



**O**  
*THE*  
**SCALE**

# RESOURCE

**NEWS, REVIEWS, INFORMATION TO USE**

**We're putting the "modeling" back  
in Model Railroading! ©**

**Volume 13 No. 4**

**March/April 2026**

**A Special Little Joe  
The Kettle Valley Railway 1952  
Building An Open Air Engine Shed Part 1  
Building The Beast Southern Railway #5046  
Painting and Finishing AI Created Cab Figures  
C&NW System Standard 44-foot Water Tank circa 1948 -  
Part 1  
And so much more...**

20,000 sq ft Ballroom: 350+ Tables

# MARCH-O-SCALE-MEET

## MARCH 19-22, 2026



### BUY SELL TRADE!

Clinics, Discussion Groups,  
Contest & Layout Tours



For more information, go to:  
**www.marchmeet.net**

# Westin Lombard Yorktown Center

70 Yorktown Center  
Lombard IL, 60148

Call 1-888-627-9031  
Ask for March-O-Scale Meet  
Rate \$133/night\*

\*Wednesday thru Sunday Night

#### THURSDAY

Fox Valley O Scalers  
Operating Session: 12 pm - 5 pm  
Layout Tours

#### FRIDAY

Dealer Setup: 1 pm - 9 pm  
Layout Tours  
Evening Discussion Meetings

#### SATURDAY

Dealer Setup: 8 am - 9 am  
★ Show: 9 am - 5 pm ★  
Model Contest, Clinics  
Layout Tours

#### SUNDAY

★ Show: 8:30 am - 2 pm ★  
Layout Tours

✂ PLEASE PRINT LEGIBLY

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Name: \_\_\_\_\_  
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CITY/STATE/ZIP

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Make checks payable to: Pioneer Limited LLC  
Mail registration form to: P.O. Box 643  
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Email: Chicagomeet@yahoo.com

Registration (1 or 2 days) \$30.00 ..... \$ 30.00  
*(Table holders also need to pay the \$30.00 registration fee)*

# of 6 ft. tables \_\_\_ @ \$60 ea/ \$70 after 3/1/2026 = \$ \_\_\_\_\_

Number of add'l. registrants \_\_\_ @ \$30. each = \$ \_\_\_\_\_  
*(Please list below—use back if necessary; children 15 and under free)*

Name: \_\_\_\_\_

Name: \_\_\_\_\_

Electrical outlet needed for table(s) \$45.00 \$ \_\_\_\_\_

TOTAL AMOUNT ENCLOSED ..... \$ \_\_\_\_\_  
*(No refunds after 3/1/26)*

Spouse's Name (no charge — *Only needed if attending the show*) \_\_\_\_\_

**\*\*No trading Friday Night.**  
**Access to hall for setup purposes only.**



Questions? Visit **MARCHMEET.NET**, call 414-629-0030 or Facebook: <https://www.facebook.com/MarchOScaleMeet/>

The parties whose names appear above, have agreed to hold harmless all of the organizers, the sponsors, Pioneer Limited LLC, the Westin Lombard Yorktown Center, and others, singly and collectively, for any injury, harm, loss, damage, misadventure, or inconvenience suffered or sustained as a result of participating in this March O Scale Meet 2026, or in connection with any activity related to this event, whether of negligence by agents under their employ or otherwise.

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## O SCALE LOCOMOTIVES & ROLLING STOCK

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### Steam Locomotives

ATSF 4-6-4 Hudson, PRB, Late version, New, FP, Can Motor, Lights, Road No. 3458, Korea .....	\$795
ATSF 4-8-2 Mountain, SS 3rd, New, Early version, FP, Can Motor, Road No. 3733, Korea .....	\$1095
ATSF 2900 Class 4-8-4, USH, L/N, Pro Paint, Lights, Road No. 2900, KTM Japan .....	\$795
ATSF 2-10-4, USH, L/N, CP, Pro Weathered, KES Drive, Can Motor, Lights, No. 5028, Japan .....	\$1195
B&O 2-8-0, SS 3rd, L/N, FP, Can Motor, Lights, Road No. 2381 .....	\$595
B&O P7 4-6-2 Early, Custom/Ken Henry, EX+, CP, Black, Can Motor, Road No. 5317 .....	\$1595
B&O P7E 4-6-2 Pacific, Semi-Streamlined, SS 3rd, Mint, FP Blue, Spoked Drivers, No. 5315 .....	\$1195
B&O S1a 2-10-2, SS 3rd, L/N, FP, Vandy Tender, Can Motor, Lights, Road No. 6224 .....	\$1095
B&O S1a 2-10-2, OMI, EX+, FP, Upgraded Details, Road Pilot, LEDs, Road No. 6217 .....	\$1395
B&O T4a 4-8-2, SS 3rd, EX+, FP, Weathered, Upgraded, Can Motor, Lights, No 5560 .....	\$1095
B&A Class K6a 4-6-2, SS/Custom, L/N, CP, Upgraded, KES Drive, Lights, Road No. 590 .....	\$1595
C&O J2 4-8-2, SS, EX, CP, "G. Washington", Can Motor, LEDs, Road No. 542, Korea, NOB .....	\$750
CB&Q 2-10-4, P48, Scratch Built/Tom Mix, New, UP, No. 6322, One of a kind Model .....	\$9195
CB&Q S4a 4-6-4, PSC No. 17159-2, New, FP, Can Motor, LED HL, Road No. 4003, Korea .....	\$2695
GN H4 4-6-2, ORI, New, UP, Baldwin Version, Can Motor, Samhonga .....	\$1395
GN O8 2-8-2 Vestibule Cab, ORI, New, FP Black, Lt. Weathering, No. 3377, Samhonga .....	\$1550
GN P2 4-8-2, PRB, L/N, FP, Glacier Park, Can Motor, Lights, Road No. 2510 .....	\$750
GN S2 4-8-4, SS, New, Pro Paint/B. Beedy, Glacier Park, Lights, No. 2588, Samhonga .....	\$1795
IHB 0-8-0, USH, Excellent, CP, KES Drive, Can Motor, Road No. 102 .....	\$1195
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MILW F6 4-6-4, PSC #17337-1, New, FP Blk, As Shopped, Lts, Mars Lt, Road No. 135, Korea .....	\$2750
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MILW F7 S/L Hiawatha 4-6-4, Weaver, New, FP, Can Motor, Lights, Road No. 100 .....	\$795
NYC CCC&St.L 4-6-2, Scratch Built, L/N, Pro Paint, Gray Boiler, Can Motor, No. 6467, Exquisite .....	\$2895
NYC CR&I B62 USRA 0-6-0, SS/Custom, New, Pro Paint/B. Beedy, No. 223, Unjoke .....	\$1495
NYC J3a 4-6-4 Late Destreamlined, Kohs, New, FP, Mixed Drivers, No. 5451, SJ Models .....	\$6595
NYC J3a 4-6-4 Destreamlined, SS 3rd, L/N, FP, Marcus Drive, Disks, PT-4, No. 5451 .....	\$1095
NYC J3a Dreyfuss, Lionel Smithsonian, L/N, FP, Wood Case, Road No. 5454, SJ Models Korea .....	\$2050
NYC K5 4-6-2, SS/Custom, New, Pro Paint, Road No. 5000 .....	\$1250
NYC L3b 4-8-2, SS 3rd, Like New, FP, Full Cab Detail, Can Motor, Lights, Road No. 3035 .....	\$1095
NYC S1b 4-8-4 Niagara, PSC Crown, L/N, UP, Twin HL, Pittman R1 Motor, LEDs, KTM, Japan .....	\$1795
NYC S1b 4-8-4 Niagara, SS, L/N, FP, Modern, Lights, Kadee, Coal Load, No. 6024 .....	\$750
NYC U3a 0-8-0, USH/KES, L/N, CP, KES Drive, Custom Clear Vision Tender, Road No. 7820 .....	\$1195
NKP L1b 4-6-4, Weaver/Custom, L/N, CP, "Buy War Bonds" Scheme, Can Motor, No. 176 .....	\$895
NKP L1b 4-6-4, Weaver, L/N, FP, Smoke Lifters, Can Motor, Lights, Road No. 175, Korea .....	\$750
NKP S2 2-8-4, USH/Custom, L/N, CP, Detailed, KES Drive, Can Motor, Lights, No. 763 .....	\$1195
N&W Class J 4-8-4, SS, EX, CP, Can Motor, Tuned Drive, Lights, Road No. 608 .....	\$725
NP A4 4-8-4, OMI O151.2, L/N, CP, Gray Boiler, Lights, Road No. 2677, Ajin, Korea .....	\$1695
PRR B6sb 0-6-0, PSC, New, FP, Standard Version, Can Motor, Lights, Road No. 1670 .....	\$1250
PRR H8sc 2-8-0, Key, New/TR, UP, Standard Front End, Can Motor, Samhonga, Korea .....	\$1595
PRR H9s 2-8-0, Key, New, FP, Postwar Details, Lights, Road No. 1132, Samhonga .....	\$1625
PRR H10 2-8-0, Key, New, FP, Std. front end, Stoker, Window Glazing, Lights, No. 8014 .....	\$1650
PRR I1sa 2-10-0, USH, New, Pro Paint, DCC/Sound, Postwar Detail, Upgraded Drive, No. 4658 .....	\$1295
PRR J1 2-10-4, WSM, EX+, CP, Marcus/KES Drive, Upgraded, Can Motor, LEDs, Road No. 6453 .....	\$1595
PRR K4 4-6-2 Postwar, Kohs, New, FP, 130p75 Tender, Road No. 3863, SJ Models, Korea .....	\$5495
PRR K4 4-6-2 Prewar, Kohs, New, FP, 130p75 Tender, Road No. 5339, SJ Models, Korea .....	\$5395
PRR K4 4-6-2 Pacific, Kohs, L/N, FP, As Built, 110p70 Tender, Road No. 5431, SJ Models .....	\$5295
PRR K4sa 4-6-2, Kohs, New, FP, 130p75 Tender, 1 of 10, Road No. 612, SJ Models, Korea .....	\$6495
PRR M1 4-8-2, WSM, L/N, CP, USH 210p75 Long Tender, Lights, No. 6813, KTM Japan .....	\$750
PRR M1a 4-8-2 Prewar, OMI, New, UP, 210p75 Tender, Can Motor, 1996 Run, Ajin, Korea .....	\$1895
SP Lines C-10 2-8-0, PFM, New, FP, Prewar Lettering, Can Motor, Lights, No. 2852, Samhonga .....	\$2095
SP F-3 2-10-2, PSC, Post War, L/N, FP, Can Motor, Lights, Road No. 3666, Korea .....	\$1995
SP GS-4 4-8-4, PSC, New, FP, San Joaquin Partial Daylight, De-Skirted, Road No. 4439 .....	\$3250
SP GS-4 4-8-4, SS 3rd, L/N, FP, Daylight, Postwar, Can Motor, Lights, Road No. 4450 .....	\$1295
SP MT-4 4-8-2, SS 3rd, New, FP, Postwar, Can Motor, Lights, Road No. 4360, Korea .....	\$1195
SP MT-5 4-8-2, PSC, Postwar, L/N, FP, Can Motor, Lights, Road No. 4375 .....	\$1995
UP 5000 Class 4-12-2, SS 3rd, 1st Run, New, FP, Can Motor, Lights, Road No. 9000 .....	\$895
UP FEF-2 4-8-4, Oil Version, Key, L/N, FP, TT Gray, Silver Stripes, Can Motor, Lts., No. 825 .....	\$1595
WM 3 Truck Shay No. 6, WSM/M. Forsyth, L/N, CP, Fireball Scheme, QSI DCC/Sound, NOB .....	\$2395
WM I2 2-10-0, OMI, L/N, FP, Lagged smokebox, Pittman Motor, LEDs, Road No. 1125, Ajin .....	\$1695
WM J1 4-8-4 Potomac, C&LS, New, FP, Can Motor, Lights, Road No. 1412, Boo-Rim, Korea .....	\$2295
Westside Lumber Co. No. 3 Heisler, WSM, New, UP, Std. Gauge, Can Motor, Japan .....	\$1695

### Articulated and Duplex Steam Locomotives

B&O EM1 2-8-8-4, MG, L/N, CP, Tuned Drive, Can Motor, Road No. 7604 .....	\$1495
C&O H8 2-6-6-6, Key, L/N, FP, Late Version, DCC/Sound, Road No. 1645, Samhonga .....	\$3095
C&O H8 2-6-6-6, Kohs, L/N, FP, Late, Version 2, Road No. 1657, SJ Models, Korea .....	\$6995
C&O H8 2-6-6-6 Allegheny, USH, EX, CP, Late Version, Can Motor, LEDs, No. 1658, KTM .....	\$1495
GN Z6 4-6-6-4, SS 3rd, L/N, FP, Pro Weathered, Lights, Road No. 4001, Korea .....	\$1495
N&W Y3a 2-8-8-2, OMI, L/N, Pro Paint, C&LS Dual Drive, LEDs, No. 2061, Ajin, Korea .....	\$1895
N&W Y6a 2-8-8-2, Kohs, Ver 2, New, FP, 1 of 17, Road No. 2164, SJ Models .....	\$7495
N&W Y6b 2-8-8-2, Kohs, Ver 4, New, FP, Ser No. 25 of 100, Road No. 2190, SJ Models .....	\$6595
NP Z5 2-8-8-4, PSC, New, FP, Black, Early Version, Upgraded Detail & Drive, Lts, No. 5001 .....	\$3195
NP Z5 2-8-8-4, PSC #16977-1, New, FP, Black, Late Version, Road No. 5008, Korea .....	\$3195
NP Z8 4-6-6-4, SS 3rd, Anniversary Series, New, FP, Black Boiler, Lights, No. 5008 .....	\$1595
NP Z8 Challenger 4-6-6-4, OMI, L/N, CP, Black Boiler, Lights, Road No. 5130 .....	\$2650
PRR Q2 4-4-6-4, SS 3rd, New, FP, Can Motor, Lights, Road No. 6131, Korea .....	\$1250
PRR T1 4-4-4-4, SS 3rd, L/N, FP, Modified, Can Motor, Road No. 5511 .....	\$1395
SP AC-5 2-8-8-2 Cab Forward, PSC, New, UP, Flat Face, Upgraded, D&D Models .....	\$3195
SP S-2 8-4-6-6-4, OMI, New, CP, Black/Yellow Lettering, Oil Fired, Ajin, Korea .....	\$2650
UP Big Boy 4-8-8-4 Early, USH, L/N, CP, C&LS Gearboxes, Twin Cans, LEDs, No. 4006, KTM .....	\$1795
UP Challenger 4-6-6-4, PSC Crown, L/N, Pro Paint, Coal Fired, DCC/Sound, No. 3916, KTM .....	\$3295
UP Challenger 4-6-6-4, Key, New, FP, TT Gray/Yellow, Oil Version, No. 3978, Samhonga .....	\$2095
VGN AG 2-6-6-6, PSC No. 15809, EX+, UP, Can Motor, LEDs, D&D, Korea .....	\$1895

### Diesel and Electric Locomotives

ALCO S1 600 HP Diesel Switcher, CB, L/N, UP, Blunt Trucks, Open Frame Motor, KMT, Japan .....	\$725
ALCO S4 Diesel Switcher, CB, L/N, UP, AAR Trucks, Can Motor, LEDs, KMT, Japan .....	\$795
ATSF EMD E8 A-B-A Set, Key, L/N+, FP Warbonnet, LEDs, All Powered, No. 80, Samhonga .....	\$2995
ATSF EMD F3 PH I A-B-A Set, Atlas, New, FP, Warbonnet, 2 Units Pwd, DCC Sound .....	\$1395
ATSF EMD F7 A-B Set, Phase II, Key, L/N+, FP Warbonnet, DCC/Sound, No. 314, Samhonga .....	\$1850
B&O EMD F7 A-B Phase I, Key, L/N+, FP, Ser #78, DCC/Sound, Road 949, 949X .....	\$1925
B&O EMD GP9, Atlas, L/N, FP, Late Blue Scheme, Cat #2401-2, DC Analog, Road No. 681 .....	\$450
B&O EMD SD9 Diesel, SS 3rd, Mint, FP, DC/DCC Sound, Can Motor, Lights .....	\$750
B&O FM H-10-44, OMI, L/N, CP, Blue Scheme, Cab Detail, Can Motor, Flywheels, No. 304 .....	\$795
C&O EMD E8 A Unit, Weaver/CLW Drive, EX, CP Tri-Color, Twin Can Motors, LEDs, No. 4020 .....	\$595
CB&Q 44 Ton Diesel Switcher, Phase IIa, W&R, L/N, CP, Weathered, No. 9107, Samhonga .....	\$595
Conrail (ex PRR) ALCO C425 Phase II, OMI, New, UP, Can Motor, Nos. 2416-2446, Ajin .....	\$895
DL&W FM H-16-44 Road Switcher, Car Works, New, UP, Can Motor, C-Liner Trucks, Korea .....	\$795
DM&R EMD SD9, SS 3rd, New, FP, 1st Run, DC, Can Motor, Lights, No. 102 .....	\$795
EMD NW2 Phase IV Diesel Switcher, ORI, New, UP, 1000 HP Can Motor, Samhonga .....	\$750
Erie Lackawanna F7A-F7B, Atlas, New, FP, Road No. 7114(A)-Powered - No. 7123(B) .....	\$695
GN EMD F3 PH II A-A Units, Atlas, L/N, FP, Lt. Weathering, DC/DCC/Sound, Nos. 228-230 .....	\$750
GM&O EMD GP-38-2, Weaver, EX, FP red scheme, Can Motor, Lights, Road No. 754 .....	\$175
Morristown & Erie ALCO RS-1 Diesel, Atlas, L/N, FP, Road No. 15 .....	\$350
NH ALCO PA A Unit, SS 3rd, Mint, FP, Early Green Scheme, DC/DCC Sound, Can Motor, Lts .....	\$725
NP EMC FT A-B Units, OMI, L/N, CP, Black, Can Motors, Fly Wheels, Nos. 6002C-6002D, Ajin .....	\$1395
PRR Centipede A-A Set, MTH No. 20-2200-2, L/N, FP, Brunswick SS, 2 Rail, Road No. 5823 .....	\$695
PRR EMD E8 A-A Units, SS 3rd, Mint, FP, Tuscan Five Stripe, Both Powered, DC/DCC Sound .....	\$1550
SP EMD SW1 600 HP Phase II, ORI, P:48, New, CP Black, Can Motor, No. 1004, Samhonga .....	\$895
SP FM Train Master, MTH, L/N, FP, Black Widow, Proto 3.0, 2 Rail, Road No. 4802 .....	\$450
Washington Union Terminal ALCO RS-1 Diesel, Atlas, L/N, FP, 2-Rail, Road No. 46 .....	\$350
WM ALCO S2 Switcher, CB, L/N, CP Blk/Fireball, Can Motor, Blunt Trucks, OB, Road No. 143 .....	\$595
WM EMD SD35, MTH, New/TR, FP, Late Scheme, 2-Rail, Can Motor, Proto 2.0, Road No. 7432 .....	\$325
WM SD35, Atlas, New, FP, Late Scheme, Item #7819-2, Road No. 7433 .....	\$350
UP Veranda Turbine, OMI, EX, CP, LED HL, Special Tender, Road No. 61 .....	\$1750
ATSF RDC-1 Coach, DVP, New, FP Stainless, Interior, Can Motors, Road No. 191 .....	\$1095
C&NW RDC-1 Coach, DVP, New-TR, FP Stainless, Interior, Can Motors, Road No. 9934 .....	\$1050
PRR Gas Electric, SS 3rd, New, FP Tuscan, Interior Detail, Passengers, Road No. 4666 .....	\$625
NYS&W Gas Electric, Erie Design, OMI, L/N, CP Red/Gray Scheme, Powered, Road No. 5012 .....	\$595
NYC S1 Electric, Kit-Built, The Electric Shop, L/N, UP, Brass Construction, USA .....	\$595
PRR Class B1 "Rats", Twin Units, CB, L/N, CP, KES Drives, Road Nos. 5686/5692 .....	\$895
PRR DD1 2 Unit Box Cab Electric, MG, EX+, CP DGLE, Dual Drive, NOB, Toby, Japan .....	\$695
PRR GG1, Kohs, L/N, FP, Five Stripe Brunswick Scheme, Futura Lettering, No. 4801 .....	\$4895
PRR GG1, CB, L/N, CP, Tuscan 5 Stripe, Wolfer Drive, Can Motors, LEDs, Road No. 4912 .....	\$825
PRR P5a Box Cab Electric, SS 3rd, Mint, FP, Can Motor, Road No. 4746 .....	\$795

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**March/April 2026**

Volume 13 No. 4

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Welcome to the online *O Scale Resource Magazine*. Our magazine is presented in an easy to use format. The blue bar above the magazine has commands for previewing all the pages, advancing the pages forward or back, searching to go to a specific page, enlarging pages, printing pages, enlarging the view to full screen, and downloading a copy to your computer.

Front Cover Photo

*A westbound freight prepares to head out of Midway as Train No. 12 has cleared the main on Al Love's CPR's Southern Main Line In BC.*

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The Model Railroad Resource, LLC publishes *The O Scale Resource Magazine* six times a year. We also have a line of 3D products as well as custom printing.

[Click here for our 3D offerings!](#)

# From the Publisher's Desk

*Hello and welcome back to the Publisher's Desk, a/k/a rant, diatribe, harangue, tirade, and jeremiad page. I think I covered everything.*

**Money March** we'll call it! Two big shows are coming up shortly and you need to get to at least one of them if you can!

## O Scale West

O Scale West, the only O Scale, S Scale, and Narrow Gauge Meet west of the Rocky Mountains, is less than 7 days away. New location on a new date: the dates are March 6-7 Friday-Saturday. It is located at the Portuguese IFES Hall at 432 Stierlin Rd., Mountain View, CA. Open hours are Friday 1-4PM and 6-9PM and Saturday 9-4PM. Family registration for both days is \$35, Saturday only is \$25. [Click here for all the details!](#)

## The March Meet

The largest O scale show in the country will begin March 19th. The March O Scale Meet is a 3 day gathering of vendors, customers, clinics, and fun held annually in March in the Chicagoland area. This is the Chicago O Scale train show you've heard of. [See ad in this issue.](#)

- [Click here for On-line Registration](#)
- [Click here for Clinics](#)
- [Click here for Layout Tours](#)
- [Click here for the Main Schedule](#)
- [Click here for List of Vendors](#)

Three articles in this issue will have their authors and projects at the show. James Schultz will have his work in progress "Little Joe" on display. Marty Megregian will have a clinic and be showing his Southern Railway 2-10-2+2-6-2 locomotive which he built, and Santiago Pineda will be here with a clinic and showing his AI created cab figures. You don't want to miss this show. Remember, it's not all about spending money, but meeting new people, seeing old friends and most importantly, learning from others.

Also as a reminder, join us for the March Meet Saturday evening social. As always there will be plenty of seating, a cash bar, lots of photos/videos, and most importantly, O scale camaraderie. This year, in addition to layout photos/videos, we'll have:

- The 'rest of the story' photos from previous OSR articles that couldn't be squeezed in.
- An opportunity for anyone that would like 'walk up' with an USB stick of photo/videos of their layout or special project they are working on.

Hope to see you there.

Website: <http://marchmeet.net/>

Email: [ChicagoMeet@yahoo.com](mailto:ChicagoMeet@yahoo.com)

Happy Reading & Happy Modeling,

*Amy & Dan Dawdy*

AMERICAN SCALE  
MODELS



# Purchases and Sales of Scale Model Trains

*Estate and collection liquidations*

*Consignment Sales*

*Purchases of new, used, and unwanted equipment*

References gladly furnished.

Bill Davis, P.O. Box 1011, Oconomowoc, WI 53066 • (262) 560-1619 • bdavis148@aol.com  
Visit my website at [americanscalemodels.com](http://americanscalemodels.com) for models, detail parts, trucks, and more!

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## Structures - Small to Big Rolling Stock - Old to New



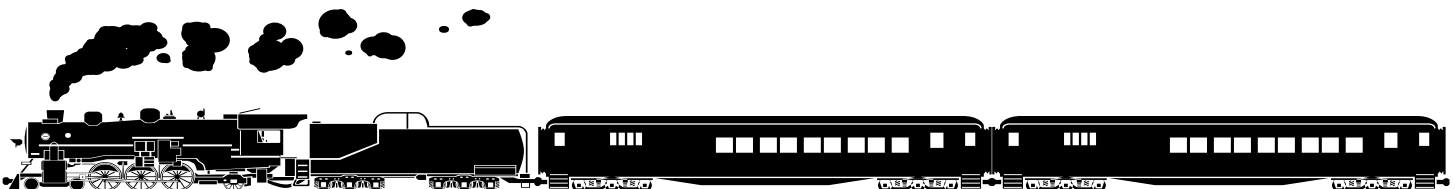
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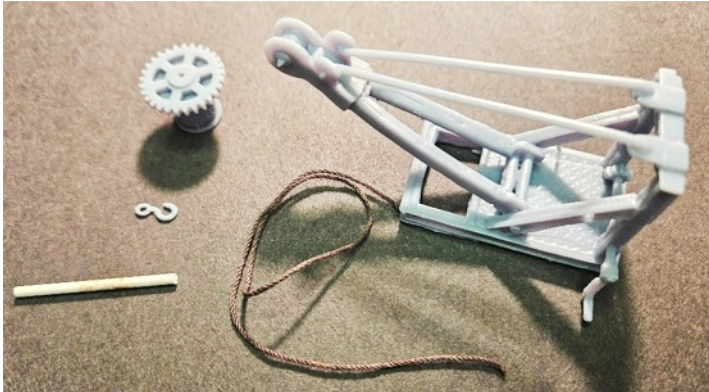
with more coming!



# NEWS YOU CAN USE

[Rusty Rails](#) has some new castings available.

RRTK-O-04 - First Item is a 3D printed Tow Truck Crane.

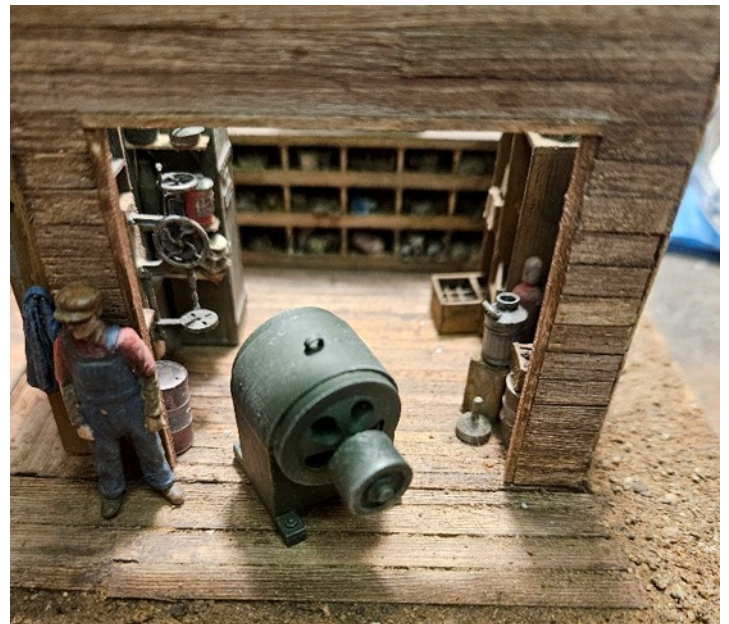


This is a kit that you assemble out of the parts provided. In the pictures you can see the parts: the crane body, cable spool, plastic rod for the cable spool, hook and some cable line. The other picture shows the assembled model ready for painting. Then you can add to the truck of your choice. The assembled Tow Truck Crane is 3" long by 7/8" wide and 2 3/8" tall and comes unpainted. Assembly instructions come with the kit.



Here you have 6 electric motors that are 3D printed for your shop or any business. 3D prints come unpainted.

Also new is a large Electric motor that is now 3D printed. Now you can get some modern power into your shops and mines. 3D items come not painted. The motor measures 1" by 3/4" and is 1-1/8" tall. Minor assembly required.



[Check their Website for all their new products.](#)



Atlas has released their O Winter 2026 Premier Catalog! [Click Here for a PDF of the Catalog.](#)

New paint schemes, and some with new numbers, for the following products:

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Atlas O Comet & Horizon Passenger Cars  
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Atlas O Extended Vision Caboose

All are available in two rail.

The deadline for placing your guaranteed order/reservation with your Atlas Dealer on the product listed in this catalog is Wednesday, March 4, 2026.

[Check all their products on their Website here.](#)



**New Decoder Installer:** I am happy to announce a good friend of mine, Brian Huang, is now offering decoder installation services. You may remember Brian as the “Test Track Guy” at the March Meets where he would test locomotives for attendees and dealers.

Brian is concentrating on decoder installs for 2-Rail O-Scale locomotive models using SoundTraxx decoders: Econami, Tsunami2, Blunami, and Tsunami SoundCar. Brian has good experience and proven results in performing “Battery-on-Board”, aka “Dead Rail” installs using Blunami decoders.



The image above is of two 2-Rail O-Scale model locomotives that Brian converted to on-board battery power using SoundTraxx Blunami decoders. One of Brian’s favorite sayings is “If I can get it apart, I can install!”. You can contact Brian at [2RailBrian@gmail.com](mailto:2RailBrian@gmail.com).



## What's New At All Nation Line:

All Nation Line kicks off the 2026 year with a classic — the Megow C&O steel caboose KIT is back on the rails and ready for builders who crave the real O-scale spirit. It is just what you need to have some fun in the new year.

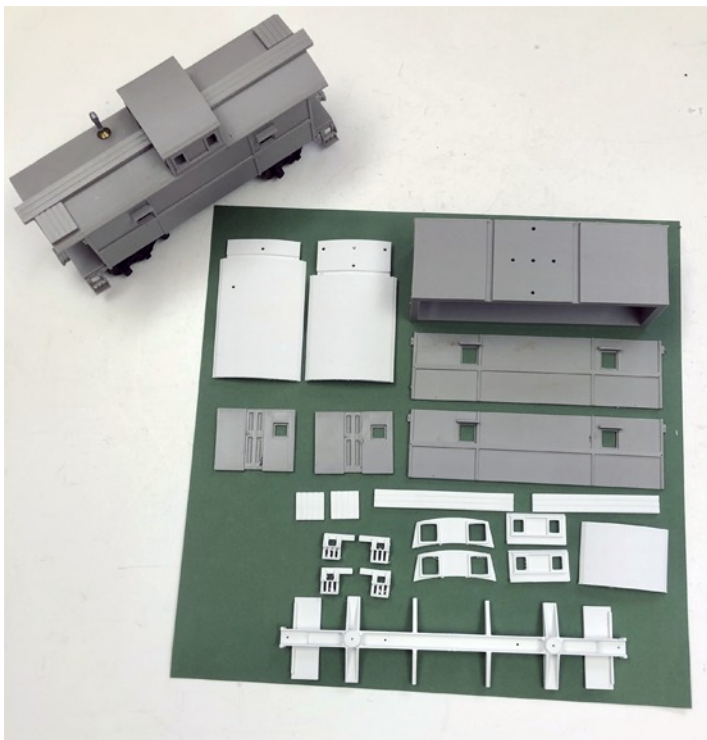


[Click here to view: Scale Model Railways Megow 31' 3" C&O Steel Caboose KIT With 1 Pair L Gauge Pop-on Pop-off Trucks 3D Printed PN#3620ANKLGauge](#)

[Check out all their products here!](#)



New from Pre-Size Model Specialties: Pre-Size Model Specialties announces a new double bridge pier, #655.



