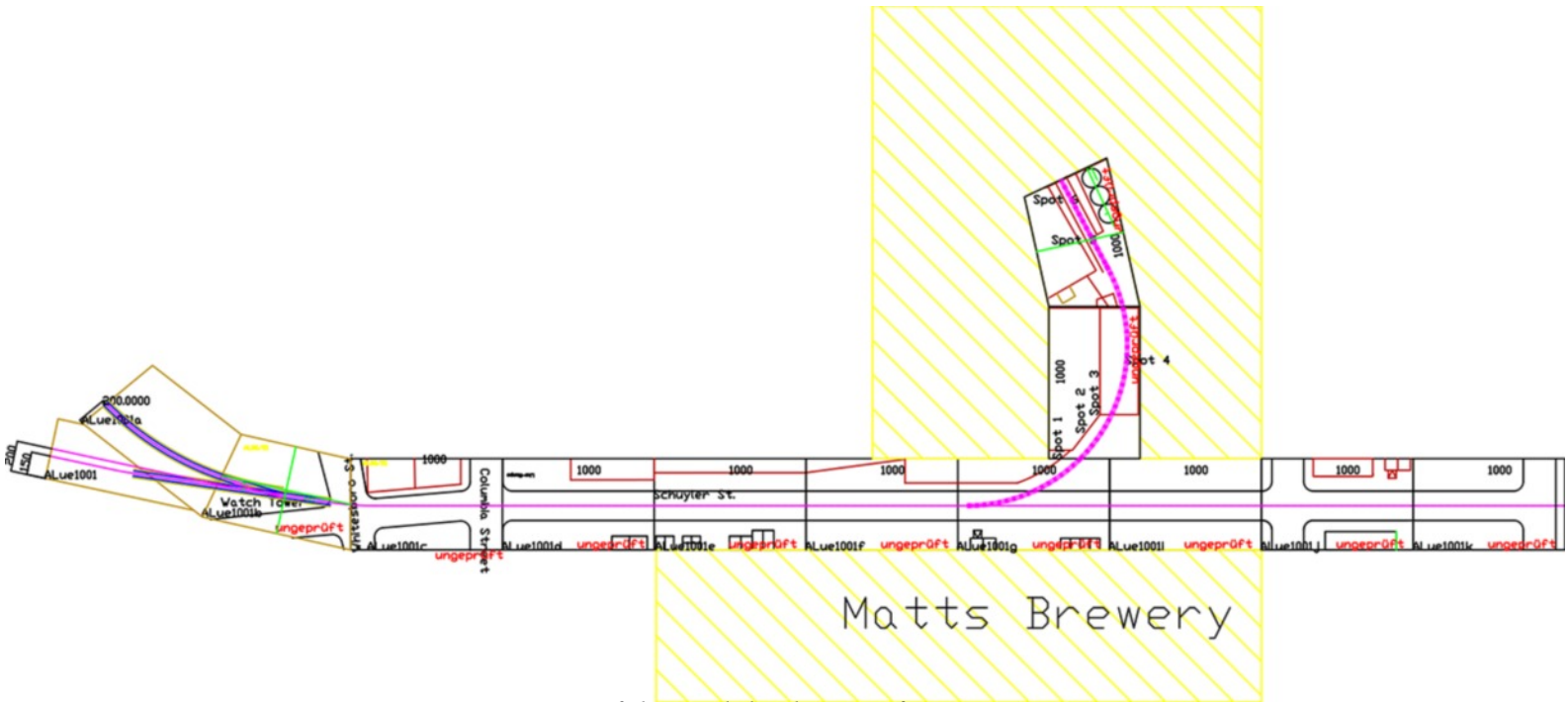
*In the meantime, I built 10 m of module depicting Matts Brewery and a couple of yards before and after it showing Schuyler Street. With artist’s license, I put back the turnout into working conditions so we can use it for our layouts to allow for some more interesting traffic. I was lucky to find a model of NYS&W 3040 (former MTH).”*

I have seen this video and a few others taken at the same location. It’s a must see for anyone who thinks you need a large area for O scale switching. Lets look at Andreas take on this.









*Drawing of the modular layout of Matts Brewery.*



And this beauty on the left from [Carl Rowe](#):

*“A night time picture of the repair and welding shop on my build of O’Neills Fabrication. Here’s the interior of the shop.”*

# O SCALE SHOWS & MEETS

Have an upcoming O Scale event? We would like to help publicize it. Send us the information up to one year in advance, and we'll place it here along with a direct link to your Website and/or Email. [Click here to send us your information.](#)

## **2023 Cleveland 2rail O Scale Meet**

**Nov 4, 2023 9AM -2PM**

6' tables \$47/\$50 after Oct 1st. Admission to all \$7.