[Click here to view: Scale Model Railways Megow 31' 3" C&O Steel Caboose Kit 3D Printed \(Minus Trucks\) PN#3620ANK](#)

Additionally, you can enjoy building this same C&O Caboose KIT with L Gauge pop-on pop-off trucks! You may then humbly run it on any L Gauge layout! The scale model will be the hit of your friends, and potentially grandchildren's, L Gauge pike.

The O Scale Resource March/April 2026



It is a match for our cut stone double bridge abutment (#653) so you can have multiple spans. See this new pier on our website at <https://www.pre-size.com/>

Update from Scot Mann: Hi Folks,

I wanted to tell you all what is going on here at Sunset Models in San Ramon for 2026 and beyond.

We have transferred our model production to Jay Escamilla of Third and Townsend Models LLC. It's a 5 year lease to own. Our models are produced by Jay, under the Sunset Models / 3rd Rail / Golden Gate Depot banners.

<https://www.thirdandtowntsendmodels.com>

Jay is an avid SP Modeler and is very familiar with the hobby. I've shown him the ropes of how to make our models and he's doing this very well. We travelled to Korea and China together late last year to introduce him to the factory people and everyone is excited about the new blood in the business.

I started at Sunset Models at age 34, and Jay is just a few years younger. He is full of energy and new ideas. He's going to make a fresh update of our product lines. I'm sure you'll be happy going forward.

His skill at Social Media has improved the market breadth of our offerings, and we are working together to make sure the Quality and the Customer Service is second to none.

Jay has hired Shani, our secretary to answer the phones and emails, as he is jammed taking on the various activities of creating these fine scale models.

I, myself, will be handling the ERR electronics business, and providing services for the 3 Rail electronics. I am not fully retired, just semi at this point.

Our mailing address is changing to the same address used by Golden Gate Depot.

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We have transferred all our parts of the last 50 years to [GET3RPARTS.COM](https://www.get3rparts.com).

This business is run by Pat Norcross in NC. Follow the link to his web site for parts.

<https://www.get3rparts.com/>

We got rid of the phones, but you can reach me at the same email address as before.

Everything is going as planned.

I hope you enjoy the new product lines and some reruns that are getting a lot of interest.

We have some left over SD40-2s, P42 and Chargers in stock. I will be posting these on our Diesel Sales Page, like I always do in the coming days.

<https://www.3rdrail.com/Diesel-SuperSale.html>

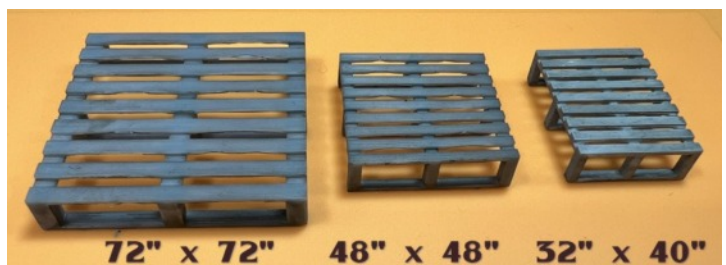
I wish you all the very best in 2026 and beyond. It's been a pleasure serving your hobby needs, and look forward to the future.

Best Regards,  
Scott Mann  
[sdmann@3rdrail.com](mailto:sdmann@3rdrail.com)



New from The Model Railroad Resource 3D Division.

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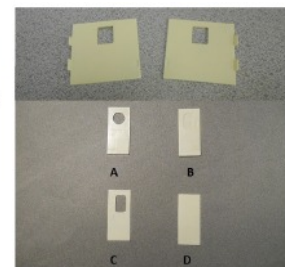
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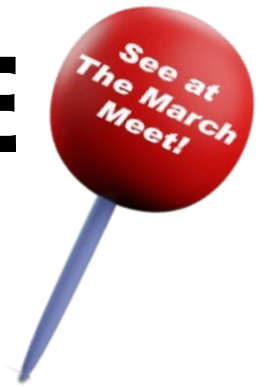
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# A SPECIAL LITTLE JOE



By James Schultz

## History:

In 1946, Soviet Russia put an order in with GE for 20, Bi directional 3300 VDC electric locomotives for use on their Trans-Siberian railroad.

The locomotives were designed as a 2-D-D-2 wheel arrangement producing a 1 hour rating of 5500 HP. They were also gauged to the Russian standard of 5'.

In March of 1947, a Cold War ensued between the Soviets and the USA halting all product shipments to the USSR. GE had completed 12 of the 20 locomotives, and now had nowhere to send them. GE immediately put out the notice to the few railroads that operated a 3000+ volt type distribution system. The Milwaukee reached out and tested one of the GEs. In 1948, GE #750 (so named for the 750 type traction motor used) was delivered to the Milwaukee. It weighed 260 tons and did not perform well during testing. Wheel slippage was a true issue. The wheel to weight ratio proved to be the wrong setup for the Milwaukee's use, and they ultimately turned them down. At the time, GE had offered all 20 motors plus all the spare parts for one million dollars.

During the next two years, GE sold 5 motors to the Paulista railroad of Brazil and 3 to the Chicago, South Shore & South Bend. In 1950, the Milwaukee was terribly short on power and came back to the table asking to buy the locomotives. This time GE had the upper hand and told the Milwaukee, one million dollars for the remaining twelve motors and no spare parts. They jumped on the offer and had the locomotives ready for service in just a few months. E20-21 (Passenger equipped) and E70-79 (Freight). 40 tons of ballast was added under the pantographs as well as raising the pantograph height to reach the 26' wire height. They proved to be the Milwaukee's single most powerful and successful motor ever fielded, and quickly became a crew favorite.

## How E78 got its Nickname:

On May 23rd, 1966, young engineer Del Hart and Fireman Jim Satake were called out of Harlowton, MT at 1800 hours to take E78, E73, GP9 #280 and their train, west to Three Forks, MT. Along the route, the train would pass through Sixteen Mile Canyon and its numerous 25 and 30 mph curves. West bound, the canyon is down hill toward the Missouri River valley. At MP 1400 (for reference, Harlowton is MP 1336) there is a 30 mph curve. From MP 1401 to MP 1405, the track speed increases to 60 mph before entering a 25 mph curve at MP 1405.4. Once engineer Hart left the 30 mph curve at MP 1400, he began to motor downhill and increased the train speed to 68 mph. This is when he had issues with its automatic (train air), and was only able to reduce the train speed to 62 mph before entering the 25 mph curve. The train blew the rail out, lying over on its right side around 2300 hours. All crew members survived the incident.

After the accident, E78 and E73 were rebuilt. E73 was moved to Deer lodge where it would receive work and it's original as delivered look. E78, however, would be moved to Milwaukee, WI for rebuilding. E78 had its cab removed and all damaged paneling. Northern Pacific F3 #6002D was retired and traded to EMD in July of 1966 and became the donor locomotive for the rebuilding of E78. Photos of this can be referenced from the book; *Northern Pacific Diesel Power Vol. 2* by Dale Sanders.

This GE built motor, now adorned with EMD cabs, paneling and grills, stood out from the rest of its brethren. Old Iron sides was born.

For the full back story of how this wreck occurred, please checkout the article by Bill Wilkerson in the *Milwaukee Railroader* Second Quarter 2009. This is an MRHA publication.



*These are all Photos by Jim Satake/ Ed Lynch collection.*

*E78 showing the damage received to the cab in the wreck.*



*The amount of force in play here was quite obvious. 300 tons of steel lays on its side.*



*E78 laying on the engineer's side in 16 mile canyon.*



*E78 in 1973 in Three Forks, MT. Note those EMD grills that have it the nickname "Iron sides".*



## **Construction Techniques**

There was more modification than scratch building on this particular model. I built an intercooler from brass wire, the all weather window was built using brass and plastic, and the fire cracker antenna was made on my lathe. The cab interior requires the most building. The controller and gauges were built from plastic and metal strip. The pilot frame / bolster was built from styrene to help support the pilot and truck.

Using an Overland Models Little Joe, I cut apart both cabs and removed details surrounding the cabs. Using All Nation F-Unit cabs, I cut these into the Overland body and reused the Overland nose. I used epoxy and brass plate to attach the cabs. I used Automotive Bondo and multiple layers of primer to smooth the cab to match the car body.

Since I model in P48, I had to modify the side frame “in” for when the new drivers are placed. You will see this with the frames next to each other. Compare modified, to unmodified. Also note the new bolster design. For whatever reason, Overland poorly designed the pilots on these models. I modified them with a bolster to help alleviate the teetering of the pilot.

### **Detail**

Cutting in metal All Nation cabs to an Overland body is easier said than done. This required very careful cutting with a jewelers saw. I marked areas along the cab section to be removed and slowly cut along, checking as I went.

The All Nation cab required some modification to fit against the Overland body. By leaving a lip on either side of the cab. This allowed the body and nose to sandwich against the new cab. I then mixed up epoxy and shimmed up everything square on the wood blocks. Once everything dried, it was on to body work. This required multiple layers of Bondo, sanding and primer. This is a process that required patience. It was repeated four times, more if needed.

Body work complete, roof details were added. Most are commercial parts supplied with the Joe. I had to make the fire cracker antenna, as well as a set of inter cooler pipes on the leading cab. The hatch below the engineer window was built from styrene and the latches were cut in. The all weather window was scratch built to match the prototype. The side grills were supplied by P & D, but were cut apart to match properly. The cab interior was heavily modified to match that of the few photos that exist.

### **Truck Frames**

Since I am modeling P48, much work must be performed to the trucks to make them prototypical. The frame had to be brought in almost ¼”. The pilot was not well thought out, and I built a bolster to support the truck frames and pilot. Note the new nickel silver cast drivers for this project.

### **Cab Interior**

Very few photos of the cab interiors exist. Using photos and descriptions provided, I was able to render the controls very closely.

### **Finish and Lettering**

Jem primer was used as a base for this model. It will self etch to prepared metals and provides a great surface for paint. Tru-Color paints were used. Circus City provided the custom details. I used a Paasche single action air brush to spray the paint. Tamiya tape was used to give the crisp line between the orange and black. Decals were set over a couple day period with many applications of Solvaset. The final finish is provided by Krylon Matte Clear (part #1311) straight from the can with an even layer across the model.



*New donor cab to be used with the Overland car body*



*New cab ready for install. Nose will be removed from overland cab*



*Left: Cab to nose conversion is mostly complete.*

*Below: Both cabs installed on the car body and now checking fit to the frame and trucks.*

*Bottom: Multiple primer coats to help blend everything in. On to paint!*



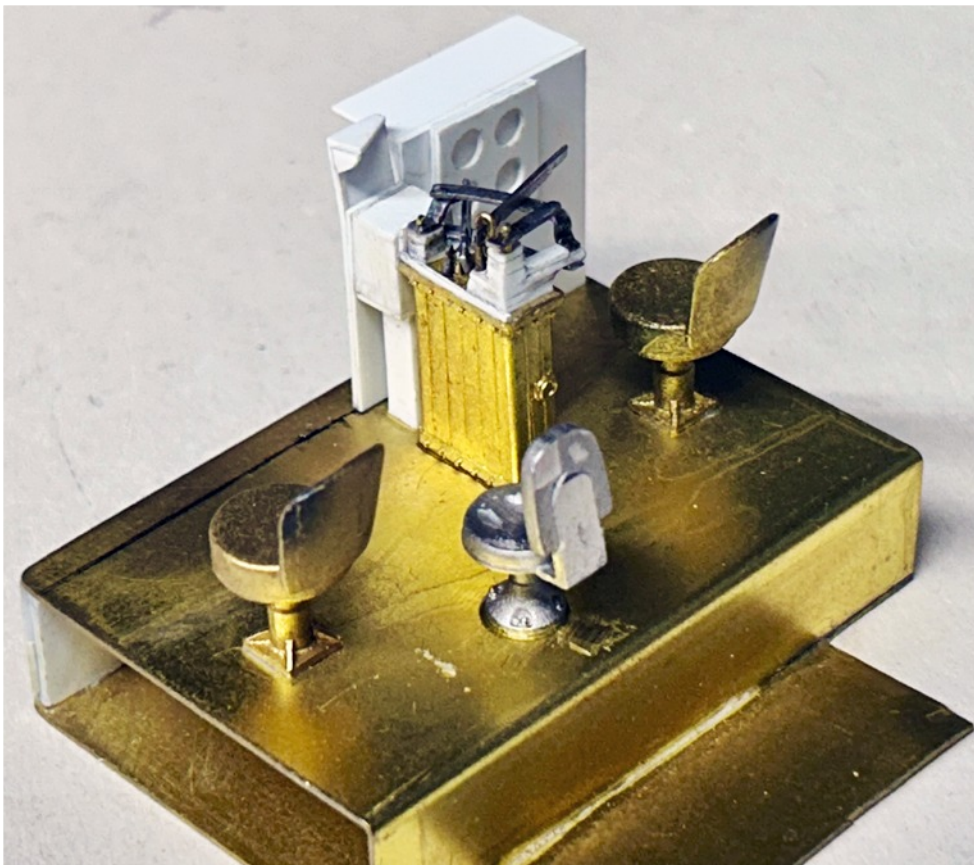
After completion and rebuilding, E78 continued its quiet hum across the mountains until the Milwaukee officially turned the juice off on June 15th, 1974. Most of the Joes were stored at Deer Lodge until they were moved to Chehalis, WA for dismantling. E70 was the only Joe to be saved and now sits in Deer Lodge, MT as a permanent display to a once mighty electrification.



*Above: The donor drive ready to install into the newly completed E78 body.*

*Below: E78 is now painted. Tru color paints were used. The side grills are P&D F unit grills cut and spliced together to match the Joes' carbody.*





*New cab interior is now more appropriate for the Little Joes. Throttle, reverser and Wiley Throttle are all present. (The Wiley throttle made it possible to run diesels in tandem or separate of the electrics) To be added will be the automatic and independent brake stands.*



*One of the side body number boards has survived 50+ years and now hangs on my wall as a reminder.*

# Building The Beast



**My nickname for one of the weirder steam locomotives  
ever built**

**By Marty Megregian**

Southern Railway #5046 was a one off 2-10-2+2-6-2. The locomotive was constructed from a first run SRR 2-10-2 from 1917. The duplex or tractor was built to work the Asheville district, specifically the Loops and/or Saluda grade. The design was basically a glorified booster engine, as the tender drive was used as desired by the engineer. It was a simple engine with separate controls. **See slide 1 of 5046.**

## Slide 1



SRR also built 6 2-8-2+ 2-8-0 and one 2-8-2+ 2-6-2. All were built in 1917. These were used until around 1923 and then converted back to standard 2-8-2's. The 5046 lasted until 1926 before reverting back to a 2-10-2. **See slide 2.**

## Slide 2



The issues were many, but the two major issues were the boiler wasn't really designed for the additional steam use, and the other factor was tractive effort decreased as the coal and water were used.

The project started as a stock Sunset Samhonga built USRA 2-10-2, done around 1978. **See slides 3 and 4 on next page.**

The most important aspect of the model was it came equipped with Southern valve gear. Otherwise, this would have been a much more difficult project and probably not done. **See slide 4.** One thing that is vitally important is the engine was "Southernized", with virtually no looks of a URSA engine in the end.

Why build this? My inspiration was the late Joe Scales. Best I know, he was the only other person in the world crazy enough to attempt this. Joe did not try for an accurate model, just one that satisfied his curiosity

Slide 3

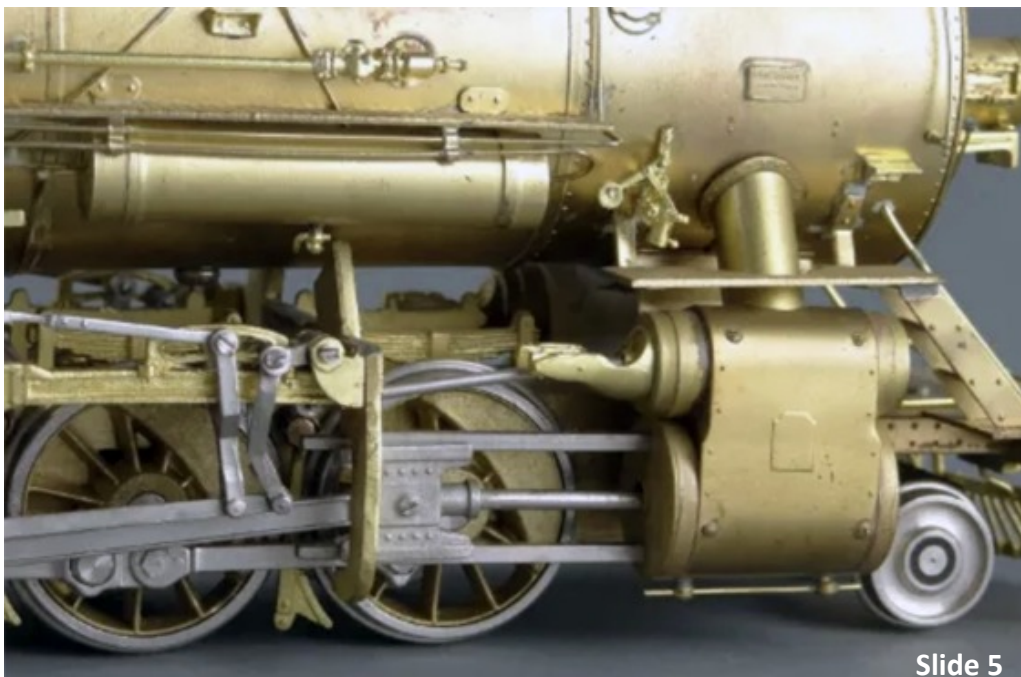
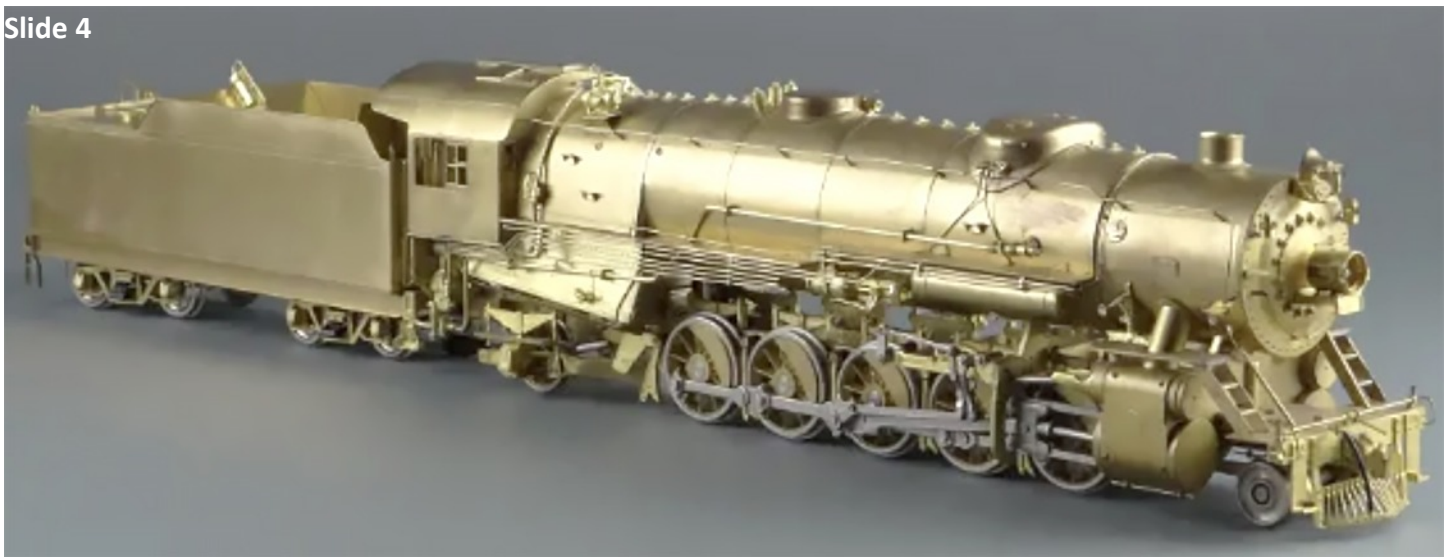


about this engine. Joe was big time a Southern modeler. I would call and blame him for putting the idea in my head to do this. He always got a good laugh out of it!

The problem was, I didn't know where to stop. I only know do it right and go for the gold. That meant using Joe's work as a guideline and then correcting or changing the things he wasn't interested in.

So the starting point was the boiler. First thing was adding the big steam pipe on the right side. **See slide 5.** Bending it was a challenge as multiple bends causes red brass to easily fracture. Rather than go through the running board it was done in two pieces, but you really can't see it.

Slide 4

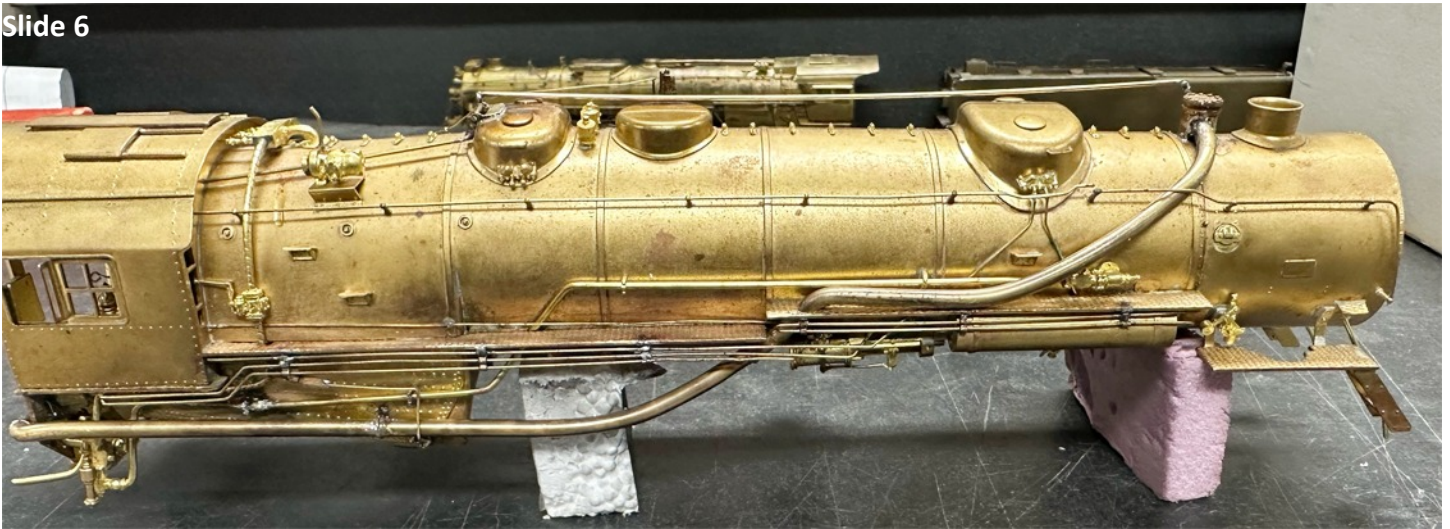


The fun part here was I intended to bring that pipe all the way to the tender cylinders. **See slide 6.** The left view was the stock engine; the right showed the pipe extending back to the bottom of the cab.

Another major change was the left side needed an additional air pump and changes in the coils. **See slide 7A and B. See slide 8A and B** for a picture of the left side of a first run 2-10-2, which was the only guide to the left side as there are no known pictures of 5046 from the left side.

Slide 5

Slide 6



Slide 7A



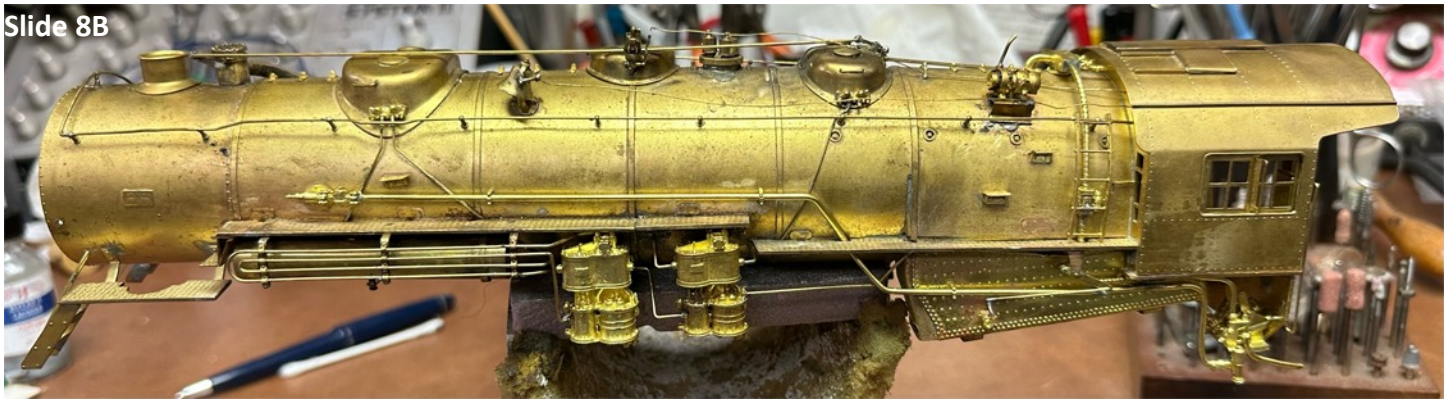
Slide 7B



Slide 8A

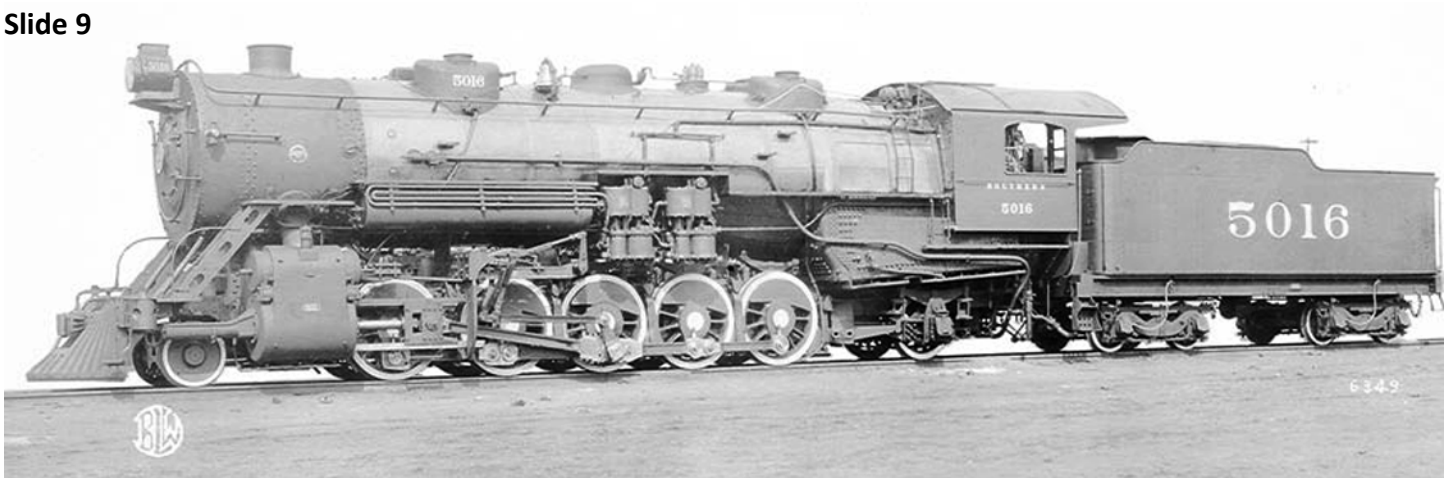


Slide 8B



A major addition was adding front extended piston rod covers. These were obvious on 5046 and all first run 2-10-2 engines the Southern had. These were later removed, although 5046 kept them the entire time it was a duplex/tractor. **See slide 9.** Left side stock model, right side shows the scratch built parts. **Slide 10A and B** shows them installed.

Slide 9



Slide 10A



Slide 10B

The boiler front got attention. The headlight was on top of the smoke box front and the bell went top side. **See slide 11A and B.** Left side is the stock model, right side modified.

Slide 11A



Slide 11B



Next was attention to the drive. The stock drive I have written about before. It was the worst ever in an O scale engine. The replacement was a Sofue gearbox I acquired 25 years ago. Ron Dettmer made the gearbox axle bearings for me. **See Slide 12A and B.** Left side is the plastic stock drive, the right the Sofue gearbox on a custom torque arm. It is whisper quiet.



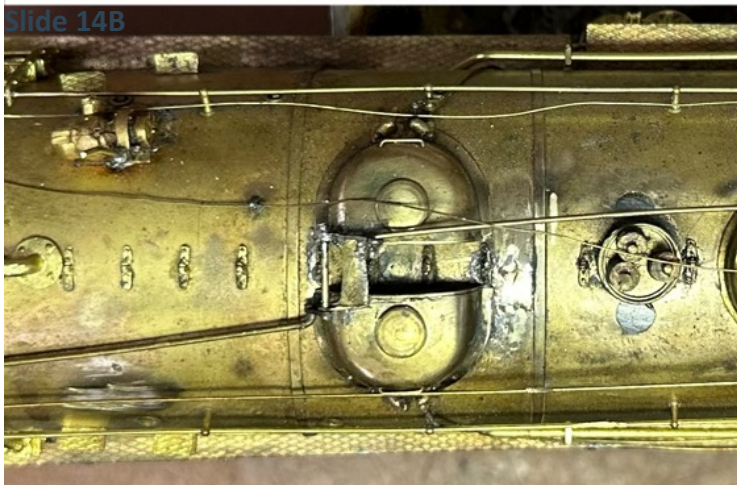
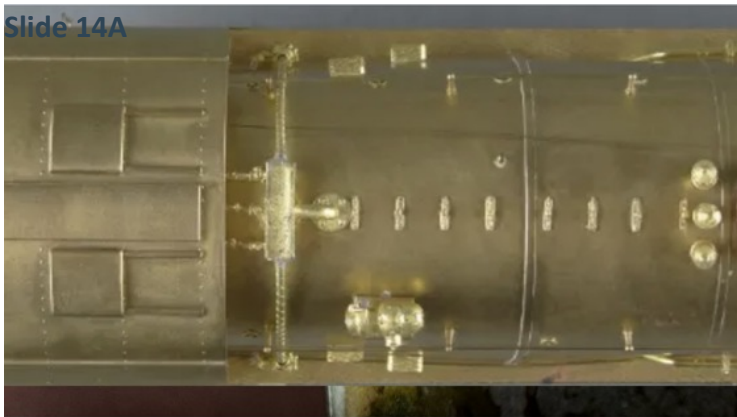
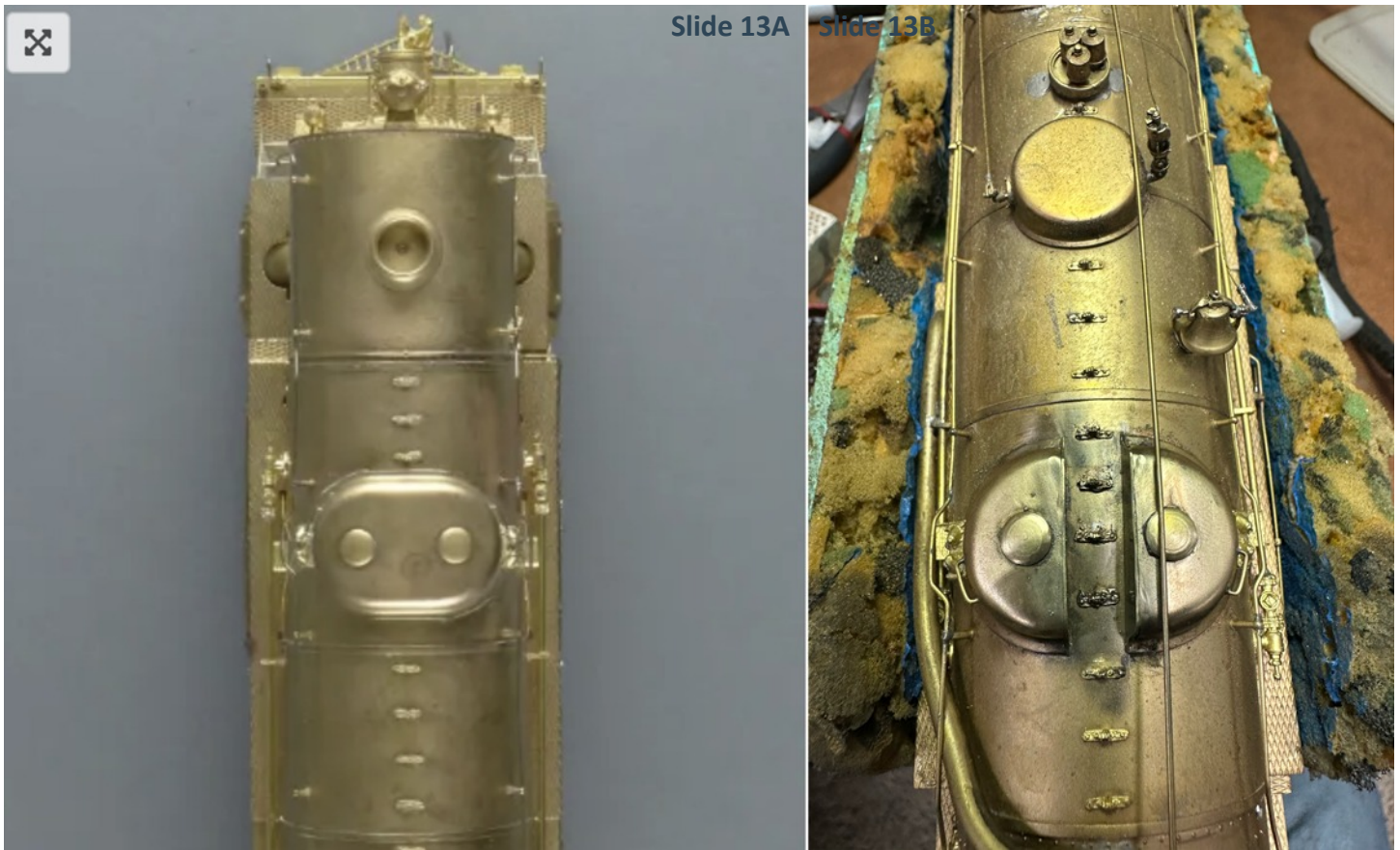
Slide 12A



Slide 12B

The boiler top got a lot of attention beside the steam dome and throttle scratch built for the rear engine. Southern, at that time, had restricted tunnel clearances.

As a result, they split the sand domes on these engines and the early articulateds. The stock domes were split and re-installed with space between them. **See slide 13A and B.** Left is stock. Right shows the front split dome. Both domes were split. **Slide 14A and B** shows the split rear dome.



Now we get to the tender. **See Slides 15 and 16** for stock tender views.

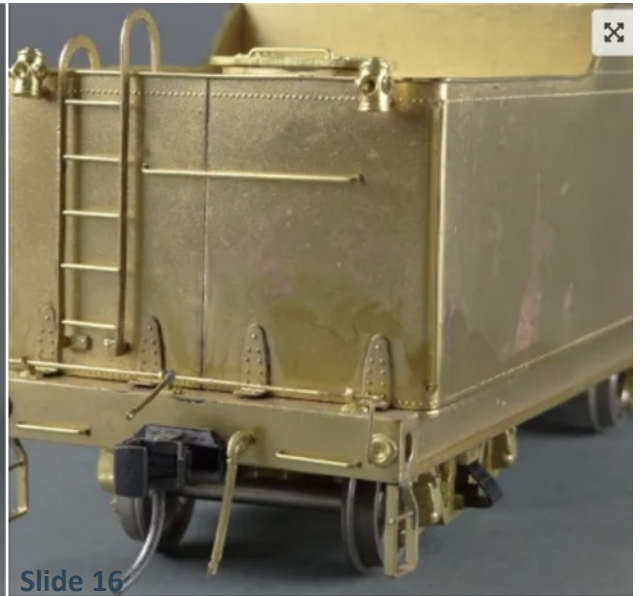
This is where the real fun began. Building a chassis for the tender engine was one of extreme frustration. I could have done what Joe did. Just cobble up an 0-6-0 for the tender. For me, that wouldn't work. It needed the correct valve gear, crossheads and drivers.

Turns out the rear drivers were early, 57 inch old style block counter weights. The only ones available were from Stephenson. It was the same for the frame, cylinders and some other parts including a custom made trailing truck for the large spoked trailing wheel. The brass axle bearings were unusable. Same for the square end axles, as the holes weren't machined properly and the drivers were out of round. Ron Dettmer bored them true, made new axles and bronze Oilite bearings. A NWSL gear box was used.

The side rods also were unusable. They were too long for the supplied drivers and frame, and way too bulky. Ron scratch built the side rods.



Slide 15



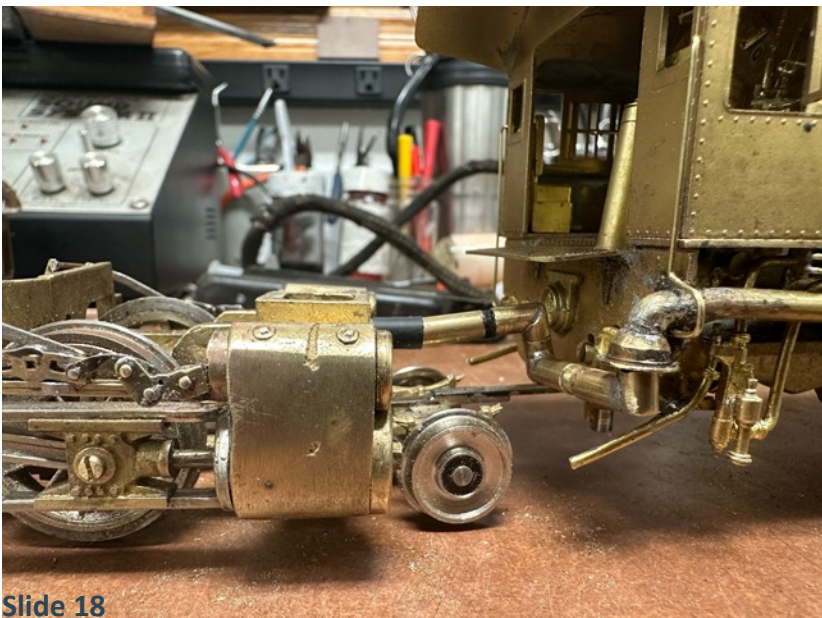
Slide 16

I scratch built the Southern valve gear using clevis valve gear parts from a stash of Weaver parts Dave Vaughan had acquired. See slide 17 for rear chassis.

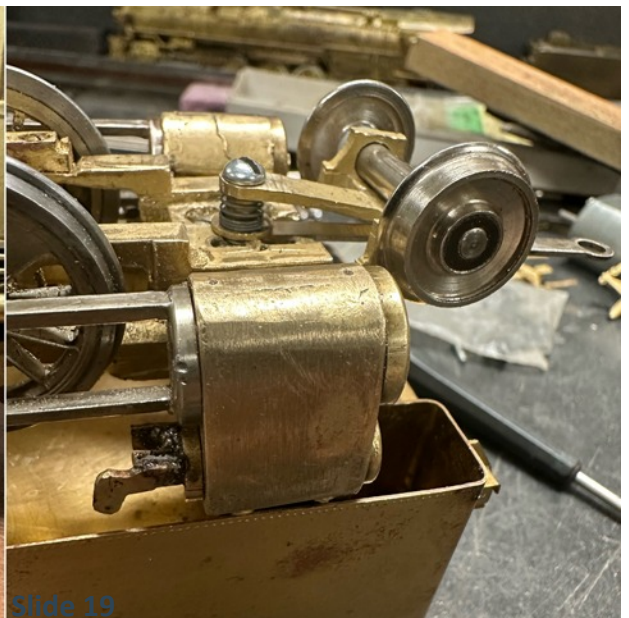


Slide 17

Slide 17 shows the flexible tender to engine steam line. Also seen is the rear engine pilot truck. See slides 18,19.

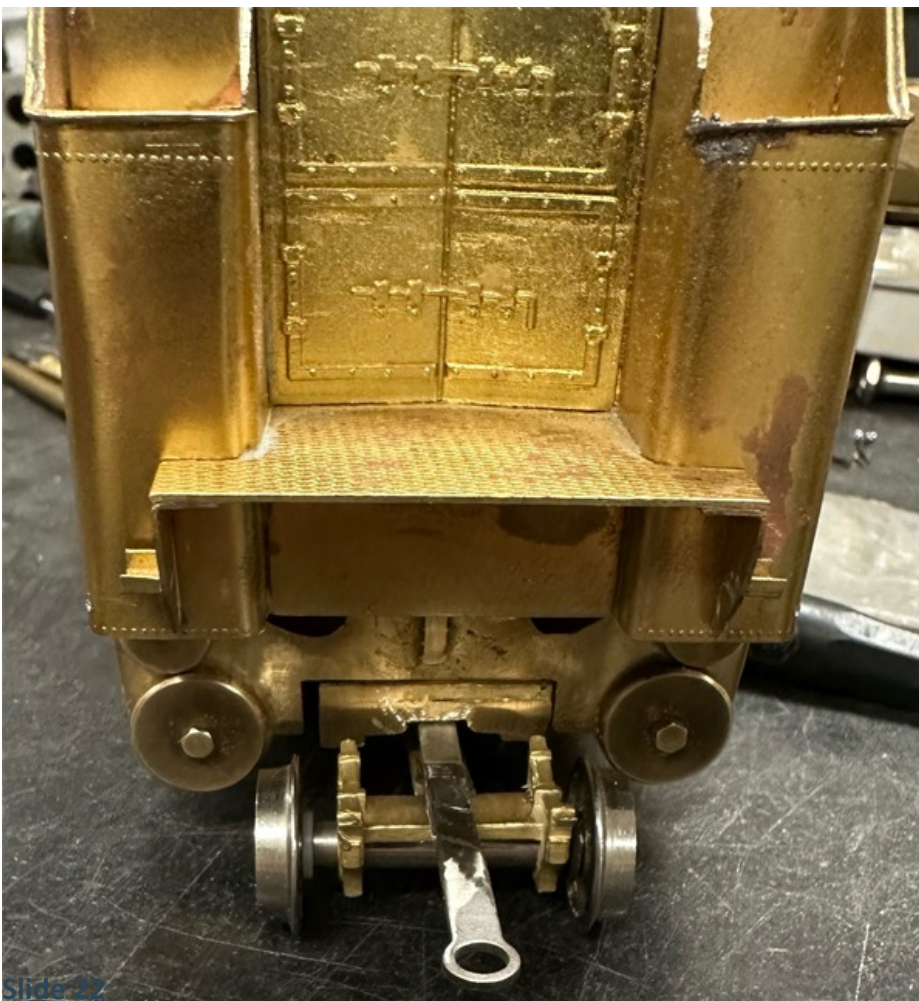
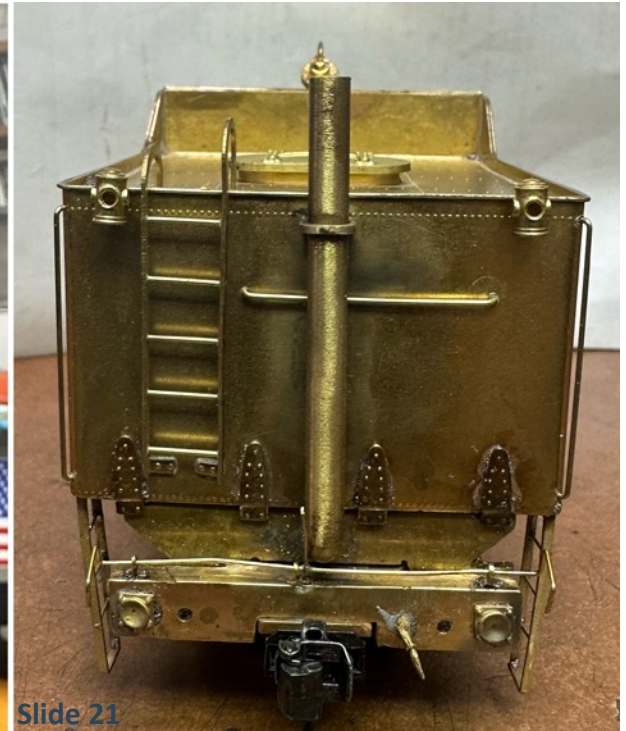


Slide 18



Slide 19

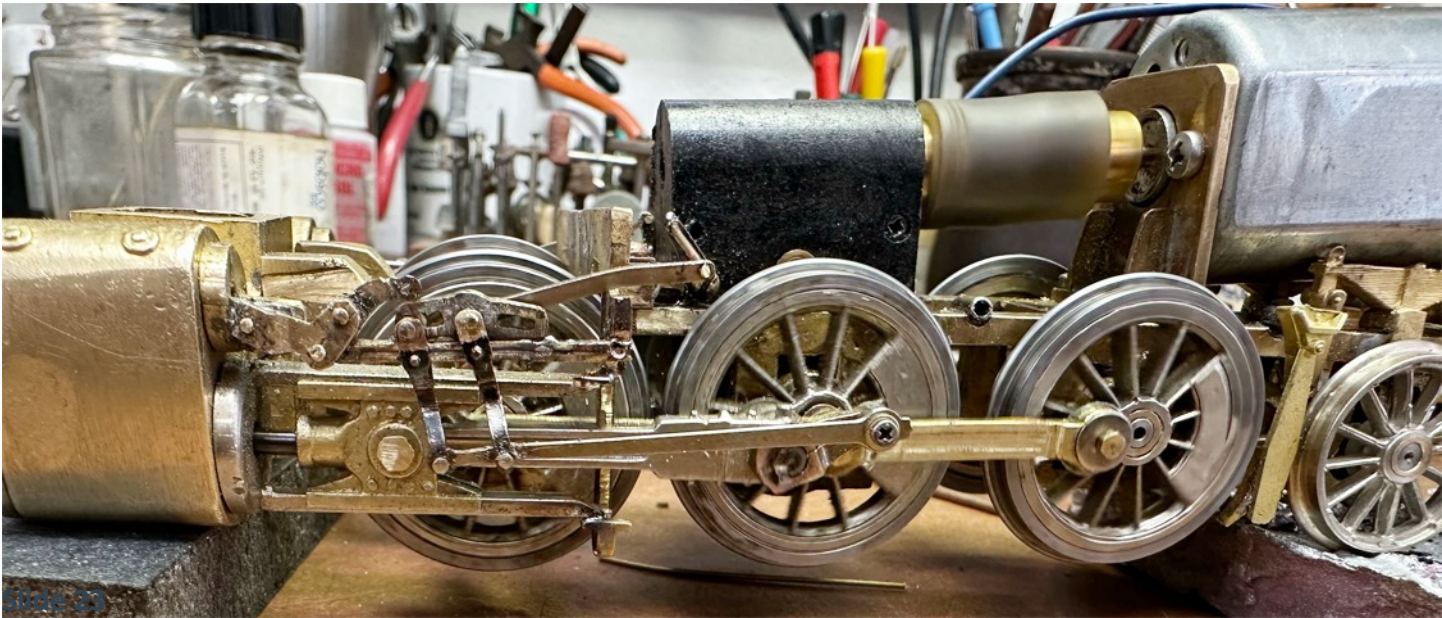
The tender back shows the rear exhaust stack. Of interest, is this is NOT an articulated locomotive. I like to call it a poor man's articulated however! The rear steam would have had a higher pitch as it was steam exhaust only; basically like a fireless cooker. The front locomotive had combustion and steam mixed and was a deeper, fuller more normal chuff. See slides 20, 21.



Then, the tender front needed some attention. The front wall was shortened to clear the cab. See also the modified draw bar. Usually, the drawbar is attached to the engine and the tender hooked to it. In this case, it was the opposite as the front truck was attached to the same screw as the draw bar. Tolerance in this area was very close. See slide 22.

Now you can see the full chassis and drive. The rubber tube was changed for a custom modified Overland universal. See slide 23 next page.

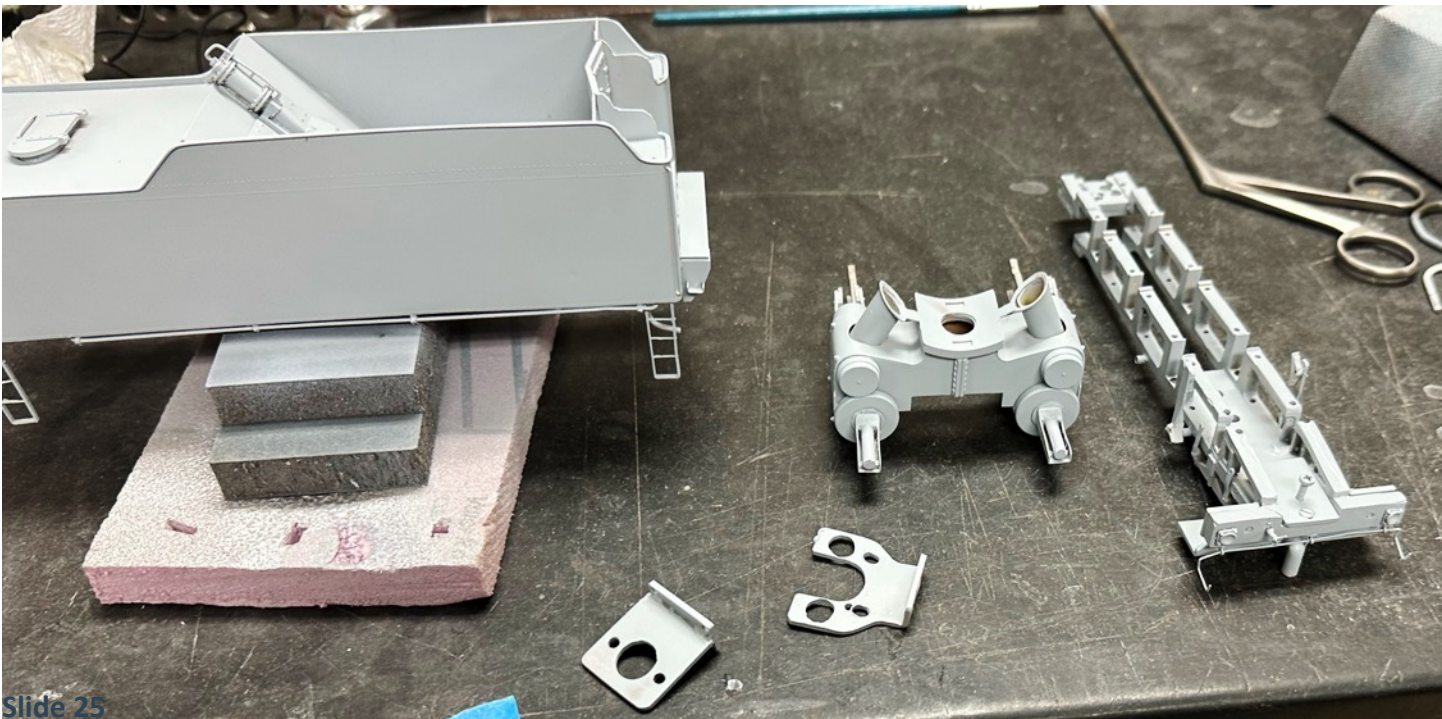
Slides 24 and 25 (Next page) show everything in primer. I used SEM grey for this. Slides 27-31 show the finished model.



Slide 23



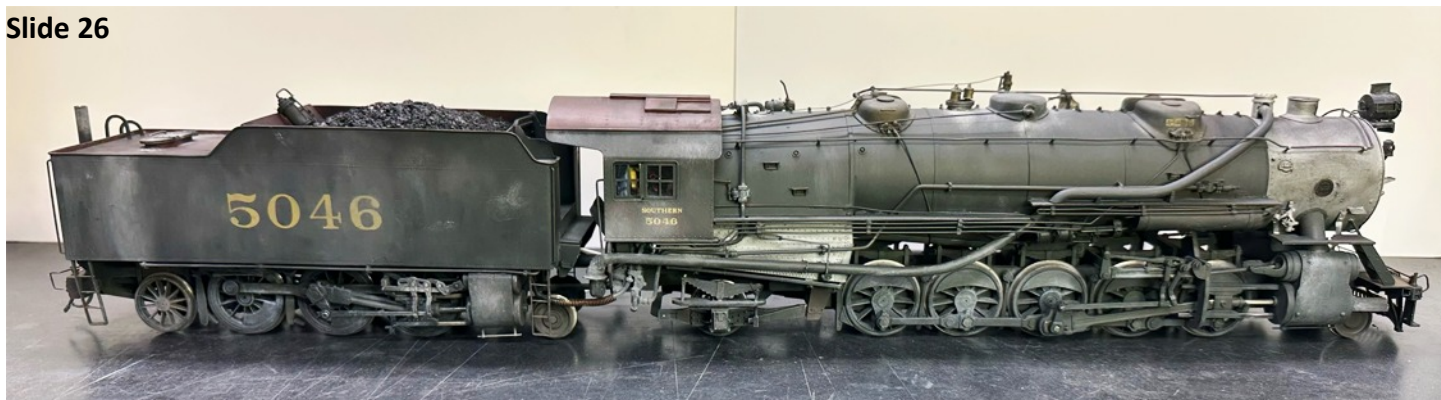
Slide 24



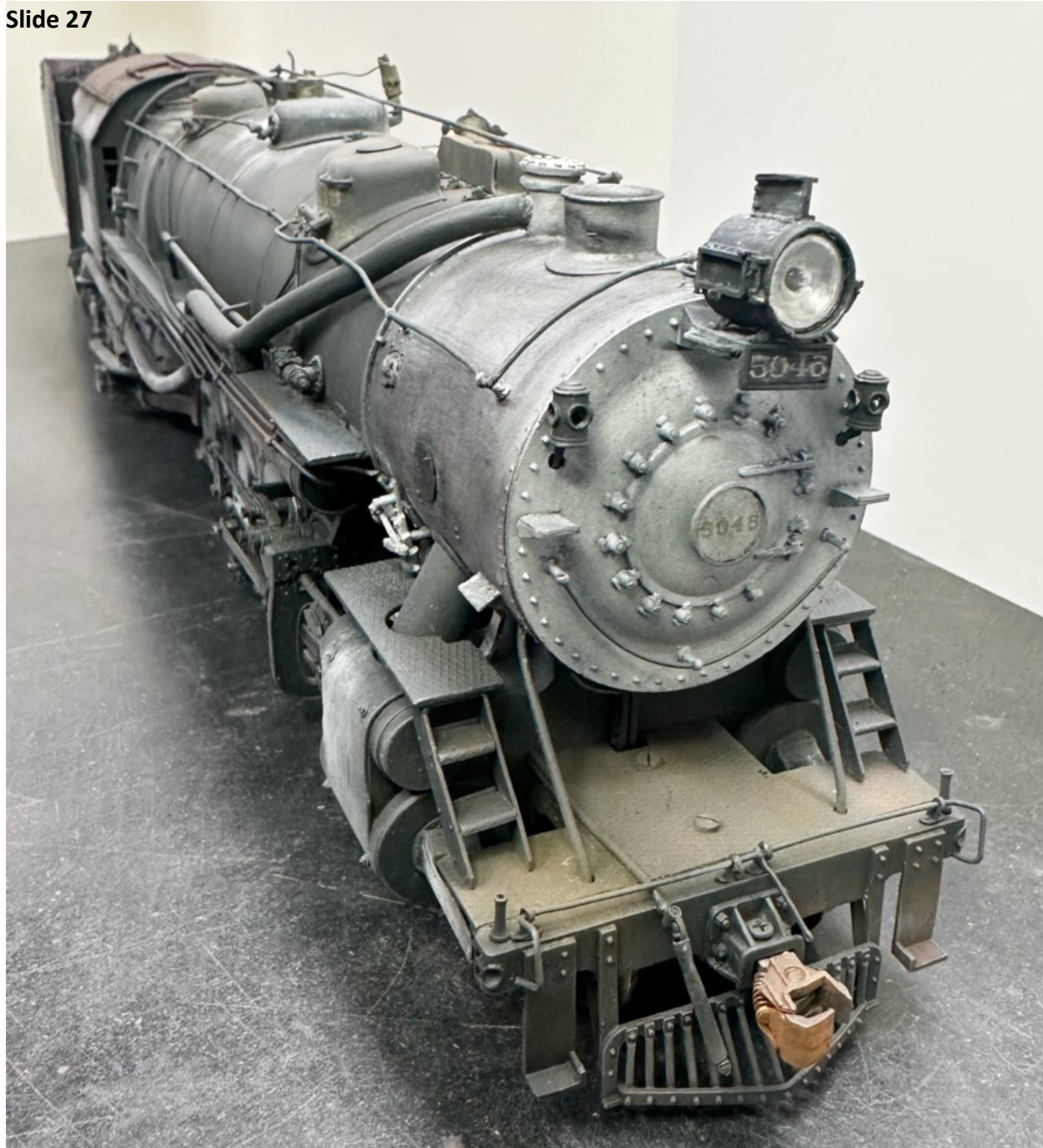
Slide 25

Paint used was Scale Coat 1, mostly grimy black and then graphite and oil for the firebox and smoke box. A Testor's 60% dull coat to 40% gloss was used over the paint. PanPastels were used for weathering. PanPastels are self-setting and no seal coat was used over it. It left a very believable final look to the model. **Slide 26 - 30.**

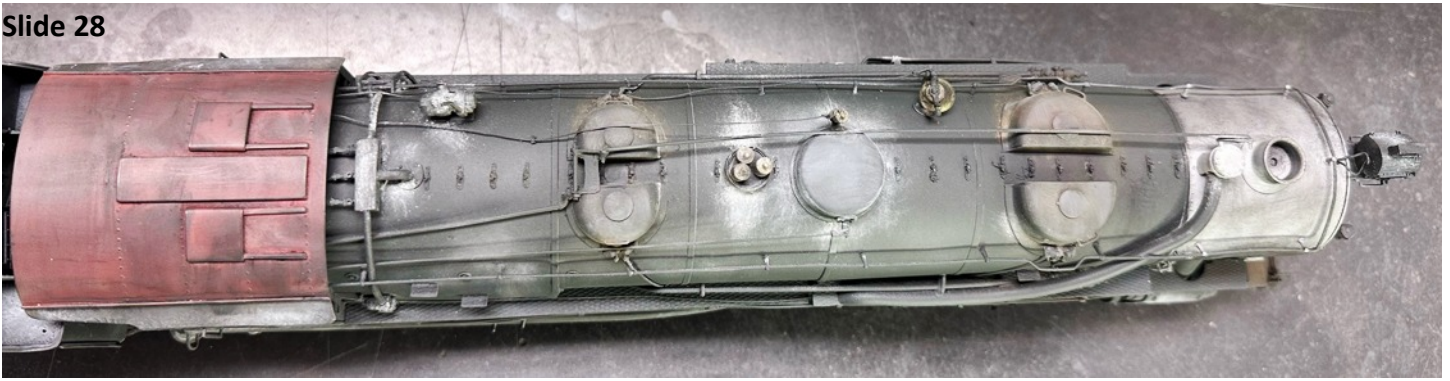
**Slide 26**



**Slide 27**



Slide 28



Slide 29



Slide 30



Slide 31



Slide 31 has the real thing on the top, the model on the bottom.

Slide 32



Control is from a pair of Blunami 4408 decoders. This was necessary due to the entirely different drive trains and the entirely different sound profile of the front and rear engine. A matched pair of speakers were used, one in the boiler and the other in the tender. The original tender floor and another tender were used to speed match the engine and the tender running at the same time on parallel tracks

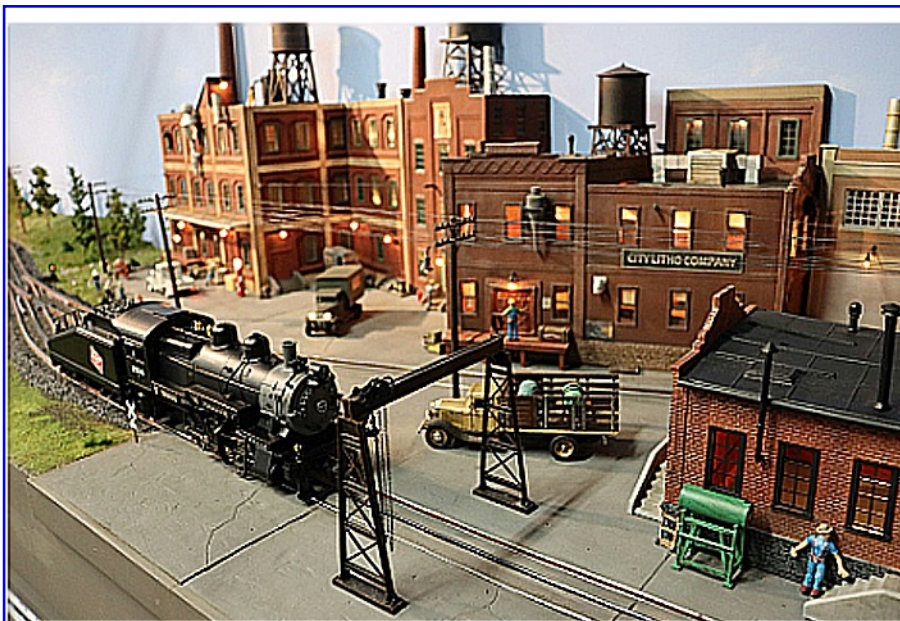
Slides 32, 33, and 34 are the model running on the beautiful layout of the late Ray DuVarney. Performance was flawless. This project took approximately 4-500 hours of time over a 15 month time period.