Friday load in 1PM to 5PM

Contact Sam Shumaker, 440-248-3055 or Email at [j3a5436@gmail.com](mailto:j3a5436@gmail.com)

## **2023 Danville Indiana Train Show O/S**

**November 18, 2023 10AM - 3PM**

This will be the 16th year that volunteers from the CID have put on this outstanding multi-scalegauge Train Show. The 2023 Show will highlight O-S Scalars in an exclusive 2,000 square foot room for vendors, layouts, and socializing. Last year we had 14 vendors with 32 sales tables dedicated to O-S Scale Trains. Make your plans early to attend. We expect to sell out the O-S Room. [Please download the O & S Vendor Invitation here](#) to secure your sales table.

NOTE: Please register and make payments to the CID contacts on the Vendor Flyer. S Scale Contact: Charles Malinowski, [bmry1905@gmail.com](mailto:bmry1905@gmail.com), 317-730-3187 O Scale Contact: Eric Peterson, [ericgpet@gmail.com](mailto:ericgpet@gmail.com), 904-607-7035

## **O Scale South 2024 Train Show**

**Saturday, February 24, 2024 9am - 2pm**

Cross of Life Lutheran Church, 1000 Hembree Road, Roswell, Georgia.

Admission: \$6.00 (your spouse and children free).

Tables \$30.00 each (includes admission).

On3, On30, On2, Proto:48, and 3-rail scale welcome.

Modular display and Layout tours available.

Contact [Dan Mason @daniel@masonlaw.us](mailto:Dan.Mason@daniel@masonlaw.us) or 770-337-5139 to reserve tables and information.

## **O Scale March Meet**

**March 14-17, 2024**

Westin Lombard Yorktown Center

Lombard, IL

The March O Scale Meet is a 3 day gathering of vendors, customers, clinics, and fun held annually in March in the Chicagoland area. This is the Chicago O Scale train show you've heard of.

Website: <http://marchmeet.net/>

Email: [ChicagoMeet@yahoo.com](mailto:ChicagoMeet@yahoo.com)

## **Harrisburg All O Scale Meet**

**April 6th, 2024 9AM-3PM**

Sponsored by: [Narrow Gauge Modeling Company](#)

New Hope Church

584 Colonial Club Drive, Harrisburg, Pa. 17112

[See the Facebook page for more information.](#)

## **Strasburg 2 Rail Train Show**

**April 13th, 2024**

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

## **Harrisburg Narrow O Summer Meet**

**June 7-8, 2024**

Friday June 7, 11am to 6pm

Saturday June 8, 9am to 3pm

Sponsored by: [Narrow Gauge Modeling Company](#)

New Hope Church

584 Colonial Club Drive, Harrisburg, Pa. 17112

[See the Facebook page for more information.](#)

## **Strasburg 2 Rail Train Show**

**August 10th, 2024**

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

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## SHOWS & MEETS

**March O Scale Meet**  
**March 14-17, 2024**  
<https://marchmeet.net/WP/>  
**Ph. 414-322-8043**

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## PROTO 48

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**Web: [www.protocraft48.com](http://www.protocraft48.com)**

## Advertisers Index

All Aboard Trains	Pg. 17
All Nation Lines	Pg. 3
Allegheny Scale Models	Pg. 16
Along the Railroad by Howard Serig	Pg. 32
American Scale Models	Pg. 9
B.T.S.	Pg. 28
Berkshire Car Shop	Pg. 17
Berkshire Valley Models	Pg. 16
Cleveland Two Rail O Scale Meet	Pg. 8
Clover House	Pg. 8
Delta Models	Pg. 17
Des Plaines Hobbies	Pg. 17
Excelle Lubricants	Pg. 57
Get Real Productions	Pg. 57
March Meet Chicago	Pg. 4
Millhouse River	Pg. 16

Model Railroad Resource 3D	Pg. 27
Model Railroad Resource 3D	Pg. 28
New Tracks	Pg. 95
O Scale Turnouts	Pg. 16
Protocraft	Pg. 17
RailFonts.com	Pg. 57
Right-O-Way	Pg. 16
SoundTraxx	Pg. 8
Stevenson Preservation Lines	Pg. 17
Streamlined Backshop	Pg. 28
The Public Delivery Track	Pg. 16
The S Scale Resource Magazine	Pg. 95
Trainz	Pg. 9
Tru-Color Paint	Pg. 9
Wasatch Model Company	Pg. 16