Slide 33





*Pictures of the prototype from <http://southern.railfan.net/>*



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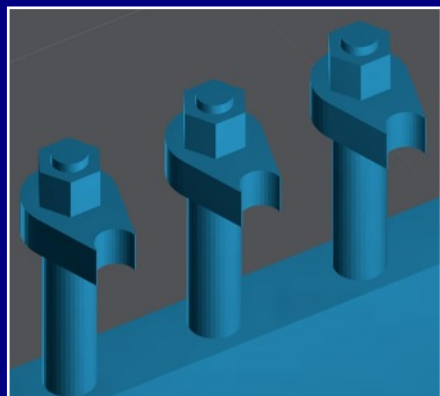
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# PAINING AND FINISHING AI CREATED CAB FIGURES

By: [Santiago Pineda](#)

I've never enjoyed painting figures, and I imagine the vast majority of you neither. The task is repetitive, painstaking, tedious and time-consuming. Nonetheless, figures (especially cab figures) compliment our beautiful models in a special way. After all, the moment we look into an engine cab, we engage in a mode of vision similar to that of the microscope, where we find ourselves looking at a world within a world. The objective for of this piece is two-fold. [First, to demonstrate the quality of the AI created figures depicted in the previous issue.](#) Second, to provide some tips and techniques for streamlining the process of painting scale figures with acrylic and oil paints.

Meet Santiago at The March Meet!



*The finished cab figure collection.*



*Once secured, the figures were covered in a neutral color.*



*The second step was painting the seat and feet black.*

The first thing I do when painting figures is to secure them to a movable base (think Popsicle stick) so I can handle them without using my hands. This allows me to easily rotate them during the painting process while preventing scuffing and oil marks from rough handling. By doing this I also minimize the chance of the figures falling to the floor. If you ask me, this is a non-negotiable. So, first I had my engineers secured to a cardboard base using double sided tape. Then, I hand brushed a thin layer of gray acrylic paint all over the figure. This step helped me to better see the figure's surface while providing some build up for subsequent layers. In other words, this was kind of a "primer layer".



*To ease the process, the underlying layers were painted first. In this case, the skin.*



*Since the shirt and hats are “over” the skin, these were painted afterwards.*

Once dried, I painted the seats and engineer’s feet black. The reason for this step is these areas were easier to touch up than to paint later on in the process. Now, here’s my rationale for the order of painting: ***to create definition between colors and areas, you need to be able to brush confidently on the figure. This means that areas that are “under” get painted first while areas that are “over” are brushed on later.*** In other words, what is “underneath” gets painted first. In this case, the skin is under the shirt, the shirt is under the gloves, and so on. By doing this, the process becomes less tensioning and more care-free.



*Facial features were suggested but not meticulously depicted.*



*A light wash helped soften the facial expression.*

In this line of reasoning, after painting the skin I continued with the shirts and hats. Later, I depicted the overalls, the bandannas and the leather gloves. Remember to keep your paint mixes somewhat thin. You want your acrylics to be thick enough to give you noticeable coverage, but thin enough not to diminish all the figure's details.

In my opinion, depicting facial figures in high resolution is optional. I agree that when it's done well, it can add magnificent accents to the figure, but when it's done wrong it can most definitely ruin an otherwise good figure. At any rate, what you do not want to do is depict oversized eyes or mouths. Here, I simply suggested the figure's eyes with a small brush mark using burnt umber. And the mouths with a small dab of a slightly warmer skin color. Then, to smooth the expression, I applied a light wash of the skin's color to help tie everything together. The advantage of using this method was that it significantly reduced the working time; especially since I was painting several figures at the same time. Also, keep in mind that you can do touch-ups during the entire process. And, since acrylics dry quickly, all areas can be retouched repeatedly.



*Facial expression were suggested using quick markings and a light wash.*



*A light gray was applied to the clothes to tie the look together.*



*Once dry, the figures showed a more natural look.*

The following step was similar. To bring figure's look together, I applied a light wash over the figure's clothing. This time, I used a light gray color. A common mistake in figure painting is using colors straight from the tube. Doing this creates toy-like figures that lack nuance of color value due to their high saturation. In this case, it was tempting to use a simple white wash, but applying a light gray instead, rendered more natural better results.

One of the easiest ways to enhance the look of painted figures is performing dark a pin-wash. A dark pin wash is a controlled weathering technique that uses heavily thinned oil or enamel paint to accentuate recessed details such as creases, panel lines, and rivets. Mixed to a very fluid, almost ink-like consistency, the wash is applied sparingly with a fine brush and allowed to flow by capillary action along engraved lines or around raised details. The dark tone, often a mix of black and brown, creates subtle shadows that enhance depth and visual contrast without staining the surrounding surfaces. When applied over a sealed, glossy or satin finish, excess wash can be easily removed with a clean brush or soft cloth lightly dampened with thinner, leaving crisp definition that emphasizes the model's details and depth.



*Top: Excess oil needs to be absorbed from oil paints.*

*Right: The pin-wash consistency resembles ink.*





*Applying a dark pin-wash enhances the figure's details.*



*Removing the pin-wash excess was easily done with a clean brush and enamel thinner.*

Pin-washes can be applied with both oil and enamel paints, but I choose oil paints because they have higher pigment concentration and longer working times. However, to use oil paints it is best to remove the oil content by squeezing some paint on a piece of cardboard and letting it sit overnight. The cardboard absorbs the oil and you're left with a highly concentrated pigment paste over a halo of oil on the cardboard. By doing this, the oil paint's curing times are reduced considerably, as oil paint can take weeks to dry due to the amount of linseed oil they contain.

To make the pin-wash mix, I used 80% enamel thinner and 20% oil paint. But first, I mixed burnt umber and black to create a dark brown color. Then, I applied the mix sparingly on the figures using a soft brush. This made the figures look dirty at first, but keep in mind that this was not the final look. After a few hours of drying, I used a clean brush and enamel thinner to carefully remove the pin-wash excess. Since the pin-wash wasn't completely dry, most of it was easily cleaned. However, I was careful not to remove it from creases and wrinkles.

*The pin-wash added depth and detail to the figures.*





*Close-ups make us forget how small these really are.*



*A pair of modern day freight cab figures..*

In summary, make sure you secure your figures to a movable base for easy handling. Beware of the paint viscosity. Not too thin that it doesn't cover, not too thick that it obscures details. Don't use colors directly from the tube. Mix with grays and others to reduce saturation and widen the color value. Use washes to bring the look together. Don't be afraid to touch up throughout the process. Finish with a pin-wash to enhance the details and create depth.

Much like wiring, painting figures is hard to love. I hope I was successful in describing an easy-to-follow workflow for painting scale figures. But more importantly, I'm thrilled to have followed through with this project. I no longer need to pay for nor use figures that I found lacking. It's funny how almost every aspect of our models has gotten better over the years except for realistic, true-to-scale figures. And, the best part is AI engines will continue to improve to the point where drawing for 3D printing will no longer be necessary.

*Note that the figures size is right on that sweet spot between the oversized Artista figure (left) and the undersized 3rd Rail figure (right)*



# THE KETTLE VALLEY RAILWAY 1952

## THE CPR'S SOUTHERN MAIN LINE IN BC

By Al Love

My fascination with models, and particularly railway models, can be traced to my childhood years in England where I was born. Train trips from Manchester to visit family in Scotland were exciting times in the early post-war period, and the fascination with steam was planted in my brain. My first model was a Hornby clockwork tank engine and an oval of track. Even as a child I wanted more.

My family moved to Canada in 1952, and I got my first Lionel train set shortly after. With help from my father, we set up substantial sized layout in the basement but, by my late teenage years, the toy train look and three rails sent me searching for greater realism. It was then that I turned to HO. Over the next four decades, several layouts were constructed and each one moved me slowly from fictional settings to modelling actual



*The eastbound Kootenay Express is arriving at Midway BC, the east end of the Kettle Valley Railway. In charge is P1a Mikado No. 5101 which was regularly assigned to this train. On the abandonment of the KVR, the station was saved and moved a short distance to the south beside BC Hwy.3 and now houses part of the local museum. The chimney in the foreground marks the engineman's bunkhouse as Midway was a crew change location, and for that reason, no train order board was required on the station structure. All through trains would take on water and change engine crews at Midway.*

locations and presenting them in such a way, even though compressed, were clearly recognizable by those familiar with the area. I think the model magazines and the Internet have played a huge part in providing more information to the modeller to allow historic representations of railways as they existed years ago possible.

I'm sure the reader can appreciate the phenomenon of looking for a new house and searching to see where the layout was going to be built. Having lost my first wife to liver disease, I was fortunate to remarry a former university friend. We decided to make a fresh start and sold our previous homes and built a new one with ample space in the basement for my empire and her craft room.

### The KVR in O Scale

Construction of the present layout commenced in late 2012. The room measures thirty feet long by approximately twenty-five feet wide. All of the benchwork is of 1x4 in a grid pattern with two feet between cross members and supported by 2x2 legs. The track plan was devised while the house was under construction. Key locations along the KVR that fit the space available and could configure to the necessary curves were considered. Historic accuracy was very important in the track arrangement, but compromises had to be made due to space limitations.

It is necessary to point out that the layout was never intended to be an operating one but rather one that would allow continuous running with the possibility of some switching at a few locations. Midway, the east end



*Extra No. 5249 has just passed the telephone shack at Brodie Junction four miles west of Brookmere. Trains that were heading into the Coquihalla on the far leg of the wye were required to gain clearance from the dispatcher in Penticton before proceeding. Both bridges cross the Coldwater River. From this point the locations that we have modelled do not hold true to the sequence of the actual locations along the KVR, but some were too impactful to resist.*

of the KVR, and the yard and engine facilities at Brookmere were a must. Merritt came into focus because of the track flow and bulk oil facilities. That left the remaining space to find locations that could represent the Coquihalla Pass. A tall bridge, Ladner Creek, a interesting station stop, Iago, and a snow shed that was interesting, Shed 15.

All ties were cut from clear pine boards and rail and switches are made from code 148 steel rail. Hand laying allowed for the pleasing flow of track work. Turnouts are powered by Tortoise switch machines with the exception of the shelf add-on which are all hand throws.

I have failed to mention the shelf portion of the layout which was added a few years later. I have always been fascinated by the CPR's lake operations in British Columbia, and on a previous layout, had included the Slocan Lake tug and barge operation which entailed placing the whole train onboard a barge to be shipped nineteen miles up lake and offloaded at the Rosebery slip. The Slocan lake and branchlike service lasted until 1988.

### **Not Just a Labour of Love**

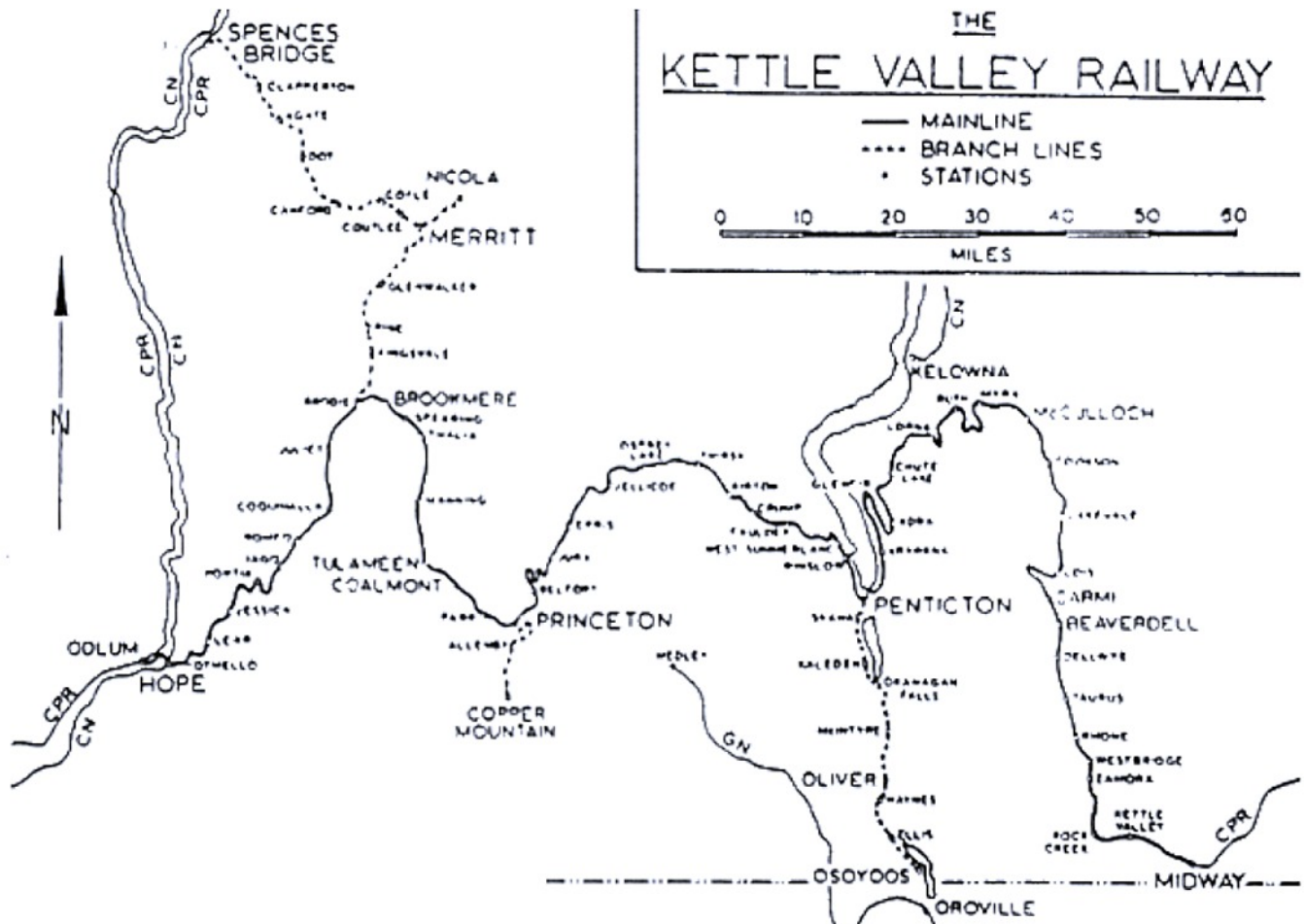
I have been blessed to have been assisted, from the time of constructing the benchwork until today, with loyal and talented modellers. Joe Dodd has been helping in all facets to be followed shortly after its beginning by Bob Huculak. We meet almost once a week and discuss our visions for "getting it right". Al Hough has helped in many ways from caboos construction to being our "locomotive whisperer".

Special mention needs to be directed to Anthony Craig, whose Kettle Valley Model Railway Blogspot and his personalized assistance, have been vital to making the layout a reasonable representation of the former Kettle Valley Railway as it was in 1952. Thanks to all for it has not just been a labour of love.

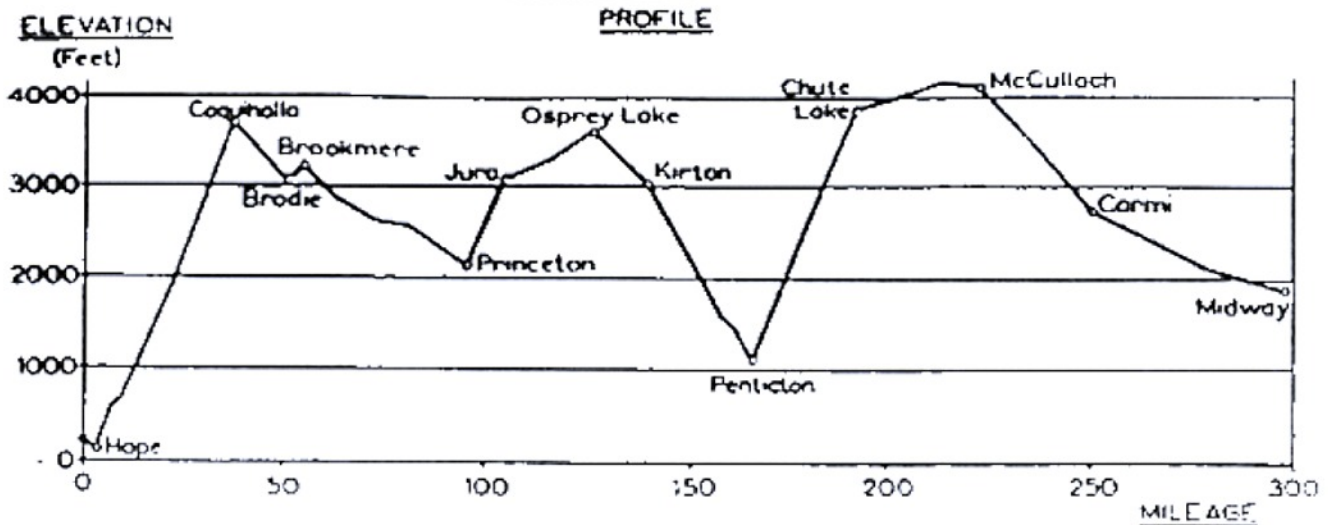


*Just after crossing the Coldwater, our train eases to a halt to take on water from the unenclosed tank at Iago. Chief engineer of the KVR during its construction, Andrew McCulloch, named many of the stations in the Coquihalla after Shakespearian characters. Iago's section house/station's original location was in the middle the siding on the line on the west side of the Coquihalla Pass, but was situated on an avalanche slope and, for the safety of the agent and his family, it had to moved to a position at the east end of the passing track. It was a must to model this location.*

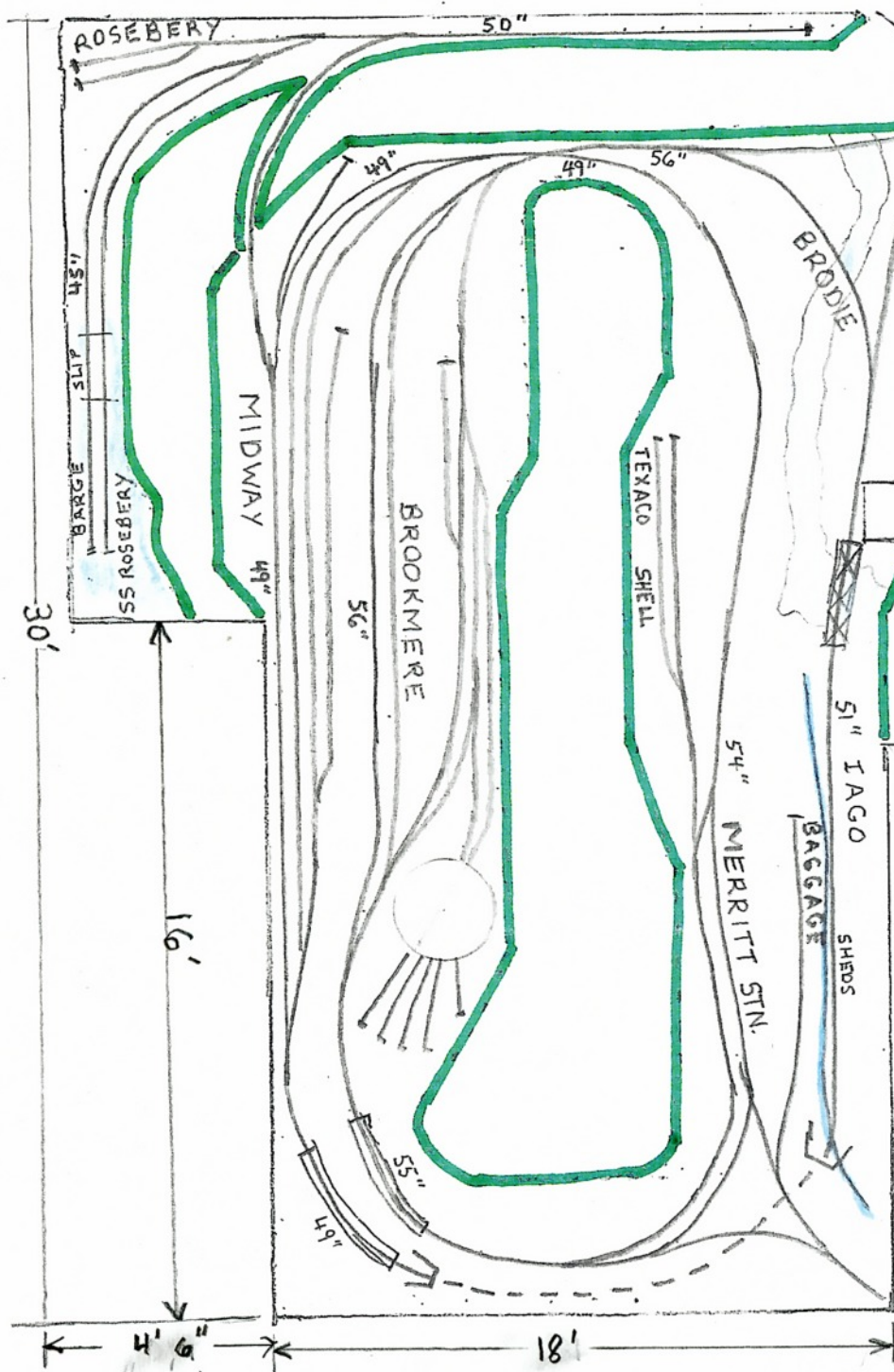
# MAP OF THE ORIGINAL ROUTE OF THE KETTLE VALLEY RAILWAY



## KETTLE VALLEY RAILWAY



# Al Love's 'Kettle Valley Railway' 1952

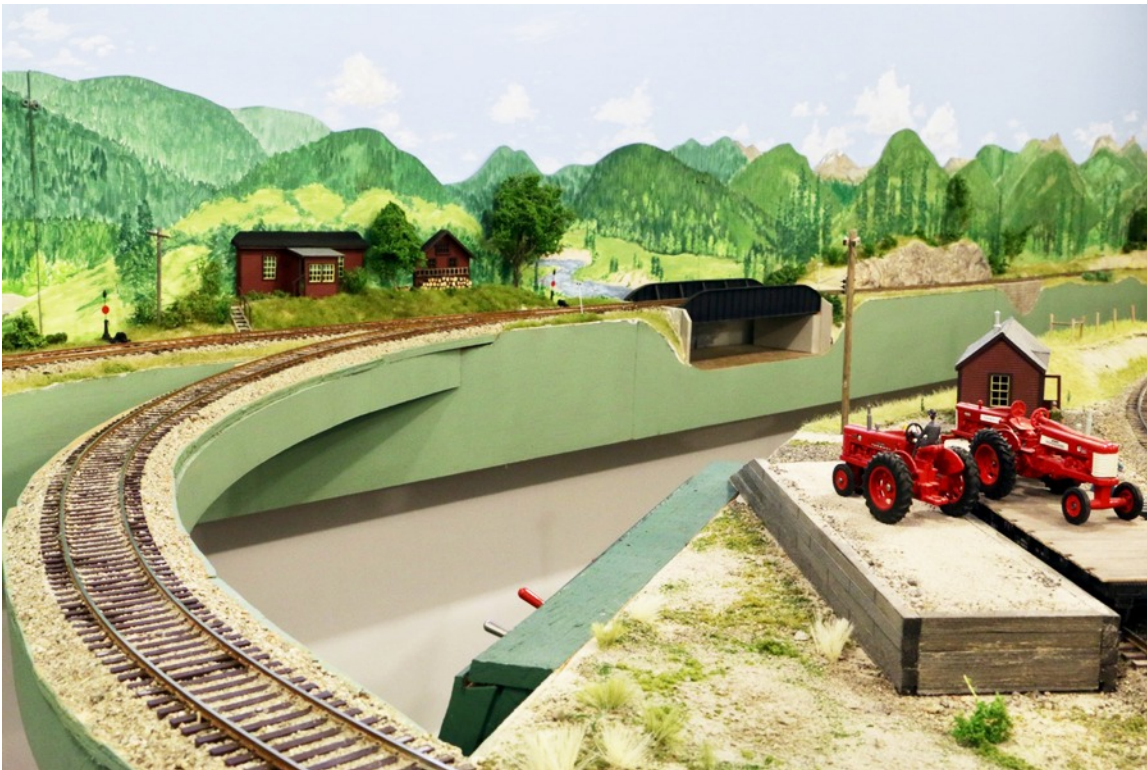


O scale 2 rail  
 Handlaid Code 148  
 Mainline Curves 60" rad.  
 Grade max 2.5%  
 Switches #5 + #6  
 Mainline run 110'

Era 1952  
 - transition to diesel  
 1953  
 - steam conversion to oil  
 1949  
 - track height above  
 floor eq 50"



*Mountain railroading, like the KVR, required dealing with snow and avalanches; and the KVR had to deal with massive amounts of snow. A mile and half east of Iago was Snow Shed No. 15, a 360' wood structure, that was hit by a massive avalanche on January 25, 1951. It tore out 170' of the central portion of the shed leaving only the two wood structure ends. The company decided to replace the destroyed portion with a reinforced concrete structure which still stands today, decades after abandonment of the KVR, along the well used cycling rail trail. A bit more ground cover needs to be added to complete the scene.*



*The track to the left leads to a shelf addition that allowed for the modelling of a favourite, and unique, lake rail service in the Slocan Lake region east of KVR territory. The CPR had sternwheeler, tug and barge operations on lakes in the BC interior and the Slocan Lake service lasted until 1988. From the south end of the lake, the whole train was loaded onto a two track barge and transported nineteen miles north to be offloaded at the slip at Rosebery, where it would continue by rail to Nakusp.*

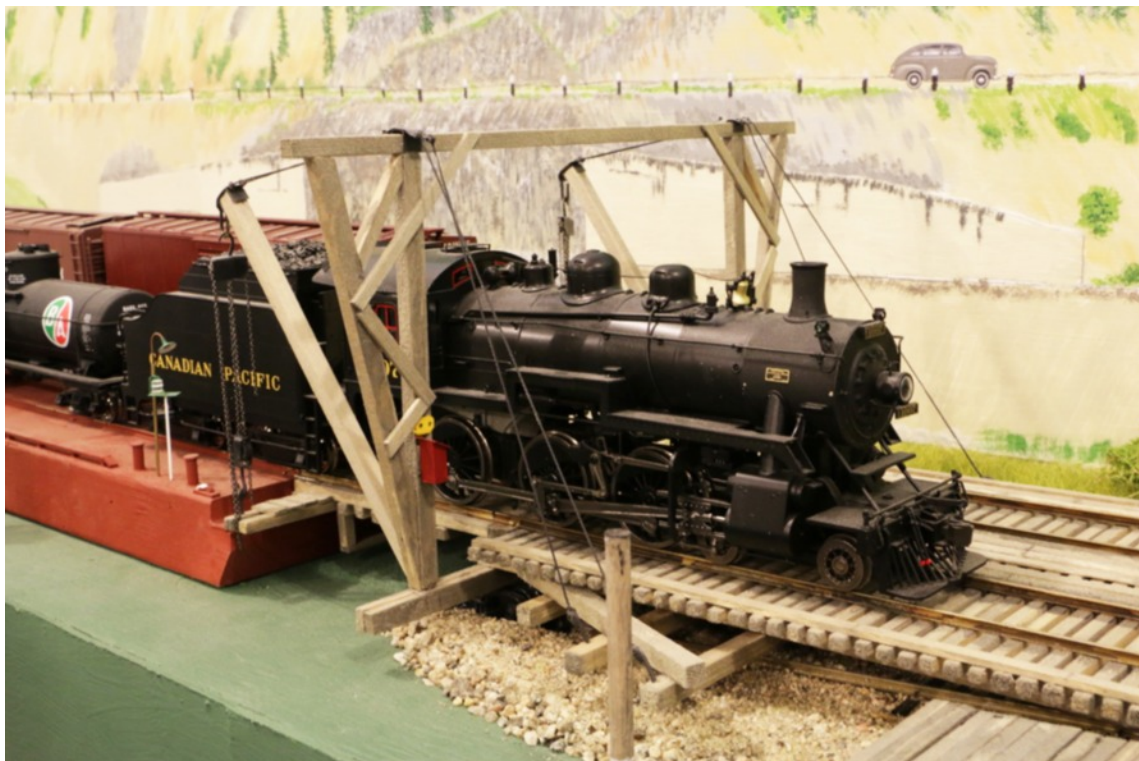
*The ruggedness of the KVR route through the Coquihalla was the inspiration for this scene of a pair of deck girder bridges crossing a fast flowing glacier fed creek. The upper one was inspired by the Ladner Creek bridge two and a half railway miles west of Portia. That span of 560' and a height of 225' dwarfed our representation. The bridge is built from styrene sheet and dimensional strips including all of the cross braces which were individually applied piece by piece. The footings are all at different heights which improved the appearance but added to the structural challenge on installation. The lower bridge deck is supported by carved Styrofoam piers. It was originally intended to be a wood trestle, but there was not adequate clearance to accommodate the width of the tall trestle bents at their base.*



*The Rosebery station building is shown with a pair of flanged wheeled vehicles that were used by the CPR in the Kootenay region. The one on the left is M600 which was assigned to passenger and light freight service on the 36 mile Lardeau Sub. at the north end of Kootenay Lake. The details for modelling the black sedan No. 620 were obtained from a photo of the original taken at Nakusp while it was still in service.*



*Since the layout is set in 1952, the lake service needed to be accurate for the time period; thus it was necessary to scratch build the steam passenger tug that performed those duties since its operating life lasted until 1956. The steam passenger tug, the SS Rosebery seen here, lasted until 1956 when it was replaced by the diesel tug Iris G. The lake surface has yet to be applied to this scene showing the SS Rosebery with the 220' steel barge connected to the moveable slip thus allowing the D10 4-6-0 No. 1007 to commence its offloading. Below the vehicle on the backdrop are two concrete retaining walls that protect the trackage along the shoreline that led northward to Nakusp.*





*A westbound freight prepares to depart Midway as Train No. 12 has cleared the main. Engine No. 5249 is a P1n class Mikado and was one of several Consolidations fully rebuilt into the 2-8-2 wheel arrangement. Consolidations and Mikados comprised the bulk the KVR's motive power to the end of steam. The layout is based on how things were in 1952, just as the first diesels were making their appearance. Steam power was in its last months, and by late 1953, the CPR's dieselization of the southern mainline was complete.*

*Extra 5249 is seen alongside the Goldwater River as it passes the mile board for Merritt.*

*The interior of British Columbia is in the rain shadow of the Coastal Range of mountains and summers are usually hot and dry. It is ranching country, and one of North America's largest ranches is located on a branch line nearby.*

*Coal mines and lumbering also contributed to the local economy. The coal, however, was of poor quality and locomotive crews were happy when the CPR changed over to fuel oil along the KVR in the late 1940's.*

## THE KETTLE VALLEY RAILWAY





*Train No. 41 is arriving at Brookmere with a three wood coach consist headed by G5a light Pacific No. 1200.*

*The facilities at Brookmere were originally jointly shared by Canadian Pacific and Great Northern, but the GN use was short lived. The tracks to the left of the station were to be for use by the GN, and the yard tracks and engine service leads to the right were KVR/CPR*



*property. Thus the enclosed water tank was equipped with two spouts and remained that way until the end of steam. After the abandonment and lifting of the rails, all that remains today are the water tank and the concrete walls of the turntable pit.*

## **THE KETTLE VALLEY RAILWAY**

*Riding the turntable is N2a Consolidation No. 3629 awaiting its next assignment while D4g Ten Wheeler No. 479 performs switching duties. Brookmere was a divisional point at the east end of the rugged Coquihalla, which had some of the most rugged mountain railroading anywhere with numerous bridges, tunnels and snowsheds. Heavy winter snows and avalanches often resulted in closure of the Coquihalla line and trains would be detoured at the junction of Brodie, four miles west of Brookmere, to the main line at Spence's Bridge via Merritt.*



*A local wayfreight with N2 class 2-8-0 No. 3721, a vestibule cabbed oil burner, waits on the siding for the through freight to pass before commencing its switching. The enclosed water tank was situated inside the wye alongside the section house. The Nicola branch, which served coal mines and ranches in the area, was one leg of the wye and the other was located on the rear of the station and served the freight shed.*

## THE KETTLE VALLEY RAILWAY

*Extra 5249 is about to cross Merritt's Quinchena Avenue and pass the station. The spur to the left serves both the Texaco and Shell bulk oil dealerships that were surprisingly located side by side. A hardware store existed in a 1938 photo very similar to the Woodland Scenic one shown. It is the only non scratch built structure on the layout.*



*Engine No. 5249 has pulled alongside the oil service tank so as to be ready for her next assignment.*



*This elevated view of Brookmere shows the two spout enclosed water tank and the engineman's bunkhouse to the left. The silver ball on the top of the tank indicates the tank is almost at its capacity of 40,000 gals. Beyond the bunkhouse are the Bunker C oil facilities installed around 1949. They include a storage tank, elevated locomotive service tank and a coal fired boiler house to keep the Bunker C liquid.*

*The positioning of the turntable and four stall wooden roundhouse required "modeller's license", Their true positions were to the left of the station which could not be accommodated due to width and reach. It is worth pointing out that the treed areas bordering the right edge of the Brookmere yard and the lower level Midway yard, required the construction of seven lift out sections of scenery made from Styrofoam, chicken wire and plaster. Two of these sections have been removed on several occasions in order to access the most inaccessible turnouts on the layout.*



*Road grading is almost completed along the Nicola Road. From 1/43 scale grader and pickup truck to 1/64 tanker and finally several 1/87 vehicles, this scene of the Nicola Valley covers about seven feet across.*

## THE KETTLE VALLEY RAILWAY

*The bulk oil dealership in the foreground gives way to the station visible in the distance. We have been fortunate to uncover photos taken from the top of the enclosed water tank looking towards the bulk oil tanks, as well as, aerial photos that show the dealership structures and the small house that occupied the area alongside the spur tracks.*

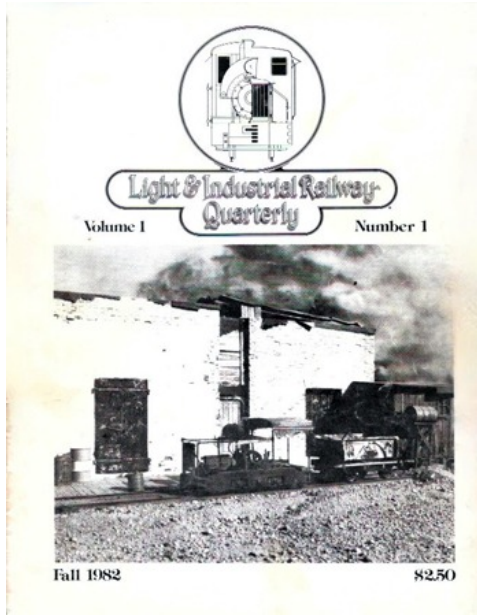


# Building An Open Air Engine Shed Part 1

By William W. Davis

INTRO:

Back in the early 80's Tom Yorke went on a tour of the Cuban sugar cane railroad. In 1982, he started a magazine called "Light And Industrial Railway Quarterly". Sadly, this magazine was short lived, only 4 years. However, it had some of the neatest modeling ideas and plans. I built more models, per capita, from that magazine then any other at that time.



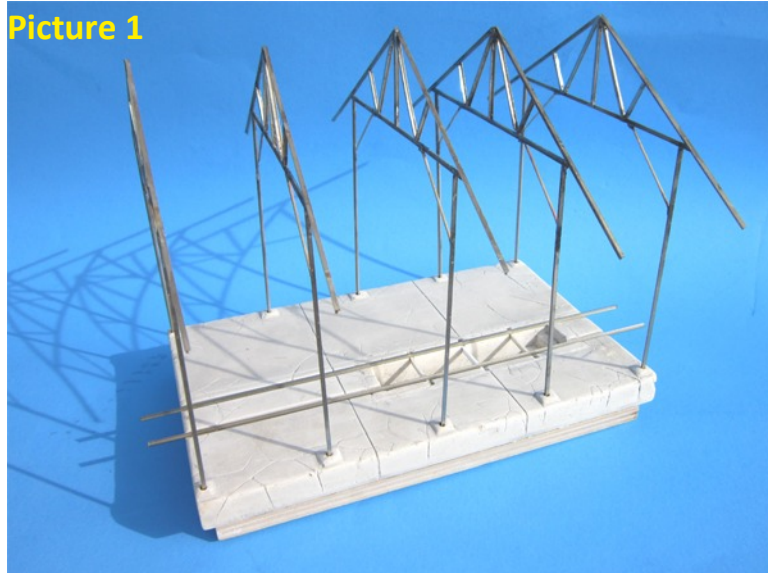
In the very first issue, Tom presented drawings and an article about the Cuban open air engine sheds that he saw on the tour. The concept interested me; and I decided that one day I would build a model of an open air shed. One of the things I really liked was that being open, all the details inside could be easily seen.



In time, Tom produced a kit for an open air shed. I ordered his soldering jig for making the rail structure, the concrete floor, the inspection pit and the cinder block wall castings with the intention of scratch building the rest of the structure. All these parts were plaster castings. I also decided to build it as a diorama. I wanted it to have three points of interest. The open air shed being the main thing, but the area in front and a bone yard of parts, etc. in the back being the other areas. So I started designing a diorama of an open air shed based on my given and drawings along with the pieces I bought from Tom. In this series: I will tell about building this open air shed diorama and the detail areas surrounding it.

## FIRST STEPS:

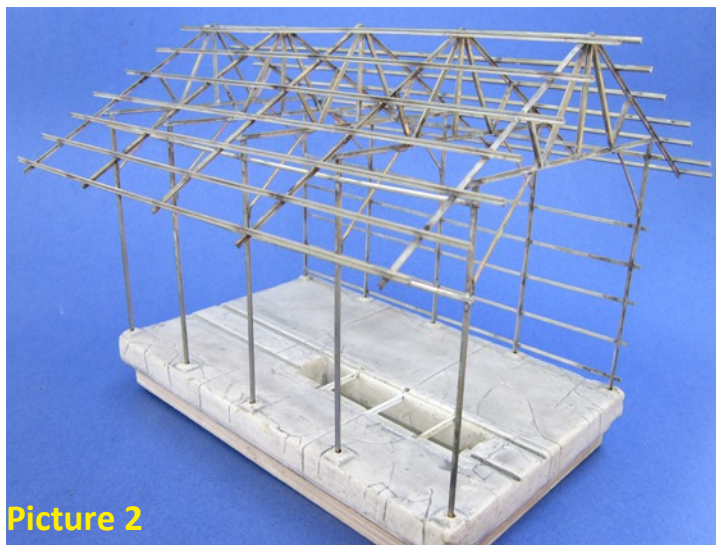
Once I figured out what I wanted to build, I started by building the base. The base is 14-1/4" long x 12-1/2" wide and 1-3/4" high and was made out of 3/4" wood strips. I mitered the corners and added wooden support strips. These strips would support the pieces of Homasote I was going to build the structure on. Next, I figured out the location of concrete floor and cut a hole in the Homasote. Once that was done, I added some contours since ground is never perfectly flat.



Picture 1

Next I started building the basic building. I figured, based on Tom's casting, that the building would be 33' long x 19-1/2' wide x 24' tall to the highest part of the peaked roof. With the roof overhangs it was 36' long x 36' wide. In Tom's article, he mentioned that they built the open air sheds out of old rail. I had a bunch of Code 70 rail that I cut to size and then soldered the upright together using Tom's jig. (Picture 1) Unfortunately, I don't have a picture of the jig.

Once those were done, I stuck them in the concrete base where they were designed to be located but didn't glue them in place. I soldered the rail pieces to support the corrugated roofing. Also, since I planned to have the back wall done in corrugated sheets, I soldered rails to support that wall's corrugated pieces. I soldered a rail about 4' from the top. This gave the rail structure stability and strength. Finally, I added sway pieces of rail to the ends and wire sways in an x pattern on the side not shown in the pictures 2 and 3.



Picture 2

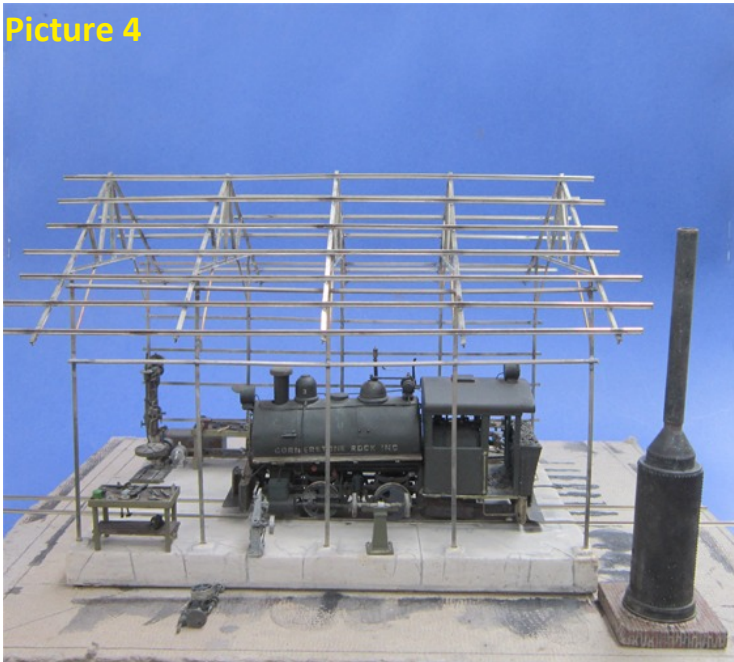


Picture 3

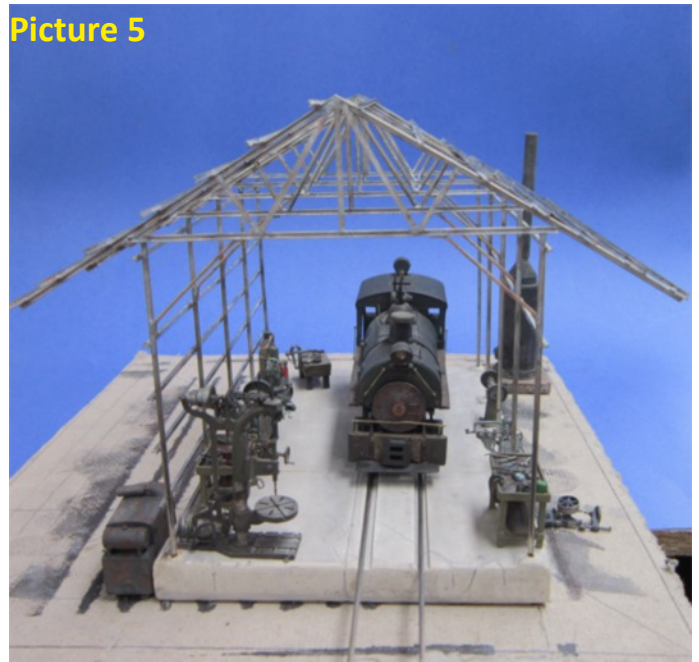
## LAYING OUT THE EQUIPMENT:

Now that I had the basics of the structure done, it was time to figure out the best layout for the machinery and other equipment I planned to use for the diorama. Over the years I have collected machine shop machinery kits by CHB and Western Scale Co. I wanted to have several pieces of machine shop type machinery in the open air shed. However, I didn't want a complete machine shop, just enough equipment to do basic repairs needed to keep things running on the railroad. Anything major would have to be sent out for repair. I decided I would have a lathe, a drill press, a grinder, a powered hack saw, a post drill and a stationary boiler. I laid these

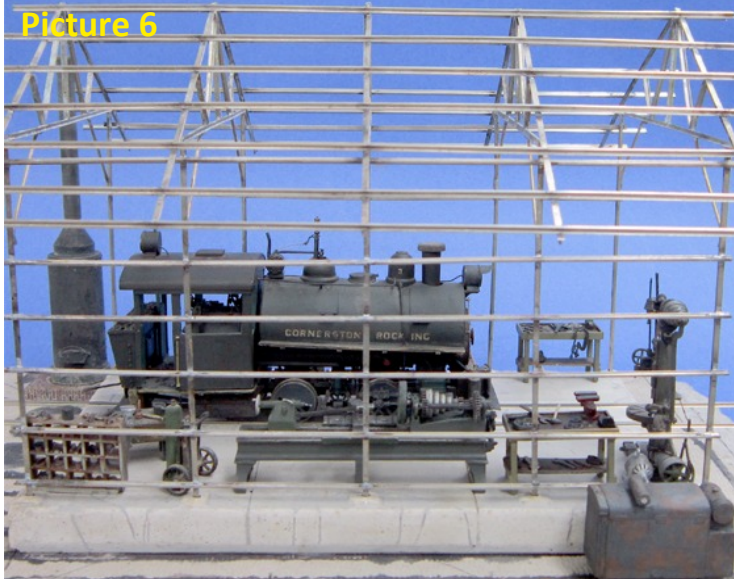
Picture 4



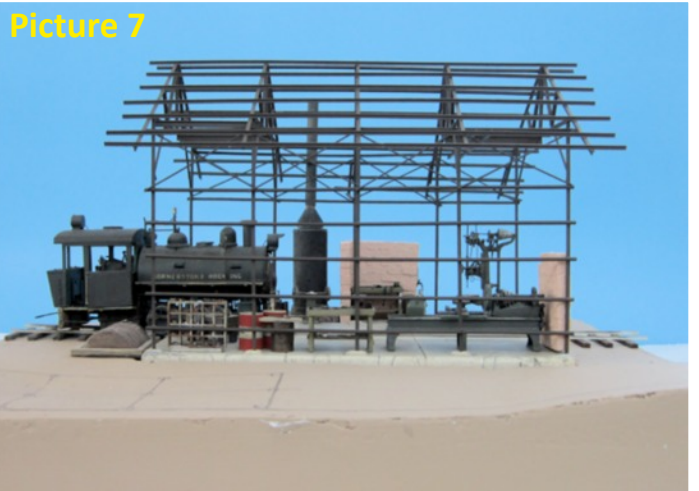
Picture 5



Picture 6



Picture 7



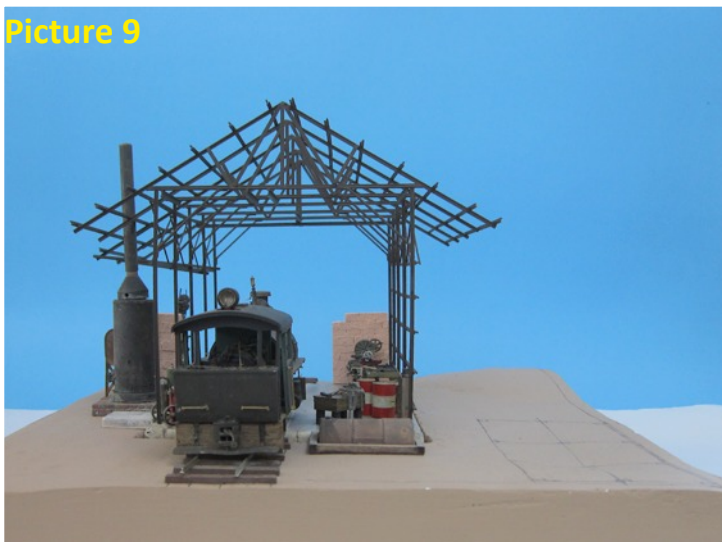
Picture 8



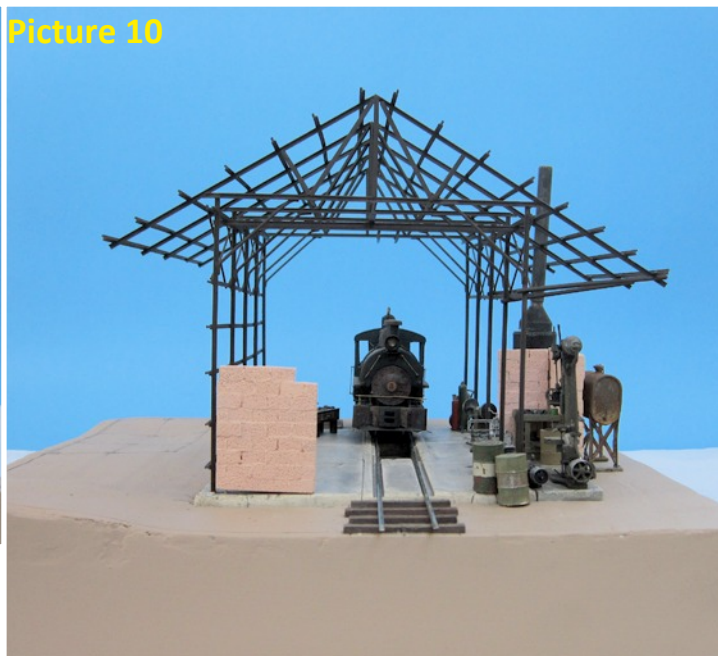
tools out in an arrangement that I thought would look and work best. (Pictures 4, 5 and 6)

I then sent pictures to my circle of friends asking what they thought of the equipment layout expecting to get “wow, you nailed it”. What I got was “it would be better off if you did it this way”. Now I like to get other opinions on a project of this size because it can be good to see things from another’s perspective. This proved very true, and in this case better than what I originally had in mind. I took a combination of their suggestions and redesigned my equipment layout. I also cut the cinder block wall section so that they could be arranged differently than Tom had designed them to be used. He used them for the back wall, but since my back wall would be corrugated sheet, I would use the cinder block wall casting in different locations. I painted the cinder block wall sections a pinkish color which I felt would be a good contrast to the other colors on the diorama. Plus, I have seen cinder blocks that color. Now I have to admit that because of my friends’ input, the layout of equipment, etc. was now a much better arrangement as you can see in in the pictures . (Pictures 7 - 11)

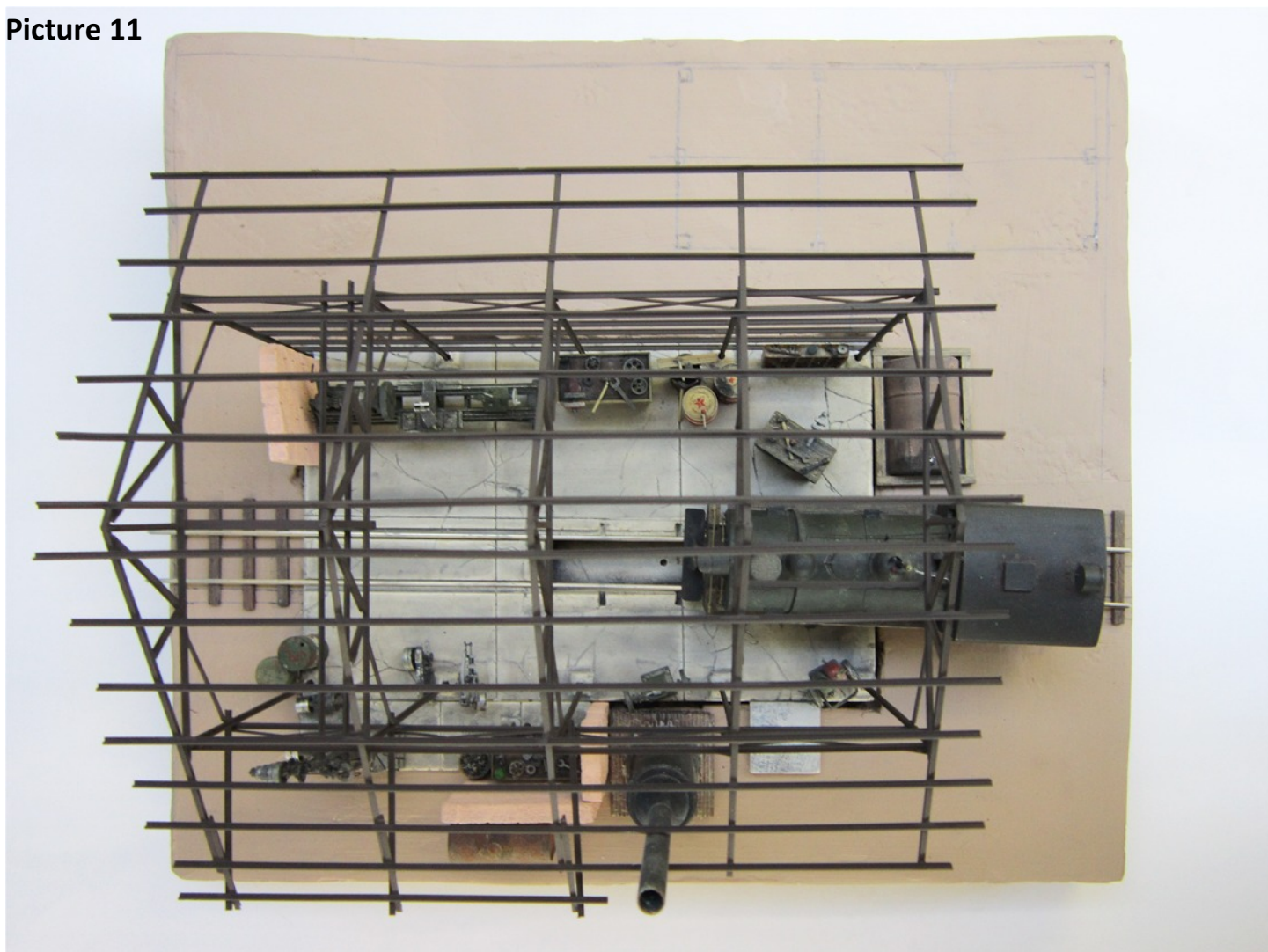
Picture 9



Picture 10



Picture 11

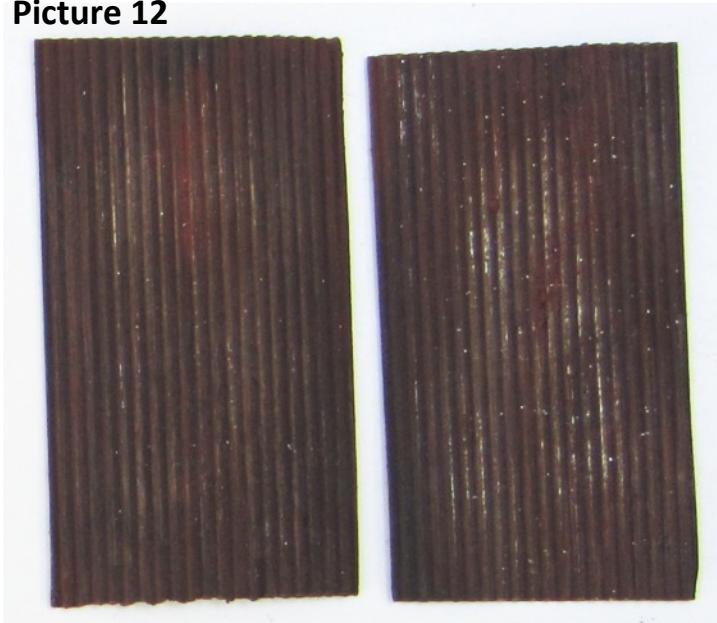


## CORRUGATED SIDING:

You might have noticed in the pictures that the rail structure has been painted a rusty brown color. Also, the wires sways are now in place. If I remember correctly, I used Floquil rail brown to paint the rail structure. Since I wanted this to look very industrial, I decided to have a lot of corrugated sheet material on the structure. I did the back wall of the shed, the roof, the upper front wall area and the back fence in corrugated sheet. More about these as this series progresses.

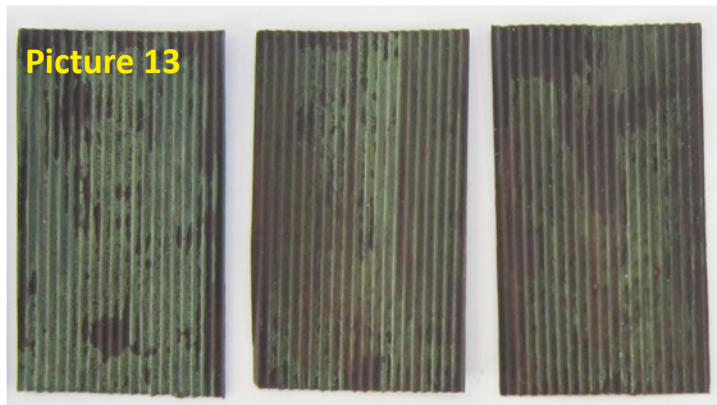
The first step was to make metal pieces by cutting up Coke cans into a size that was close to the size I needed for corrugated pieces in the model. Then my friend, Ted Edgell, lent me his machine that he uses to turn out corrugated material. It was originally used to make filters, but now it was just right for making O scale corrugated material.

**Picture 12**



I tend to like use green on a structures. So most of the corrugated pieces received a green color on one side and weathered rusty brown on the other. Here is how I painted and weathered these pieces of corrugated sheet. First I primed the pieces with a cheap gray primer that I bought at Walmart. This primer was really great and very cheap. It worked well on models because it wasn't thick and didn't cover over details. Sadly, it is no longer available. In hindsight, I wish had bought a case. The closest thing to it today is Tamiya's fine gray primer. Once it dried, I started the rusting process. To get the rust tone I wanted, I used colors from a very old stick chalk set. I ground the sticks to powder and then applied the colors using alcohol as a carrier. I used various colors of browns, redish browns, oranges and black chalks; then dried the pieces with an old hair dryer. Between each color I used a stiff brush to I remove some of the color before adding the next color. I continued until I was happy with the rust effect on one side of the pieces. I then sprayed then with Dullcote and let then set overnight. Next, I applied the rust effect on the other side using the same process. The pieces on the front wall, the fence and some of the pieces on the roof would get a coat of green on one side. I used Folk Art "Old Ivy Green" that I dry brushed on. However, I wasn't happy so I dabbed on more color with a stiff brush leaving areas of rust showing through. I then gave it a wash of alcohol which caused some of the green to discolor but not all of it. When satisfied, I sprayed it with Dullcote on this side like I did on the other side and let it dry.

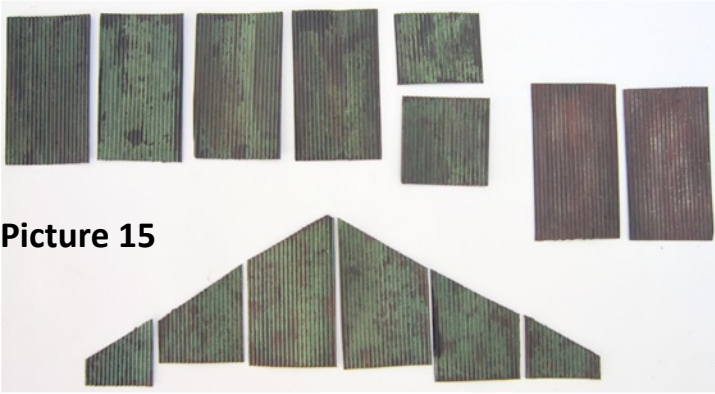
**Picture 13**



**Picture 14**



One thing I learned the hard way is you can't go back with alcohol once you spray a piece with Dullcote. Why? Because the piece will turn a chalky white color and you will lose the effect you worked so hard to achieve. Fortunately, I learned this on an earlier project. (Pictures 12-15)



Picture 15

When I finished painting the corrugated pieces, I glued them to the back wall and to the front of the rail frame. However, I didn't do the roof at this time because there was a lot of detail to be done inside, and it would be easier without the roofing in place. (Pictures 16-20)



Picture 16



Picture 17

Picture 18



Picture 19

**Picture 20**



### PARTS SHED / PARTS BIN

In the bone yard area I wanted a small three sided shed with the front side open. The shed is 17' long x 8' wide and 10-3/4' high at the highest point. I wanted it to be white; and I wanted the paint to have a heavily peeling paint look. I built the shed walls board by board and made to roof same way. I wanted it removable to better show the interior. I framed the walls with 2 x 4s and 4 x 4s and made the header across the front out of a piece of 4 x 12. Most of the siding on the structure was done with scale 1 x 12 pieces. However, some were smaller. The removable roof was framed the same way as the walls. I covered the roof with some red weathered tarpaper roofing I had in my collection of roofing materials. Since the walls were to be weathered beaten, I figured the roof should be the same way.

OK ,so how did I get the heavily peeling paint effect? Over the years I have used several methods for doing this effect. Some were very effective and realistic, while others not so much. For example, I don't like using the rubber cement method on wooden walls because paint peels by the individual board and not in blotches over several boards. When doing board by board, I prefer to do each board separately before assembling the wall. If I am working with sheet material instead of individual board, I dry brush the color by dabbing the paint on the individual board pattern on the sheet. This can be time consuming, but I feel it is worth the effort.

Let's get back to how I did this shed's coloring. Step one: I used washes of brown and black stains to weather the wood pieces. I am not a big fan of all weathered wood being a driftwood gray. In reality, wood weathers differently depending on where it is at and what type of wood it is. I usually prefer a brownish color. Once dry, I assembled the framing but didn't apply the siding yet. I want the siding pieces and the roof fascia to be painted with the peeling white paint before assembly. So now that the siding wood pieces are a weathered brownish wood tone, I started to do the peeling paint effect. Step one was to give all the siding and fascia pieces a coat of Folk Art #694 crackle medium. Once this dried, I found there was crackle paint in various a colors available from Distress Paints. I got a bottle of their white.

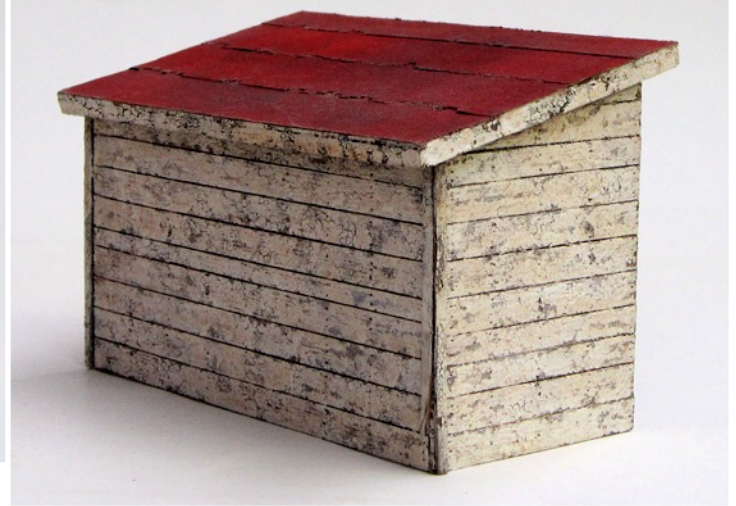
**Picture 21**



Picture 22



Picture 23



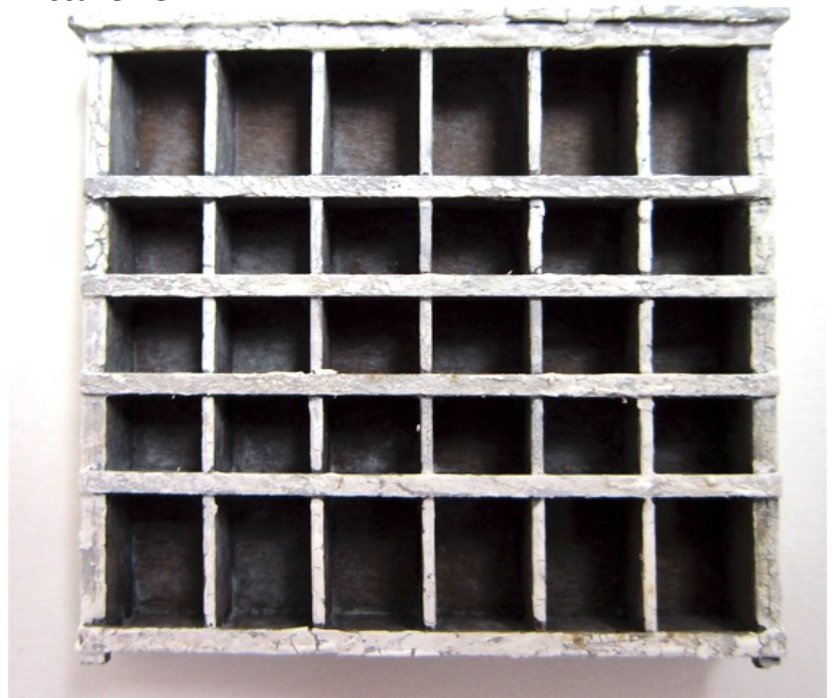
I brushed it over the crackle medium which cause it to created a very heavy peeling paint effect. I then gave the walls and fascia a light wash with a light brown for a dirty look and a wash of white to vary the tone of the white color. Once finished, I assembled the siding over the framework. The result was an old, desperately in need of repainting, shed. We will discuss the front, the base and the interior in the part two. (Pictures 21-23)

I used the same basic technique on the parts bin except I made the interior bin a darker more blackish color. Once finished and assembled, I loaded the bins and top with rusty parts and set this and the shed a side to be installed on the diorama later. (Pictures 24-26)

Picture 24



Picture 25

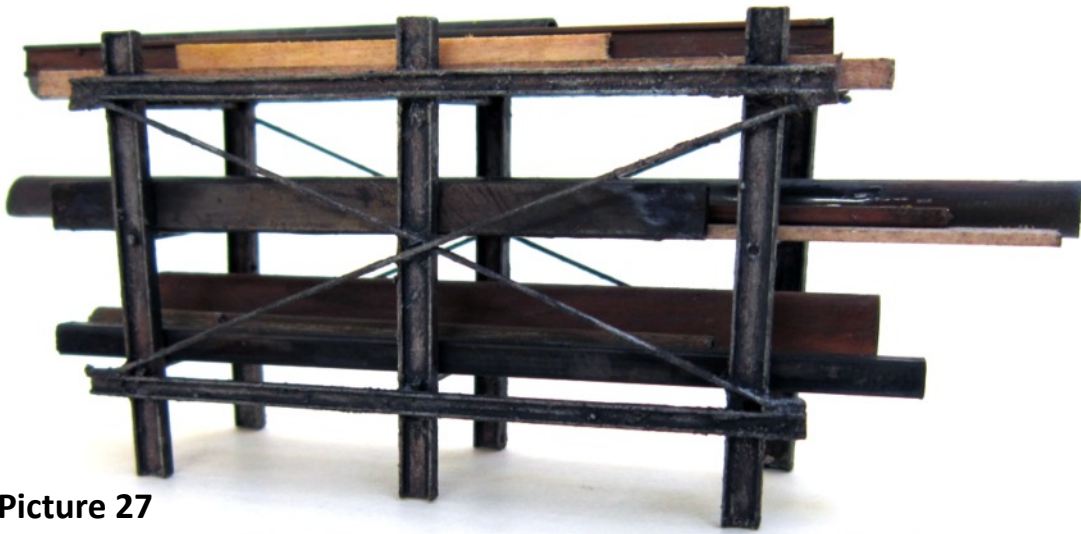


Picture 26



**PIPE RACK:**

The last part we will discuss in this part is the pipe rack. This was a rather simple detail to build, but one that really catches the eye. The size is overall height of 5' x an overall width of 4' and a length of 10'. I built this using styrene I beam and strips and wire for sway bracing and the shelf. I designed it to have 3 shelves. I painted it a weathered black color and highlighted it with a metallic steel color. I loaded it with pieces of pipe, wire and wood strips. **(Pictures 27-28 Next Page)**



**Picture 27**

Next time we will discuss more of the construction, details and how they will be located on the diorama. Until then next time Happy Modeling!

**Picture 28**



# C & NW System Standard 44-foot Water Tank circa 1948 - Part 1

By Greg Viggiano



Photo by Volker Schmidt

## Introduction and Background

This project started in the beginning of 2023 as a casual 3D design and resin printing experiment. I wanted to start learning basic CAD skills and how to 3D print simple parts.

Project-based learning has always been easier for me as a way to experiment with developing new skills like photo etching, CAD, and 3D printing - and being able to make the parts that I didn't want to scratch build, ie. the roof under-structure, the spout, and the spout support frame. This made the build a lot easier, faster, and more accurate.



Other parts such as the tank (which matched the dimensions of a 102 oz commercial size Isabella-brand tomato sauce can, luckily found at Restaurant Depot) were made the old fashioned way.

What follows is part one of this build and how I went about it using a variety of old and new construction techniques, materials, and finishing methods. I found a really nice set of blueprints for the C & NW System Standard 44-foot Water Tank that Dan Dawdy had available on the Ribbon Rail Productions Cyberspace World Railroad website: <http://cwrr.com/Drawings/cnw-16070/cnw-16070-5-2.shtml>.

Painting, finishing, and weathering will be covered in Part 2.

I would like to acknowledge that the shingles were laser-cut with assistance from Bernie Kempinski and Gene Berger provided a lot help with the photo etch work. Gene's comprehensive photo etch tutorial can be reviewed here: <https://geneberger-models.com/photochemical-milling/>

The objectives for this project included using 3D printed detail parts, CAD drawings, photo etched parts, and an Arduino remote control system.

Anyone that has ever scratch-built even the simplest of railroad structures knows the importance of precision and authenticity. Precision usually means working with correct tools for the job; and authenticity involves finding the right materials for the project.

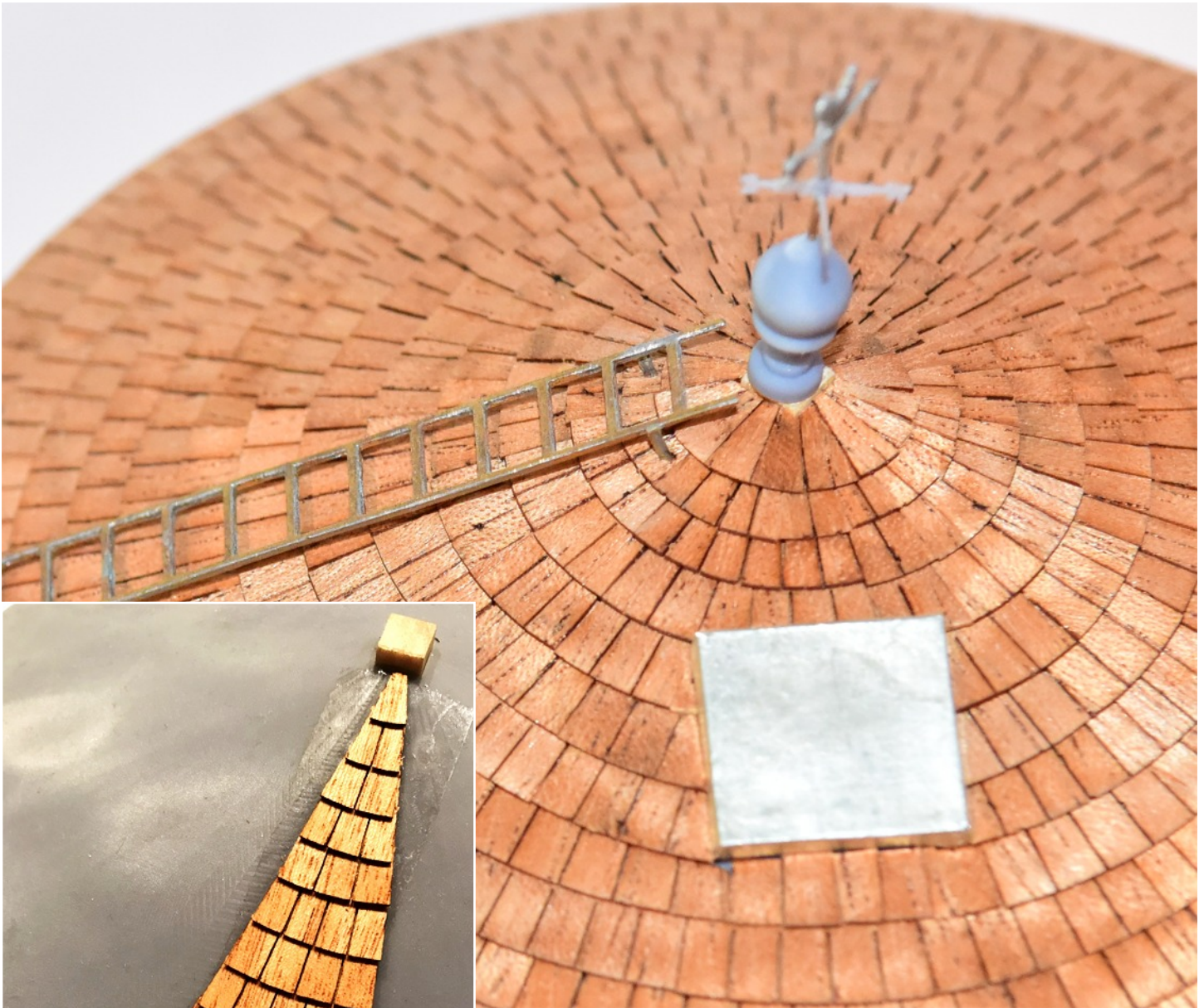
Knowing where to make the right trade-offs and compromises greatly affects the overall build quality and final outcome of the project. Starting with the original construction documents for the water tower, it was easy to see what needed to be built with wood and what parts should be 3D printed.



*Ground-up build process beginning with Hydrocal footers reinforced with yarn.*

## Shingles and Going With Grain

The roof shingles needed to be dimensionally accurate, pie-shaped segments to maintain the right grain direction from the center of the roof to the outer edge. The shingle rows closer to the center needed to have much sharper angles. Using CAD to make this pattern made this part of the build a lot easier and much more precise.



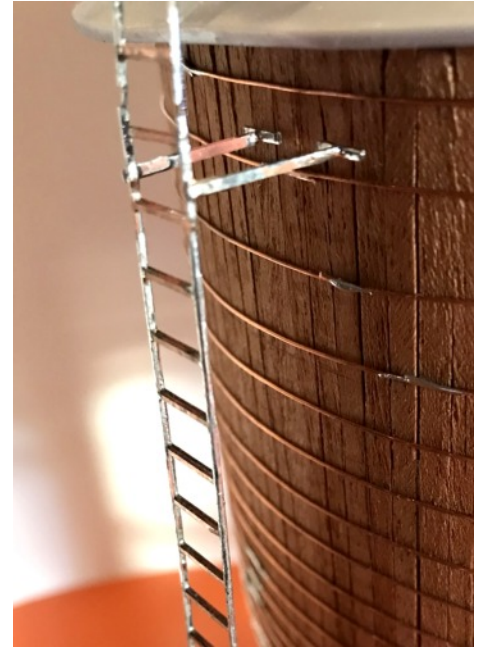
Wood veneer for the tank provided good material authenticity. Laser-cut cedar veneer (thickness .025") grain aligned pie-piece shingle slices. Champagne bottle foil for the roof hatch and spout cladding.



### The Devil is in the Detail Parts

Another experiment was to see just how small I could 3D print a detail part. The weather vane directional seemed to be a good candidate, and it actually came out pretty well.

Not being able to find a suitable ladder in the right scale or length, I elected to create a two-layer photo etched part and soldered the layers together for more structural strength. The solder flash was easily filed off to make the edges perfectly straight.



### The Spout Frame

The prospect of gluing very tiny pieces of wood together with accurate dimensionality and proper appearance seemed more tedious than necessary for this project. By designing the spout frame in Solidworks, I was able to get a perfectly exact add-on part after it was 3D printed.



As Solidworks is a CAD tool and it really doesn't allow for sculpting surface texture, ie. wood grain. Sculpting wood grain would require another process using a mesh editing tool, like Blender. Blender is a free, open-source 3D software suite used for modeling, sculpting, animation, rendering, and creating visual effects for films, games, and art. Blender can also be used for 3D printing models. Using Blender would be a good topic for a separate article or perhaps a sidebar in part 2 of this article.

### The Magic Spout

The remote controlled spout mechanism uses an Arduino Uno board and potentiometer. The design challenge came from some comments made by Tony Koester during his Zoom presentation on the February 2025 O Scale Central monthly call. <https://www.youtube.com/@oscalecentral1757/videos>

Basically, he said, "what's the point of stopping at a water tower if you can't lower the spout?" As an operator, I couldn't agree more and thereby the design challenge began.

After thinking more about Tony's comment about an operational water tower, I spent a lot of time thinking about how to engineer a mechanism to raise and lower the water spout without any visible moving parts.

I thought I had it figured out using shape memory alloy wire, but like a lot of good ideas, the theory could not be put into practice and wasn't really suitable to the physical constraints of the model.

What I needed was a completely hidden actuator and pipe movement mechanism. The solution would need a fairly sophisticated electromechanical engineering design.

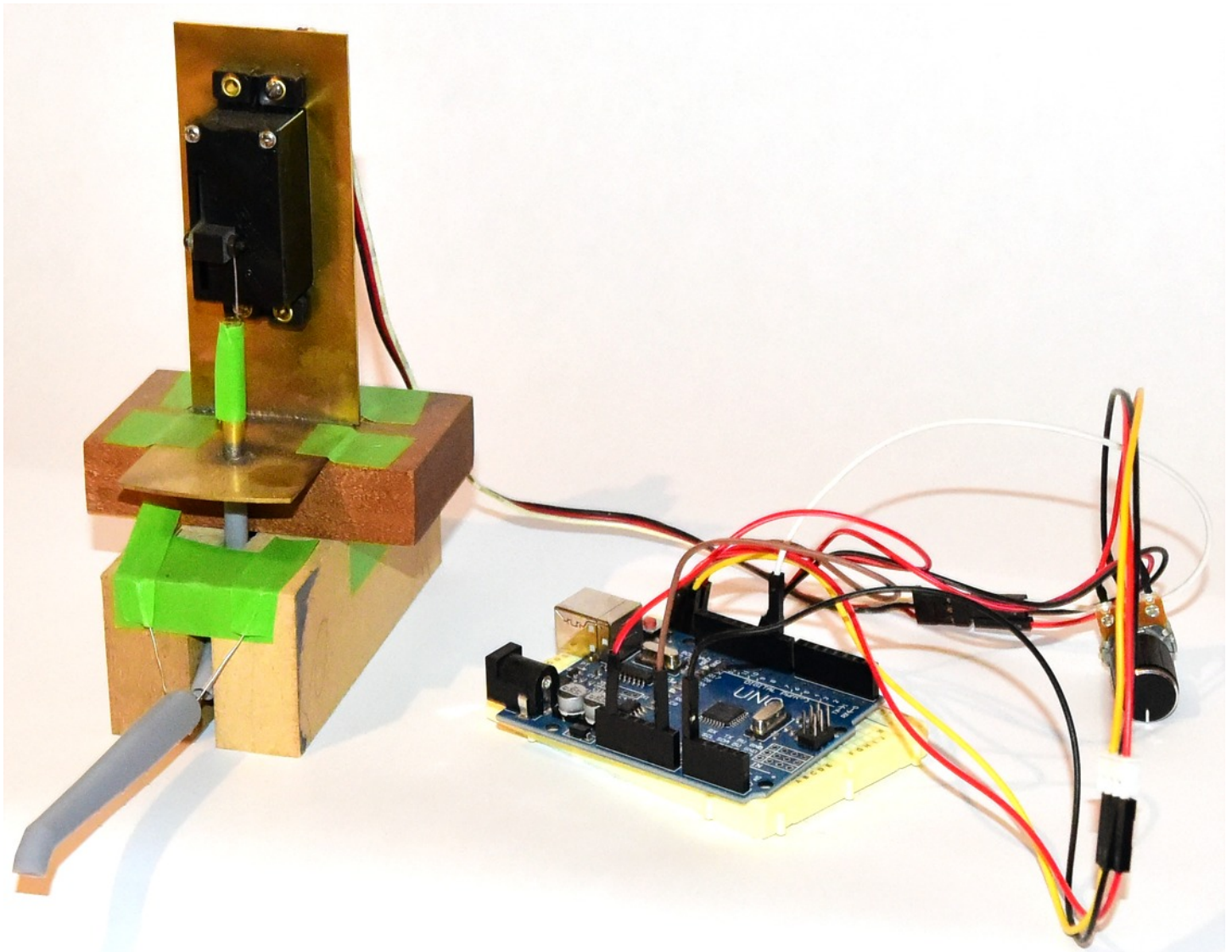
However, using an internal spring wire had some possibilities. I liked the mechanical simplicity of this approach, so I forged on to try a few other Rube Goldberg-type ideas.

Using the same spring wire approach with a guitar string and a gentle 90° bend, it occurred to me that if the wire is normally bent in the up/vertical spout position, it will become flat/horizontal when the bent wire is pulled flat through a horizontal tube. This test bed approach actually worked (with repeatable and reliable results) and might even be good for dozens of cycles. The wire in the spout pipe requires an internal tube so the wire can freely slip in and out – similar to push-pull sheathed cables that the RC model plane community has been for decades.

Using a spring wire that can maintain a soft gentle 90° bend is the key to getting the spout to go back to its raised position.

The next step was to figure out a robust control mechanism to move the wire in and out of the tube. This was accomplished by using a simple linear servo connected to an Arduino Uno circuit board with a rotary potentiometer to “dial” the spout up and down.

So, when the internal tank-mounted linear servo pulls the music wire through a tube, it is pulled flat and the hinged spout lowers and becomes level as the internal wire matches the line of the tube.

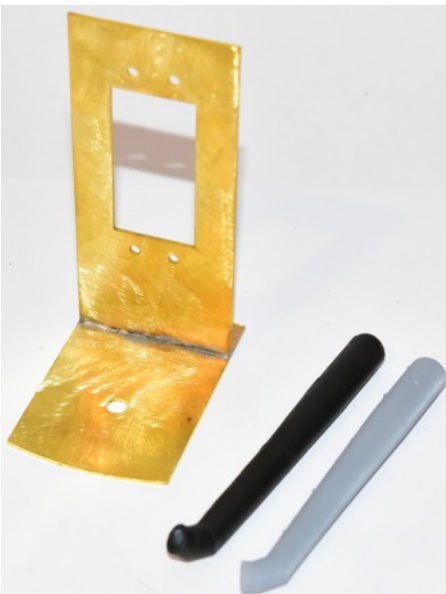


*The servo is controlled by an Arduino Uno board and a potentiometer so you can raise and lower the spout as slow as you like.*

To be fancy, the Arduino servo control could be connected to a DCC system so the engineer could raise or lower the spout from his handheld throttle, much in the way you could do switch operations with a handheld device.

If you plan to use an Arduino to control the spout, the code is very simple and is shown below:

```
#include <Servo.h>
Servo Servo1;
int servoPin = 9;
int potPin = A0;
void setup() {
  Servo1.attach(servoPin);
}
void loop() {
  int reading = analogRead(potPin);
  int angle = map(reading, 0, 1023, 0, 180);
  Servo1.write(angle);
}
```



*Laser-cut brass servo frame*

Note: a simple rotary servo could also be used if it is set up with a control horn connected to the cable.

I recommend laser cutting the servo bracket so the fit and durability is precise for maximum performance and ease of installation as a unit.

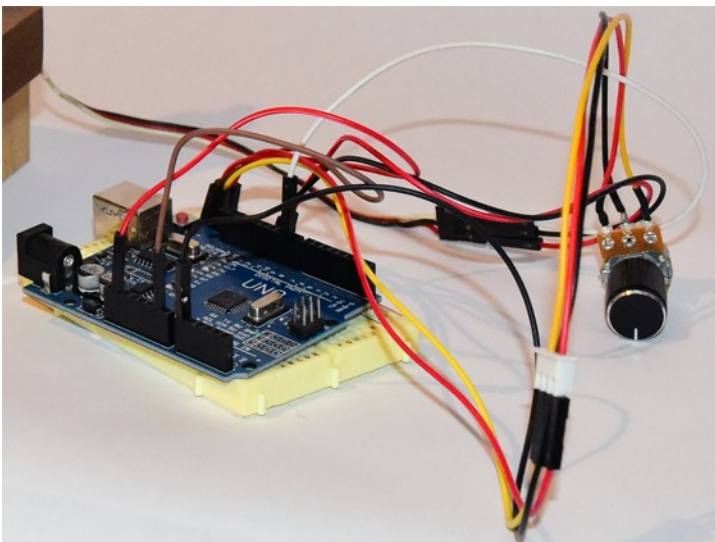
At this point, I think the mechanism is robust enough to do hundreds of cycles before it needs some maintenance and re-calibration. I'm pretty sure that given a little more time I could figure out how to make it brew a cup of coffee.

### **Final Thoughts**

The quality of model railroading construction keeps getting better with each new innovation. Over the last 25 years or so, we've seen new innovations such as DCC, Arduino, 3D design software, 3D printing, and laser cutting make notable contributions to the hobby. As we look over the horizon, what might AI bring us?

How long will it be before we can just send some pictures of a truck or a locomotive to a website and have an AI design system send back in 45 seconds a super-accurate 3D model ready for printing? or ask if we want it printed?

Stay tuned, the future is arriving on platform 42.



*Assembly remote control*

**Sidebar: Historical background on the prototype – the Redwood Water Tank in Lusk, Wyoming.**

The Redwood Water Tank in Lusk, Wyoming, a structure built in 1886 by the Wyoming Central Railway to provide water for steam locomotives, this tower still stands as a historical landmark alongside the railroad tracks. The structure is very similar to blueprints on the Ribbon Rail Productions Cyperspace World Railway website <http://cwrr.com>.

It is one of only six such redwood water tanks remaining in the United States, and the last one in Wyoming. The Niobrara County Historic Society and Fancy Farmers Garden Club erected a historical marker to preserve the tank's history. The tank was initially filled by a windmill and later by other types of pumps.

The tank is made of redwood and is approximately 25 feet high on a 25-foot base.

It is the only surviving structure of its kind in Wyoming and was placed on the National Register of Historic Places in 1991.

The Sioux City & Pacific Railroad surveyed the route from Chadron, Nebraska to Fort Fetterman



Photo by Volker Schmidt

Wyoming Territory in 1883. They organized the Wyoming Central Railway Wyo. Corp. as required by law to build or own a railroad in the territory in 1886. As the railroad and facilities were constructed, they were leased to the Fremont, Elkhorn & Missouri Valley Railroad Co. a subsidiary of the S C & P Railroad.

Frank Lusk was a director and the local agent for the Wyoming Central Railway headquartered in Lusk, the only post office on the route. Mr. Lusk was also an agent for the Pioneer Townsite Company that established the locations of depots and platted the townsites of Van Tassell, Node, Lusk, Manville, Keeline, Lost Springs, Shawnee, Orin, Douglas, Glenrock, and Casper.

All of these railroads were consolidated into the Chicago & North Western Railroad by 1903. In 1971, it halted service through Wyoming. In the 1980s, the Union Pacific Railroad, a competitor for over a century obtained this portion of the C & N W as a route for coal trains from the Powder River Basin.

The Niobrara County Historical Society obtained the tank in 1971 and has repaired and restored it with a great deal of community and individual support.

Location. 42° 45.739' N, 104° 26.587' W. The marker is in Lusk, Wyoming, in Niobrara County.

# A Pair of Exacta Wagner Sleeper Cars

By Martin Brechbiel

This is bit of a different project that relates more toward a restoration exercise. While my roster and inventory is heavy on freight cars and MoW (very heavy), it is also quite light on passenger cars. That is primarily due to there really not being a readily available selection of passenger equipment that resides in the pre-1920 era. In O scale, passenger equipment is dominated by the post-1930 time frame with heavyweight metal 6-wheel truck cars. If you go looking for wood cars with 4-wheel truck the choices are quite limited. First off the list might be LaBelle kits and then the list gets very thin indeed with next to nothing actually available in production. I was exceedingly lucky to be the recipient of an Alexander old time coach a number of years ago and would love to find another.

But this article pertains to those cars that were marketed by Crestline under the name “Exacta” that provided a line of at least 8 different wood “1898” coaches, combines, baggage cars, and then the Wagner Sleeper Car. The Wagner Sleeper Car was built by the Wagner Palace Car Company. For more information:

[https://en.wikipedia.org/wiki/Webster\\_Wagner](https://en.wikipedia.org/wiki/Webster_Wagner) and

<https://www.midcontinent.org/rollingstock/builders/wagner.htm>.

Beyond the era which they represent, the Exacta car model kits are of interest as they were initially made from embossed pressed copper alloy. All of the detail in the sides and ends were stamped into the metal giving it a 3D depth. This process of course reciprocated on the interior. Later versions of these kits were made in through the same process but were formed from some sort of plastic, the shift probably attributable to control of war time materials taking precedence over model train kits. At one point I had one of the plastic material Smoker and Baggage cars and now regret its sale, but I digress, which brings us back to the Wagner Sleeper Cars.

What seems like several years (probably more...) I bought a lot of passenger cars off eBay knowing that there were two Wagner Sleeper Cars in the mix along with a 3-4 “others” that I don’t recall now. I do however recall the box arriving in poor shape and that the packing material was wadded up brown paper, and that everything arrived in less than positive condition. The “others” were sent packing off to new homes leaving the Wagner cars. One was in fairly good shape and in fact, the body was rock solid. The other one suffered from a severe case of explosive disassembly. I’ll start with that car.

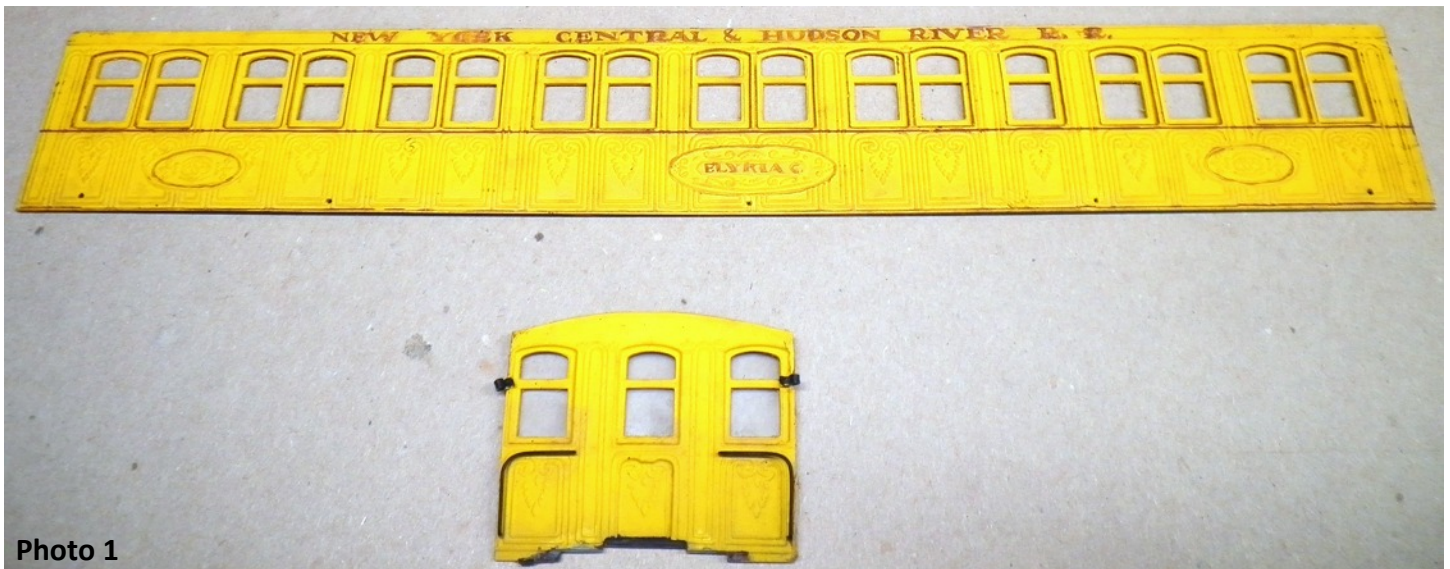


Photo 1

The yellow Exacta car is one of the later cars made of some unknown plastic material. It arrived in pieces with one end and side completely free from the body (**Photo 1 previous page**). One side and end were tenuously attached to the floor and the duck bill roof section was completely loose (**Photos 2, 3**).

Photo 2



Photo 3

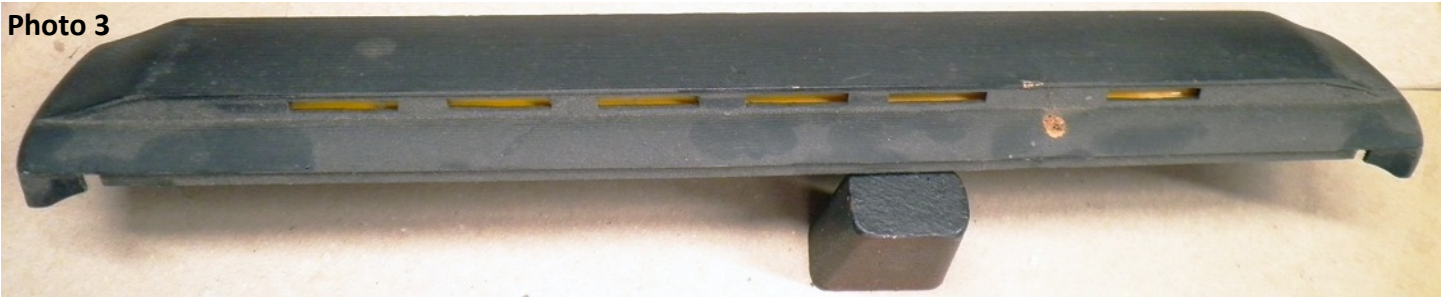
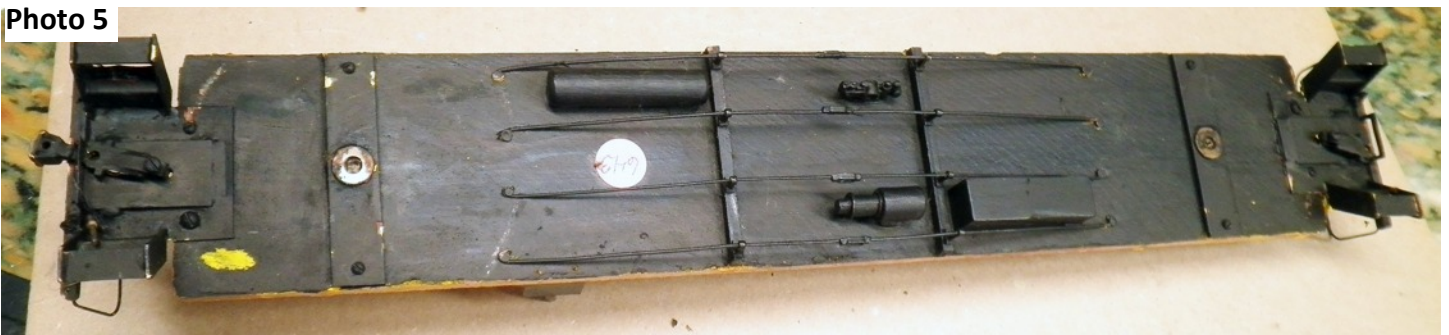


Photo 4



The interior details were largely loose in the shipping box but recovered (**Photo 4**). These were white metal and really far too late in era, so in the interests of lesser weight and something more correct, they were retired to a new home. The underbody was fairly solid albeit sparse, but also sported link & pin couplers (**Photos 5, 6**). One of these coupler units was broken, so there's a decision in the making; replace and restore or modernize a bit to a Kadee or some knuckle coupler that will fit the space. The trucks that were loose in the box that came from this car were actually quite nice given their age (**Photo 7**).

Photo 5



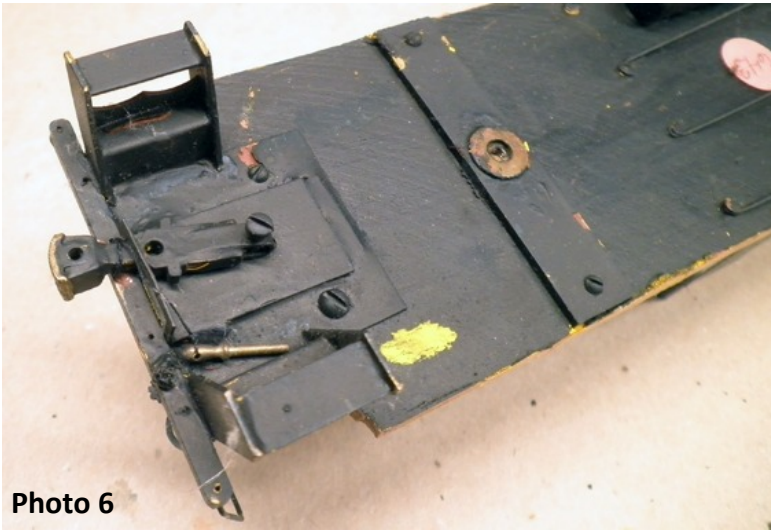


Photo 6

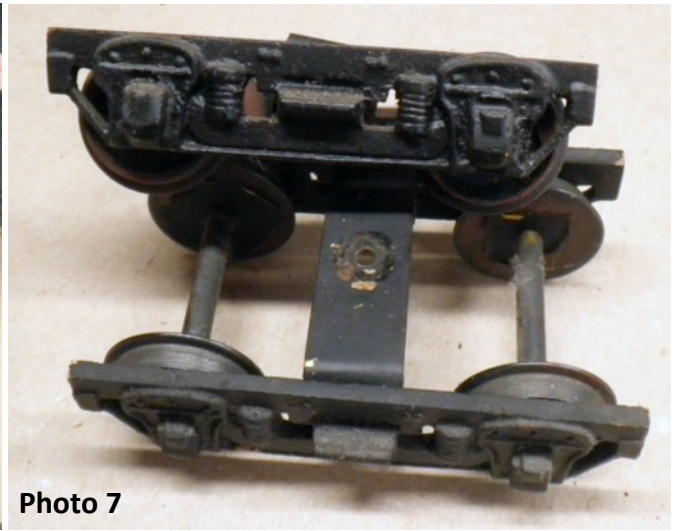


Photo 7



Photo 8

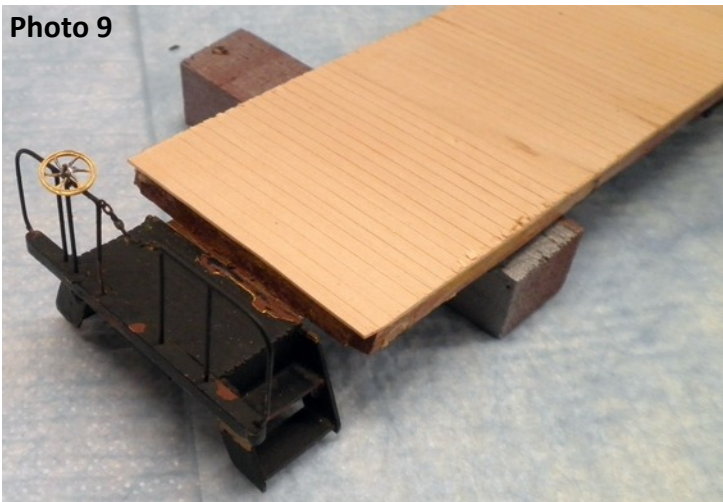


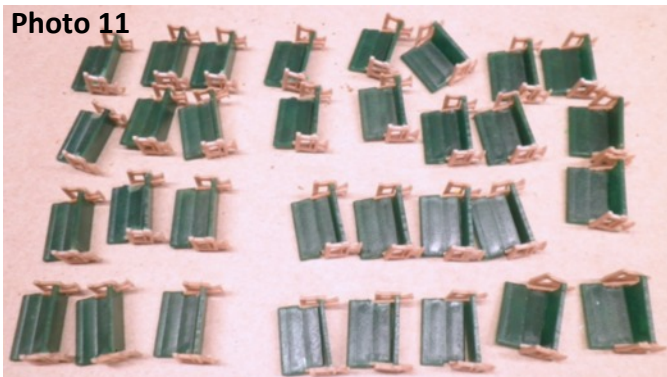
Photo 9

I started working on this car with the floor to create a stable base to rebuild upwards. Any loose joinery attaching the platforms to the floor was tightened up with CA and other glues. The end railings were reset in their respective end sill locations, but new brake wheels were added using brass PSC parts. A new interior floor was installed using 1/32" thick scribed siding (Photos 8, 9). This flooring had to be installed very slightly set back from the edge to accommodate the sides being placed back on and their having a small inner dimension thanks to the embossed exterior detail. This new floor was stained with MinWax American Cherry not having any Mahogany handy (Photo 10).



Photo 10

Photo 11



I decided on adding some more appropriate interior seating. I had several packs of what were a Walther's product of old time coach seats that were later available from Keil-line, and now probably unavailable. These have to be built up from six parts and with great care can be made to operate flipping the seat back for the traveling direction (**Photo 11**). I opted out of that exercise as my passengers are not getting that option in the fare that they pay.

The interior side walls were overlaid with 1/32" scribed siding and stripwood to build up some appearance of a wood car interior with raised paneling between the windows, window sills, and chair railing (**Photo 12**). This is not that hard to do when the car is disassembled! To complete this stage, the wood was stained with the same cherry and the spaces between the wood about the windows were given several coats of Reefer Yellow (**Photo 13**). That was not done to the top since the roof section fits down over the interior of the wall.



Photo 12



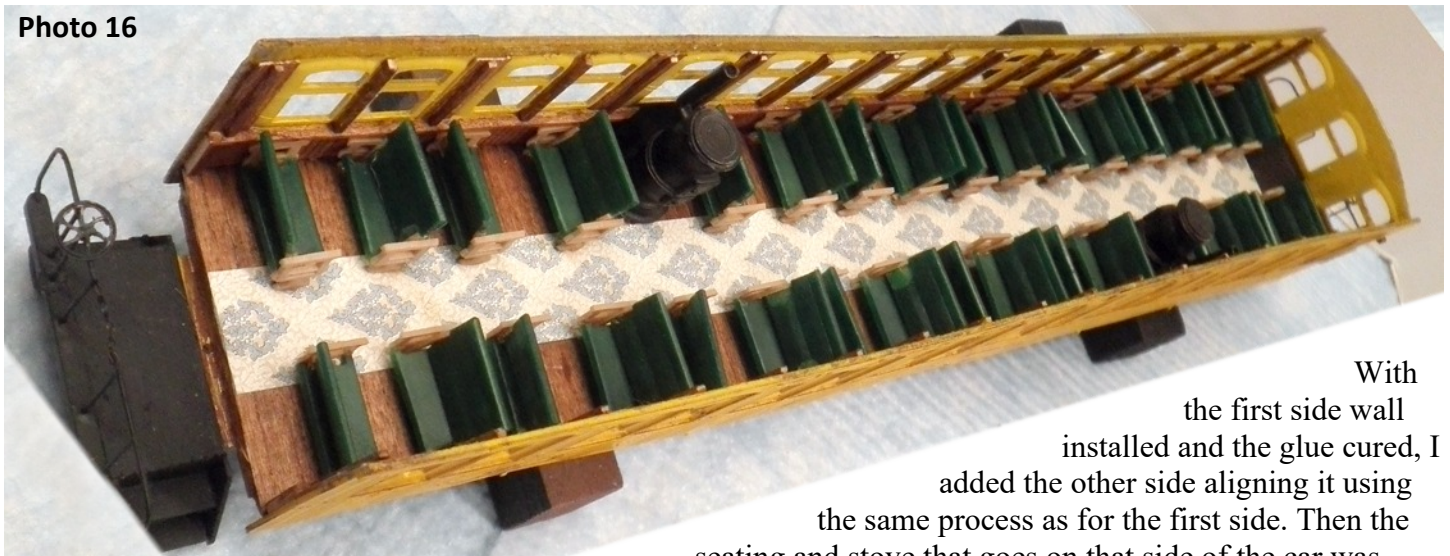
Photo 13

After cleaning, the end platforms received a coat of Engine Black. The new stained floor was further updated with a "carpet" that was in reality doll house wallpaper cut into a strip and glued into place (**Photo 14**).



Photo 14

Reassembling the car was a slow step-wise exercise. One side was re-attached to the side of the floor using Goo and CA. Getting this into exact alignment was facilitated by the fact that the sides were originally nailed in place using very small brads. I keep all of the excess small nails and brads that were supplied in older kits sorted away for just such times. I found some that were suitable and used those to both glue and hold the first side in place on the correct side of the floor as well. Once set, I glued the seats in place. I had saved the stove casting and heat shield and installed that back into place in front of the one single windows mid-car (**Photo 15**).



With the first side wall installed and the glue cured, I added the other side aligning it using the same process as for the first side. Then the seating and stove that goes on that side of the car was added into place. I decided not to panel the ends with wood, rather just painting the lower section Roof Brown and then giving the upper section several coats of Reefer Yellow. With some very light sanding and some more cleaning, the end fit down in between the two sides and down into a slot between the floor and platform. A fine film of Goo and a bead of CA secured it into place (**Photo 16**). Once that was fully set and solid, I repeated that same process to insert and secure the other end.

The roof section was something I did not feel any great need to improve other than its exterior. It was a carved to shape duck bill roof that fit down inside the car sides and also had two slots to accommodate accepting the car ends. Modifying in any significant way other than cosmetically seemed a pathway to bad results in putting it back on the car. The existing locations for the smoke jacks for the two stoves inside were filled with the remains of the broken off castings. One of these popped right out given a bit of percussive persuasion. The other one had to be drilled out. I used a pair of Train Craft castings that were drilled through and painted Engine Black as replacements secured with Goo and CA. The roof surface was covered with single napkin glued in place with 50% aqueous Titebond and trimmed to remove the excess. This new surface was painted Lark Dark Gray (Floquil) and set aside to dry for a few days. The moment of truth had me quite nervous but with only the slightest of pressure the roof dropped down into place fitting very snugly! I left it without any further manipulation. Knowing when to stop can be important. On that same theme, I did nothing more to the exterior than to clean it and did that to preserve the hand lettering that was already in place. I remounted the trucks after another good cleaning. But I confess that I converted from link & pin couplers to knuckle couplers.

I have no equipment with link & pin couplers. I did try to use a commercial link & pin but the install would have required altering the end platforms. So, I'm calling this car done for the present (**Photos 17-19**).

**Photo 17**



**Photo 18**



**Photo 19**

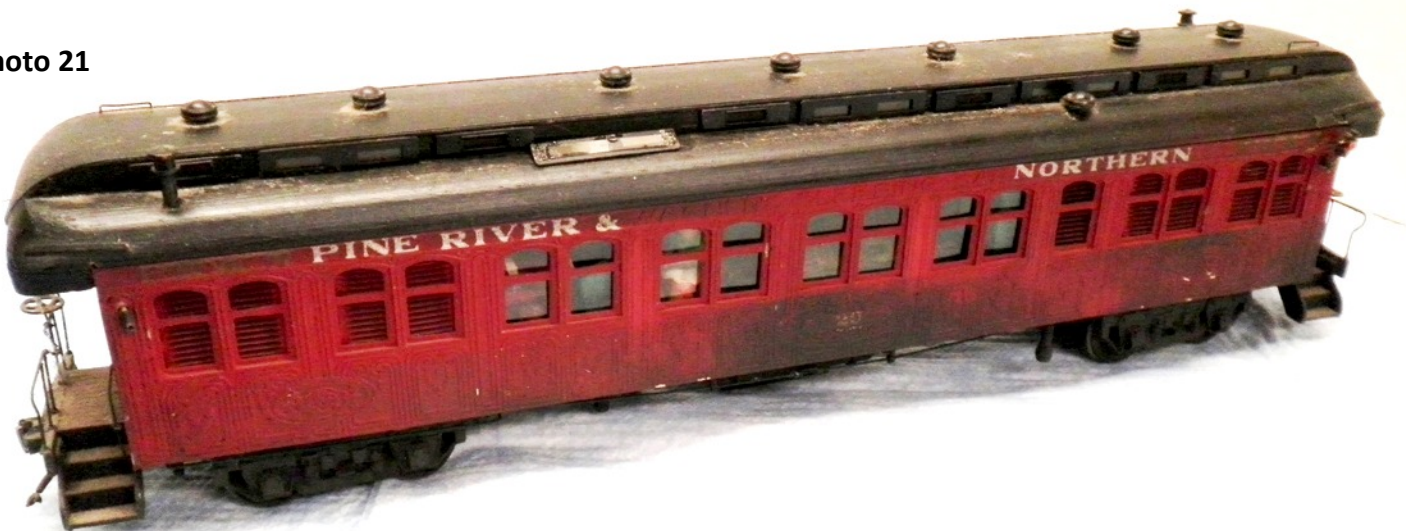


The earlier car, made from the stamped copper alloy, was next up on my bench. This car had fared far better in transit arriving fully intact (**Photos 20, 21**). It was, however, surprisingly heavy, and when I turned it over for inspection, many parts inside could be heard rattling about the space. There was a roof panel loose. The underbody was far more detailed than the yellow car (**Photo 22**).

**Photo 20**



**Photo 21**



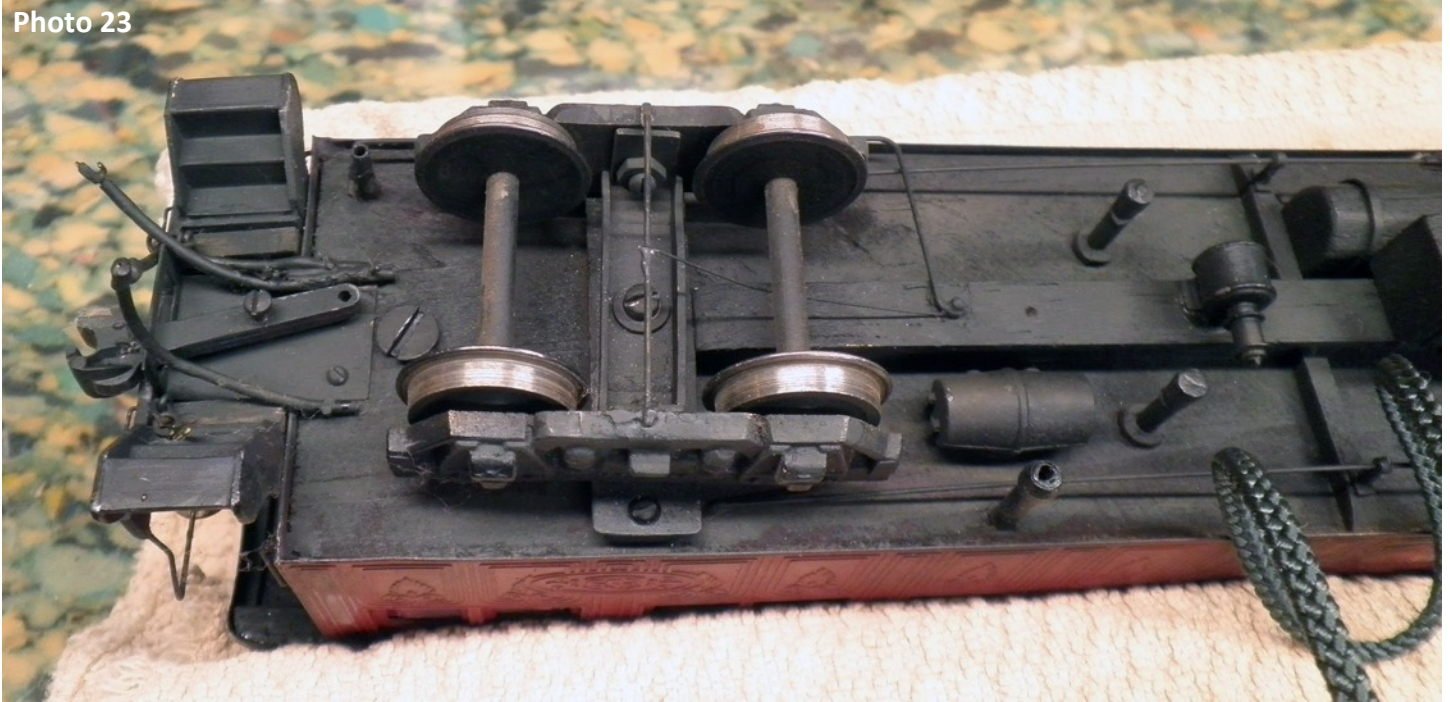
**Photo 22**



It was also not immediately obvious how the roof came off or whether it could come off. The usual first step for me had been to remove the trucks and to set those aside while looking into opening the body up to address the rattling about inside. Upon inspection of the trucks, I was confronted with another dilemma. The trucks were bridged for electrical pickup with a stiff feed that wrapped around and under each truck which then fed up

into the body at the ends through a port. During this inspection a rather large flat head screw head was found at each end (**Photo 23**). When in Rome, lefty-loosy the screw out and see what happens!

Photo 23



What happened was that 2½” wood screws were extracted from each end of the floor (**Photo 24**)! And then with a modest amount of persuasion, the roof was popped off without any fanfare. Those electrical leads were ultimately to a single rather neat looking overhead bulb that still worked just fine (**Photo 25**). Tracing the power leads back revealed that at each end there was a bare brass wire that fed down a bit of tubing (that portal that the truck lead fed into) into the floor. So the electrical contact was purely friction within that same tube. I had read of such a method being used in the past, but not encountered it in play in a passenger car.

Photo 24



Photo 25



You can also see that the roof ends were marked with an “X” and an “O” that corresponded to the same marking on the car floor interior at the ends (**Photos 26-28**). So, no worries on putting the roof back on incorrectly! Thank you original builder! One other detail here is that you can see just how thin the sides are on this car.

Photo 26

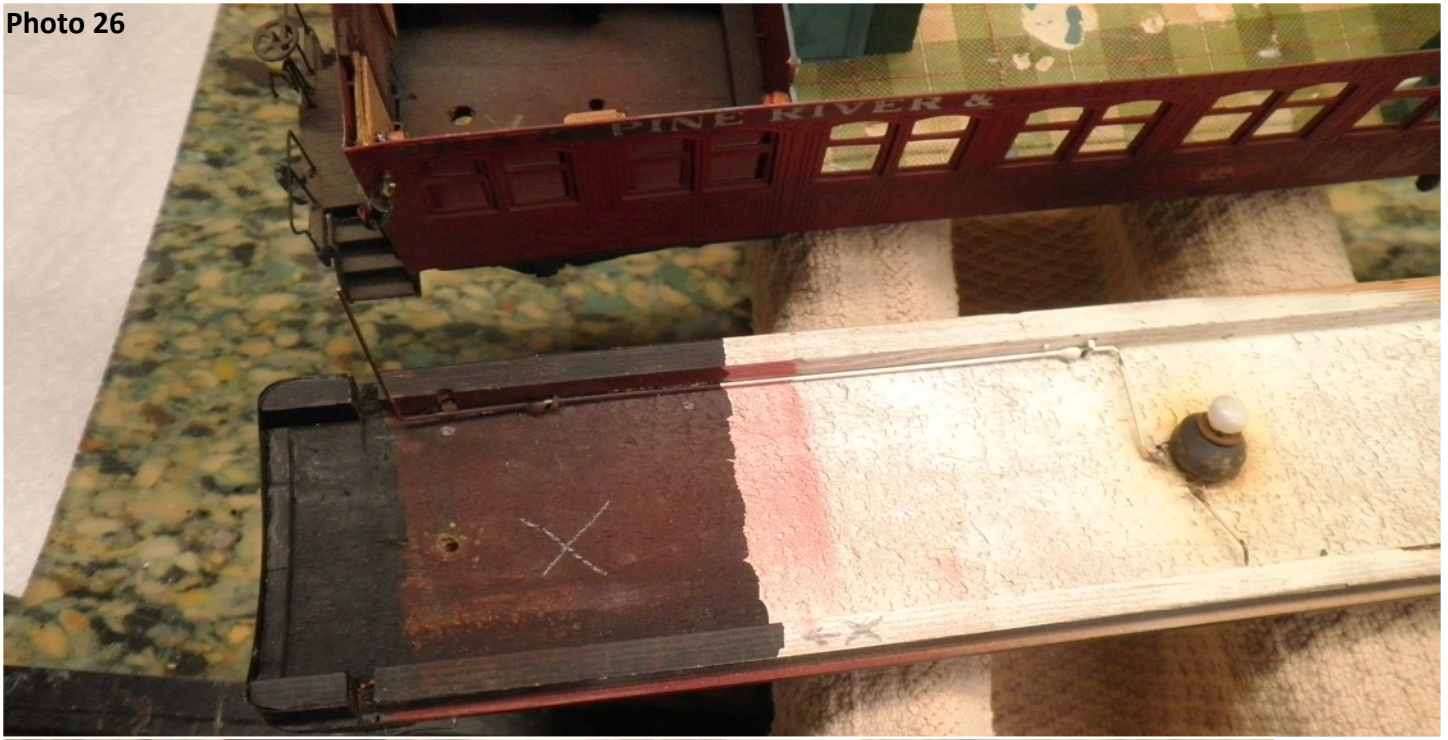


Photo 26

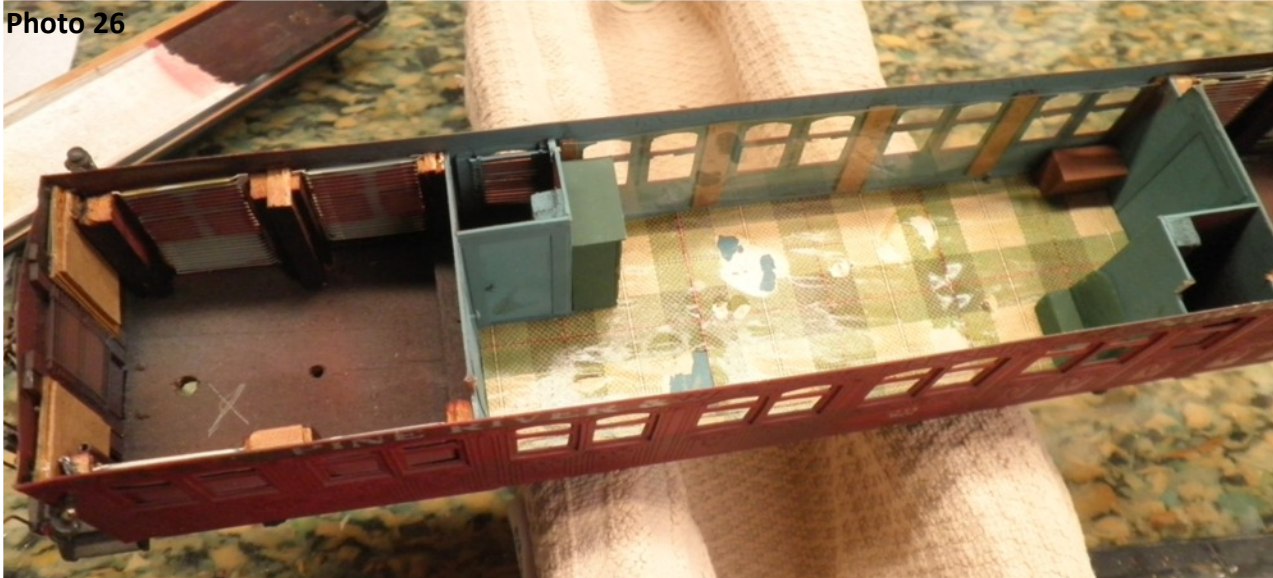


Photo 26

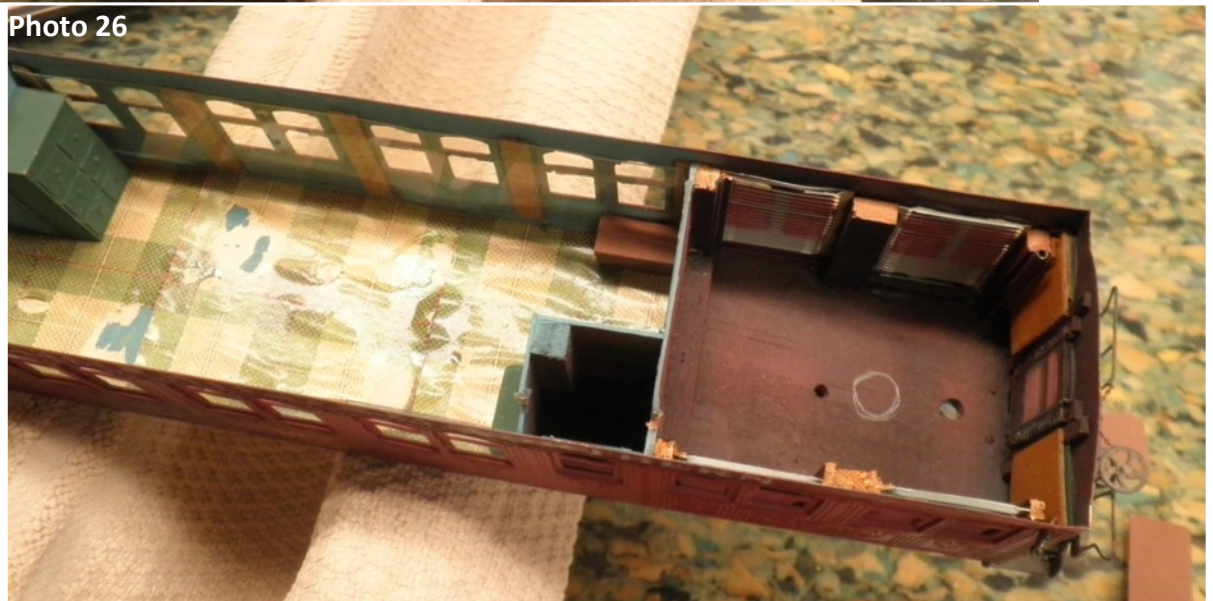


Photo 29



All of the loose interior parts are in **Photo 29**. These are not going back into their original space and later departed to a new home. You can see that only the center portion of this car had an interior. The two end compartments were not finished, however the windows were all covered with very nice metal blinds that effectively blocked one view from the exterior. As such, I saw no reason to do anything to those spaces other than a good cleaning. The center interior space was rough and had patterned paper that was not going to be recovered. The one cabinet was going to be salvaged and then a new interior of this space was going to be installed.

After that cabinet was salvaged and set aside, and the old floor paper removed, the interior was given a fresh coat of paint. Thereafter, a new floor was installed which was cut from the same paper used to make the center carpet in the yellow Exacta car (**Photo 30**).

Photo 30



New interior wood trim paneling and trim was pre-painted, cut to fit, installed, and glued into place (**Photo 31**). This is a bit tedious to execute having to work with the car assembled and on its side at an angle. Several tweezers and much patience seems to be key as well as taking lots of breaks while letting the glues to fully set before continuing.

Photo 31



I added seats (Q-Car) and tables (Keil-line) after painting them. The original cabinet was cleaned up, painted, and installed. A linked pair of seats (PNW) installed at one entry way after they were painted (**Photos 32, 33**). That completed the update of the interior to be some sort of private dining car area.

Photo 32

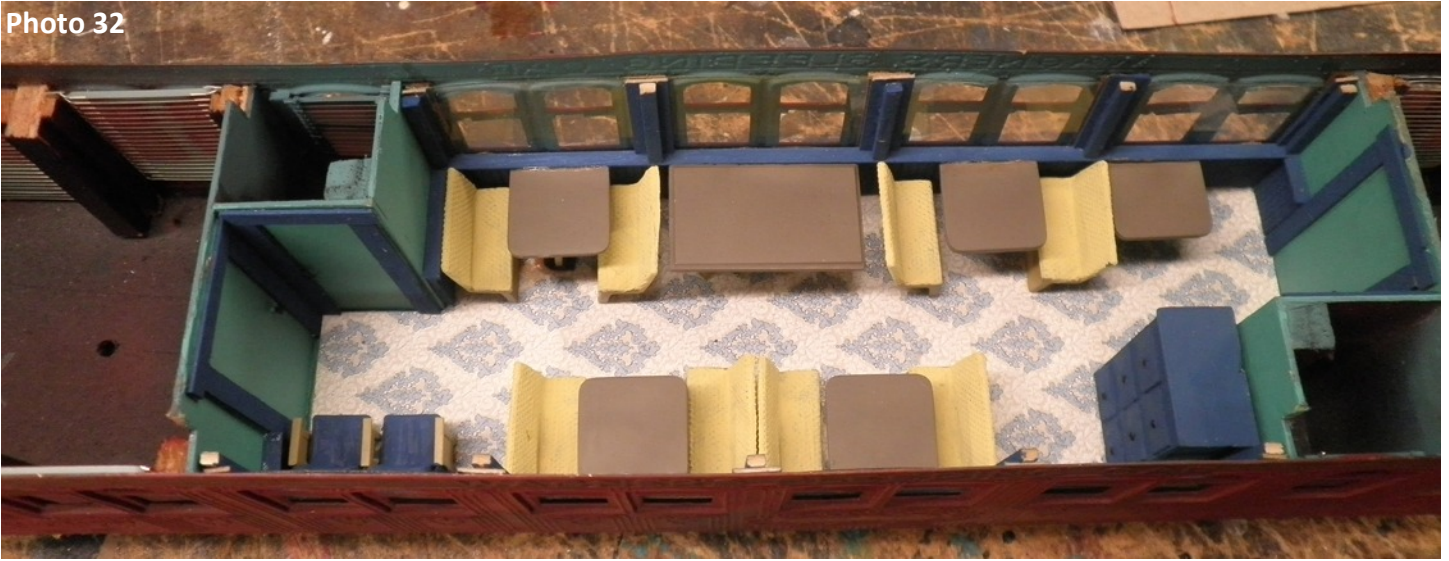


Photo 33



The exterior was a bit of a challenge. After I re-set the marker lights into place tightly, I cleaned up the platforms and railings. I really did not like any of the lettering that had been added as it detracted from the already in place embossed lettering. It's not clear in the photos but there was black lettering at both ends of the sides other than the white lettering for Pine River & Northern. This road name was also awkwardly placed on the letter board. There was also some lettering in the center emblem on the sides that was barely visible due to black overspray weathering. This old lettering was some sort of decal that was very resistant to being removed using all sorts of agents. I ended up just using lacquer thinner and a lot of Q-tips to remove all of lettering and then a layer of paint, too. This did unfortunately have the effect of attacking the original paint to a point where I had to replace it. The letter board area was painted Tuscan, not PRR Tuscan. Polly Scale made both and they are distinctly different. After I did this I touched up the original car lettering with some Antique Gold paint to high-light the Wagner Sleeper Car raised lettering that these sides express. A quick light coat of Matte Clear sealed this paint in place. Once again the roof dropped right back into place (**Photos 34, 35 next page**). Whoever built these cars did a great job! I did not bother to replace those wood screws. The roof is snug enough without them.

Photo 34



Photo 35



That completed these two Exacta cars and I'm pleased with the outcome. I'll be looking for more of the Crestline "Exacta" cars to have a bit of fun.

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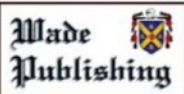
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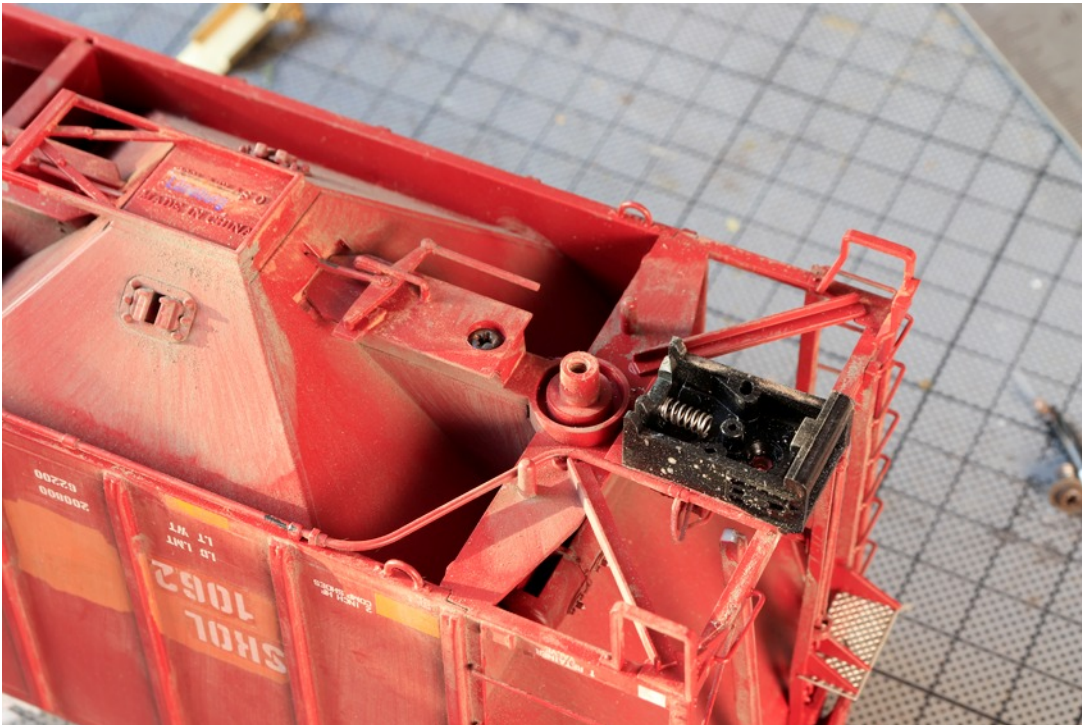
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# Draft Gear by Smoky Mountain Model Works

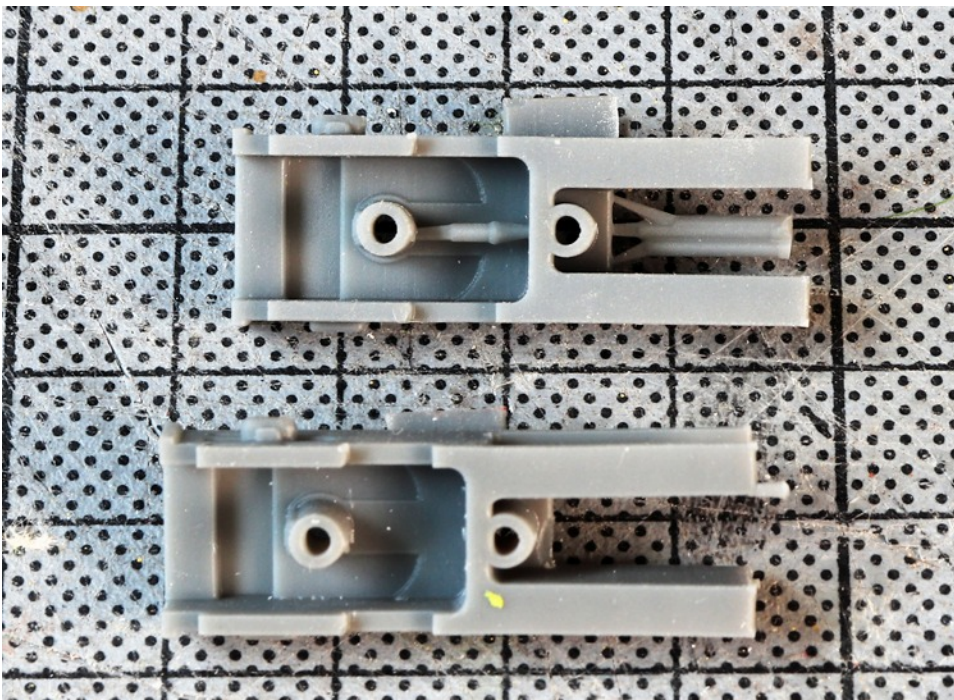
By **Andreas Lueneburg**

In a recent edition of *The O Scale Resource* (2025 September October) I was inspired by the news article from Smoky Mountain Model Works showing a Seaboard Air Line "B7" boxcar kit which is quite interesting. Alas, it is not my era, as I am modeling modern times. Browsing their website, and I was excited to find a draft gear box suitable for Kadee couplers. They arrived the other day and they look really good.

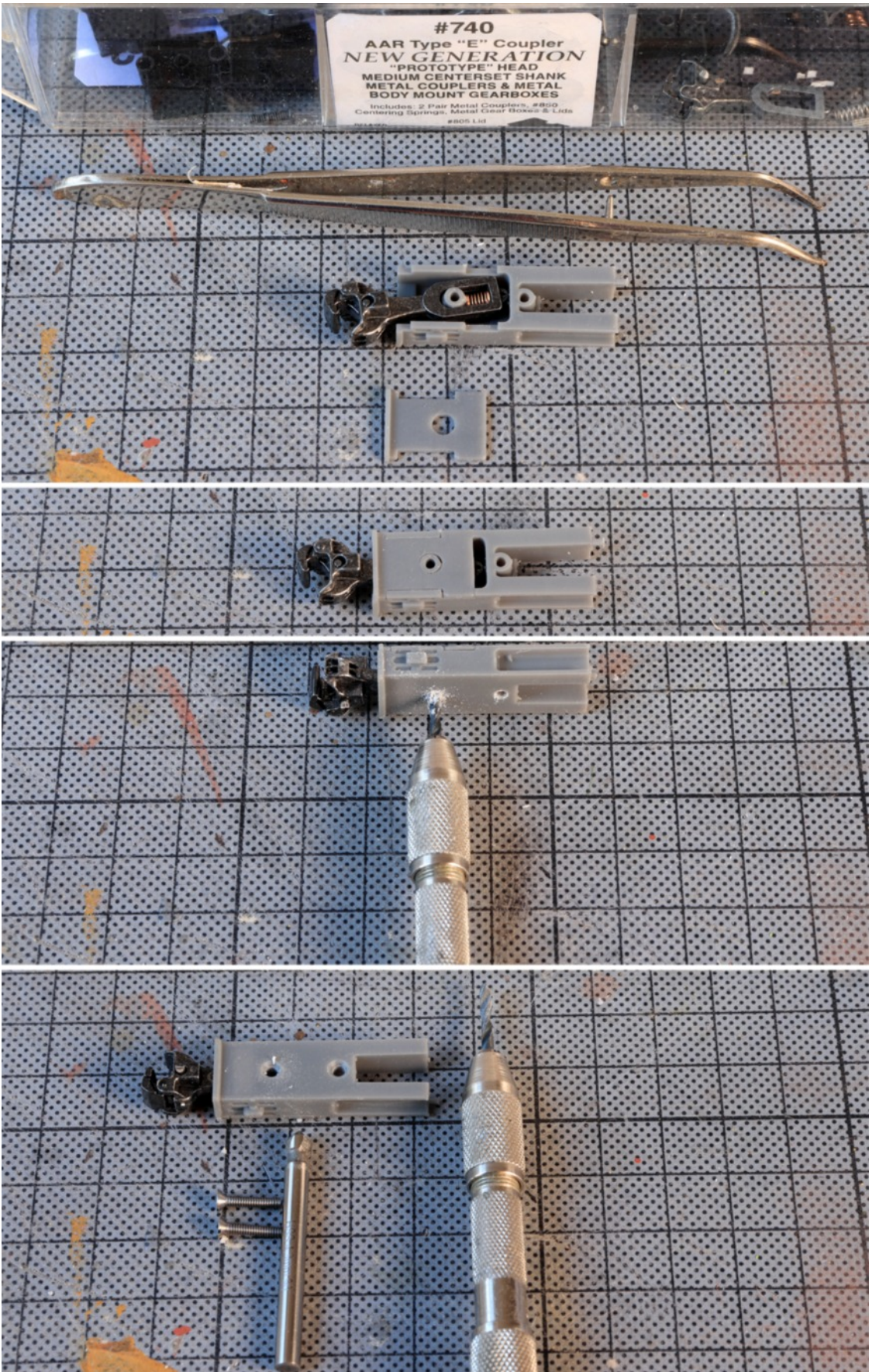
An Atlas O MASTER PS-4427 COVERED HOPPER is the first car to test them.



*The first thing to do is remove the trucks followed by the Atlas draft gear, as shown in Brian Scace's series on "A Fleet for Operations". Take a flat screw driver and pry the box open at its end. Destroying the draft gear does not matter. After removing the coupler the two screws can be loosened and the draft gear be removed.*



*Two supports need to be removed (compare top and bottom draft gear). It is an easy job with a Stanley knife, needing little force.*



*The Kadee coupler and the centering spring are placed as shown.*

*The lid fits nice and tight.*

*Special screws come with the kit, however, they do not fit the Atlas holes. Instead, 2 mm screws were used, which made it necessary to widen the holes to 2 mm. And with the covered hopper the two screws had to be shortened to a length of 8 mm.*

*Having put so much effort into detailing, countersinking the holes for the screws is the final job to make the screw head be flush with the draft gear.*



*Now the extra length of the draft gear can be cut off, I did it "in situ."*

*Ed Note: For you all who did not sit through four years of Latin, phrase "in situ," meaning "in the place" or "on-site."*



*Painting the draft gear is the final job and make the car look really prototypical.*



The left hand car was detailed with parts that used to be available at SHAPEWAYS, which went out of business, the Smoky Mountain parts are a great improvement for detailing cars, making them look so much more prototypical.

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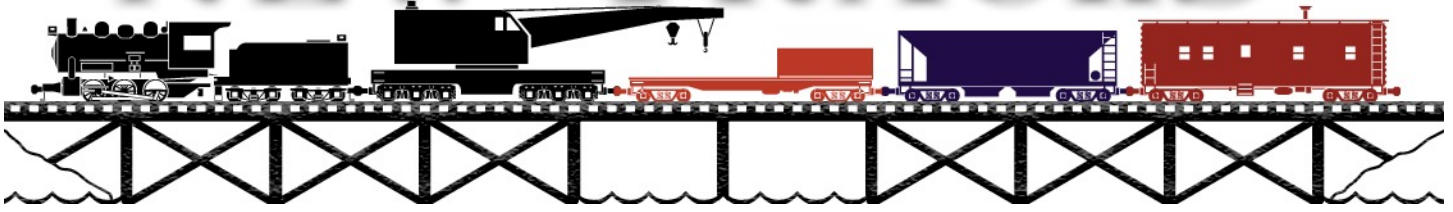
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# NEW TRACKS



## Mentor Definition: A Trusted Counselor or Guide

By Contributing Editor **Jim Kellow MMR**



Greg Cassidy NTM, February 1, 2026, at the center hall at the Great Scale Model Train Show in Timonium at the New Tracks Modeling table.

Greg's On18 module he built on the New Tracks live Zoom shows January 21 and 28, 2026 is on the table. Thanks Greg for representing New Tracks at Timonium. (I think Greg may be the next New Tracks member to be awarded the NMRA MMR.)

## Current Modelers Helping the Next Generations



I originally profiled Steven Harvath NTM in 2022, now here he is mentoring his grandson. Welcome back Steven.

Mentoring kids is challenging, rewarding, and fun. There is no doubt in my mind that model railroading can provide a solid foundation for kids to learn how to build whatever they want with their hands, gain confidence in their abilities to create models, and develop their imagination to visualize what is possible and achievable.

I love hearing from readers about their involvement with kids. I often receive comments from readers that make me feel my emphasis on mentoring kids and providing them opportunities to experience the model railroad hobby are appreciated and actually helping bring more kids into our model railroading hobby.

Steve Harvath is a talented O scale modeler I profiled in one of my articles in 2022. Here are several photos of his modeling. He recently sent me this email about his grandson's interest in model building. Thank you, Steve, for sharing your Grandson's experience. You obviously are very proud of his modeling.



Steve wrote: "I am lucky to have a 6-year-old grandson who loves trains. He sees me scratch building cars and locomotives and wanted to do the same. Together we found a prototype to build. The CP Huntington. I happened to have a leftover set of drivers and truck wheels from an old O-scale Rivarossi kit. I had him draw a plan. Then we built it out

of wooden dowels, basswood, and some balsa for the cow catcher. Explaining what a cow catcher was was a good teaching moment, as was the function of the sand dome. He made the finished model his own with the paint scheme. We need to get them hooked when they are young.”

Steve, I agree. I look forward to seeing your grandson's application for our New Tracks Mentoring Scholarship program down the road.

I so enjoy hearing stories like Steve’s about young modelers getting mentoring help to begin their journey in our great model railroad hobby.

I also love to hear from older kids about how New Tracks has helped their modeling.



Mitch (Doc) Lovelace NTM sent this email to Chris Coarse, NTM, our host for the MY BUILD monthly show segments, and me about his involvement in our MY BUILD monthly segments. If you get a chance take a look at some of Doc’s models on our recorded My Build monthly segments on our “New Tracks Modeling” YouTube channel.

“Hey Jim, and of course Chris. I wanted to share this clip with you two. I tend to be a little sentimental and potentially overly emotional when I speak about New Tracks. A situation happened this week that brought it all right back to me. Very shortly after I was introduced to the program, I realized that you were doing a My Build segment for water in June of 2023. This was the first interaction I ever had with the program and that was the exact moment that I took the hobby seriously for the first time in my life. It was always fun, but with that push and the wonderful words of encouragement that came

from that segment is what allowed me to dive straight in head first. I bring this up now, because I have liquidated the majority of dioramas I have built and also featured on segments to help finance my future projects.



The large city block, built-in N scale, that you two so graciously applauded my attention to detail for the roof section, was the first to go almost a year ago.

The waterfall diorama-the first time I had ever used any of those products (two-part epoxy, plaster Rock castings, Vines, handmade turf, hand-painted figures, etc. etc.) - just sold to a gentleman on eBay 6 days ago. He had mentioned that he was starting a layout from scratch and was essentially going to build an entire scene around my waterfall. I thought perhaps, maybe he'll think differently when it arrives ??, but to the contrary, he absolutely fell in love with it! He said that the pictures did not in any way do it justice! So I wanted to give you both a very big THANK YOU for the encouragement and the guidance and the education along this journey. I'm still creating dioramas (as I'm still renting, and tomorrow is never certain), but they are just getting bigger and bigger at this point! Thank you so much. I'm so happy to be a part of this hobby and in the company of such wonderful people! I know I'm crazy for sticking with N Scale, but there's got to be a doctor in the house somewhere! ??-Doc”

Thanks, Doc, for your comments. I look forward to seeing your next diorama. Chris and I encourage everyone, regardless of age, or experience, to consider showing your modeling on one of our monthly New Tracks MY BUILD segments. We believe you may become a better modeler from the experience on the show. Let me know what you think. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).

### **Encouraging Kids To Build Railroad Models**

I hear people talk about getting the young generation involved in our hobby all the time. But I am not sure I totally understand what it takes to get that accomplished. Do you? So, I asked Gemini AI how to do it to see some AI ideas. One idea from Gemini AI that caught my attention was “Cross-Curricular Connections: Model railroading could be integrated into school curricula, connecting it to subjects like history, science, technology, engineering, and math (STEAM). This would expose more kids to the hobby and highlight its educational value. In essence, learning to build railroad models in the future will likely be a more interactive, engaging, and personalized experience for kids. Technology will provide them with powerful tools and resources, while still emphasizing the importance of creativity, problem-solving, and hands-on learning.”

This makes a lot of sense to me as I think about the importance of model building in our hobby, and the fun kids get in learning to use the new technology to solve modeling problems and provide the creativity model building requires. It would also reinforce the STEAM education our kids could get in their early years, and help them qualify for our New Tracks Modeling Mentoring Scholarships.

I wonder if model building by hand is a part of any of our local school system’s middle and high school courses? I also wonder if any of the local clubs or other extracurricular activities offered by our local school systems include railroad model building? I wonder if any model railroad manufacturers, clubs, or associations currently support such educational programs in our local schools? Something to investigate? Particularly, as I noted in a previous article, only 20% of our high school graduates are ready to take college undergraduate level STEM programs. I think I may have found something all model railroaders should consider getting involved with. What do you think? Here is one option recommended to me.

### **Local Robotic Clubs May Be One Place To Get Kids Involved In Model Railroading**

I read somewhere if we want to talk about model railroading with today's youth we need to not expect them to come to us, but rather we need to go where they are. One of these places is your local Robotics Club. The clubs may need volunteer mentors, which is a great place for our New Tracks Mentors (NTM) to help out. There are definite linkages between robotics and model railroading. Read the following comments and check out the local club in your community. I did, and you will find my comments about what I found later in this article. Let me know what you think.

Here is what Gemini AI told me about the model railroad/robotics connection.

It's fascinating to consider how the principles of FIRST Robotics could be applied to model railroading. First Robotics is the program used by my local robotics club. Here's a breakdown of how these two concepts can intersect. Please visit FIRST Robotics website to find out about their local international programs: <https://www.firstinspires.org/>.

### “Key Concepts about FIRST Robotics:

- \* **Automation: FIRST Robotics** emphasizes automated systems. This aligns perfectly with the desire for realistic and complex model railroad operations'
  - \* Train routing and scheduling.
  - \* Switching and signaling systems.
  - \* Yard operations.
  - \* Interactive elements on the layout.
  
- \* **Programming and Control:** Robotics relies on programming to control actions. In model railroading, this translates to using software and microcontrollers (like Arduinos or Raspberry Pis) to manage the layout's functions. This enables:
  - \* Precise control of train speeds and movements.
  - \* Creating realistic operating sequences.
  - \* Integrating sensors for feedback and automation.
  
- \* **Engineering and Design: FIRST Robotics** encourages problem-solving and engineering design. This can be applied to:
  - \* Designing and building custom control systems.
  - \* Creating unique layout features and mechanisms.
  - \* Developing innovative solutions for challenges in model railroading
  
- \* **Sensors and Feedback:** Robots use sensors to gather information about their environment. In model railroading, sensors can:
  - \* Detect train positions.
  - \* Monitor track occupancy.
  - \* Trigger signals and other actions.

### Integrating These Concepts into Model Railroading:

- \* **Computer Control Systems:** Use software and microcontrollers to create sophisticated control systems for your layout.
  - \* Software like JMRI (Java Model Railroad Interface) can be used to control digital command control (DCC) systems and automate operations.
  - \* Automated Yard Operations:
    - \* Design and build automated systems for sorting and dispatching trains in your yard.
    - \* Use sensors and actuators to control switches and movements.
  
- \* **Interactive Layout Features:** Create interactive elements on your layout, such as automated crossing gates, working signals, and animated scenery.
  - \* Use sensors to trigger these features when trains approach.
  
- \* **Robotic Elements within the layout:** It is possible to create robotic elements that interact with the trains. For example a small robotic crane that loads and unloads freight cars.

### Where to Find Resources:

- \* JMRI: This open-source software provides powerful tools for controlling model railroads.

\* Arduino and Raspberry Pi: These microcontrollers are widely used in hobbyist electronics and can be used to create custom control systems.”

I believe combining the principles of FIRST Robotics with our model railroading hobby, can truly create dynamic and engaging layouts. I also believe we may be able to attract young people who enjoy robotics to our hobby, which is a major reason New Tracks exists. I hope members of our model railroad community will consider looking into their local robotics club and see if they can offer help and mentoring to the kids involved, as well as introduce them to model railroading.

Please note my following comments about my own personal experience with my local robotics club. As you will see, I believe there are many opportunities for our model railroad community to help integrate model railroading into local robotics clubs and also help provide mentoring in STEAM programs for the kids involved.

### **My Local Citrus County Florida Robotics Club**

I think combining model railroading with robotics will help more kids get a steppingstone to a “STEAM” undergraduate college education. A message from Harry Lewis, owner of Quality Production Services, a machine shop in Inverness Florida, got my attention about the local Citrus County Florida Robotics Club and its kids involvement with STEAM education as well as the model railroad connection. Here is what Harry wrote:

“Hello Jim, great article in today's newspaper. I appreciate your support of kids in STEM. I am an Inverness machine shop owner, and I participate with the local Robotics Club, I thought you might be interested in checking them out and getting involved. There are a number of high school kids from the local area that are involved in many aspects of designing, crafting, and operating competition robots. It is not an exact comparison to model railroading, but the crafting expertise can apply in many ways. Here’s the link to their club: Citrus County Robotics Club.” Intrigued by Harry’s comments, I wrote this article for my local Chronicle newspaper, but feel it tells a story that may well apply to many other local robotics clubs.

I visited my local club’s website and connected with Margo Wilson, who had written an excellent article about the local Robotics team in the April 2, 2024 Citrus County Chronicle. I also received information from a local School teacher Waunee Endsley, who coordinates and provides the leadership to the club from our local school system.

I am truly impressed with the local Robotics Club’s leadership, which has been provided by the school system teachers, volunteers, parents, and particularly the kids who have nurtured and developed the club to its current level. Congratulations to you all.

I recalled recently reading an insightful article in *Worthwhile Magazine* by Raymond James & Associates, which highlighted the growing demand for STEM skills. It reported that by 2032, STEM jobs are expected to grow five times faster than non-STEM careers, yet only 20% of high school graduates are prepared for college-level STEM programs. The article emphasized how STEM education builds critical thinking, problem solving, and leadership skills that are essential in any career.

This thought inspired a light bulb moment for me. A combination of kids involved in model railroading and robotics might just be a partial answer to increasing that 20% and preparing more kids for STEAM undergraduate college courses as well as getting kids involved in model railroading.

Wait a minute, I wonder if the trains on my model railroad can be considered robots? Can robots be designed to run my model railroad? How much can the field of robotics be useful in designing, building, and operating my model railroad?

Could robotics even enhance my model railroading experience? With questions swirling in my mind, I sought answers and found them through the Nova Artificial Intelligence (AI) app. It suggested ways robotics could transform model railroading, from automated track-laying and maintenance to interactive features like moving bridges or synchronized lighting. Incorporating robotics into your model railroad project can

significantly increase the precision, efficiency, and overall enjoyment of the hobby. Whether you are just getting started or looking to enhance an existing setup, there are numerous ways to leverage robotic technology to create a more dynamic and engaging model railroad. The possibilities are endless.”

This revelation led me back to Harry, who enthusiastically discussed the robotics club’s mentoring efforts, funding needs, and search for a dedicated year-round workspace. We agreed on the critical role our community can play in supporting this initiative. With only 20 local high school kids involved out of 4,700 public high school students, there’s immense potential for growth in the club.

But my local robotics club has needs. More mentors, especially retirees, Veterans, and professionals with technical or leadership skills, to guide the program and the students. Marketing volunteers who can help recruit more kids and parents, while attorneys, accountants, and financial experts can assist in establishing a nonprofit 501(c)(3) Florida Corporation to secure dedicated funding for the club.

A permanent location for the club is needed, perhaps at the Fairgrounds near the Citrus County Model Railroad Club or through partnerships with the local Boys & Girls Club, or the various Veterans Groups, etc. I think of the local Boys & Girls Club for several reasons. The most important is the following statement I received from Travis Anderson the Director of the local Boys & Girls club. “Boys & Girls Clubs of Citrus County are dedicating our summer program “Summer of STEM”. STEM and robotics programs at the Boys & Girls Clubs of Citrus County provide invaluable opportunities for youth to develop critical thinking, problem-solving, and teamwork skills in a fun and engaging environment.” Their interest in robotics for their kids really got my attention. Since Travis is already the recipient of model railroad donations offered by local residents who have read my previous articles, this could be a very special opportunity. Oh, my, another rabbit hole for me to explore. But nothing ventured, nothing gained, as I have learned many times from my various New Tracks activities.

After all, our community has many retirees who have moved here from all over the world and who possess very impressive talents and skills gained over a lifetime of successful careers. We also have a significant Veteran community whose members have a wide variety of leadership and technical skills that can benefit the kids. Many of these people can be excellent mentors for the kids who are interested in programs and hobbies that can help them prepare for undergraduate college level STEAM courses. It is a win-win situation for everyone involved. We older people can donate our skills to be used to help the next generation of workers complete their educational goals, and the kids can learn skills and develop leadership and business knowledge from talented experienced mentors.

I hope many of you sitting and reading this article will see the value of your participation and volunteer to become mentors. If you don’t believe me that you are needed, call Harry. He will definitely help you understand how valuable your involvement will be, and the wonderful reward you will get when you do become involved!

To provide dedicated financing, maybe the local Robotics Club can establish its own Florida Nonprofit 501(c)(3) corporation, manned by volunteers, to help the kids obtain the significant dedicated funding the club requires for the development and growth of its program.

Working with the local Veteran Associations and retired Chief Master Sgt. John Stewart, who writes the Veterans Column for the Chronicle newspaper, may help find a permanent space for the club as well as help encourage veteran volunteers. John wrote about my article about the club and its needs after reading my article in the Chronicle newspaper.

Currently my local Robotics club is primarily a program for our local Public-School kids. I recently read an article in, *The Rattler*, a Reason newsletter by J.D. Tuccille about the Public School Choice program. He stated: "By the end of 2024, more than one million students in America were participating in a private school choice program", Ed Choice’s Colyn Ritter reported this week. "Participation growth has climbed rapidly, having more than doubled since the beginning of 2020. Florida leads the way with nearly 13% of students enrolled in a private school choice program.”

Only one private school kid, and no home schooled or homeless kids are involved in the Robotics Club. I could not believe in this day and time we have homeless kids, without adults, living alone in our community and trying to get an education. It seems a county wide marketing program to increase awareness and participation by all the eligible kids and their parents or designated guardians, regardless of their educational situation, would be very beneficial to both parents and kids in our community. Frankly it may be critical if we are to increase the 20% figure of kids ready for a STEAM college education. I believe a separate parents organization, whose kids are currently members of the Robotics Club, which focuses on communicating the benefits and value of joining the Robotics Club could definitely help to increase student participation.

My comments are not meant as criticism of the local Robotics Club, but rather my opinions as to ways the program may want to consider going in the future to include more kids. This kind of program can give local kids a major step up in their future STEAM education and the enjoyment of the model railroad hobby. We all owe our kids the opportunity to join clubs such as this, and learn. I urge members of our local railroad modeling communities to rally behind their local Robotics Club. Together, we can inspire the next generation of innovators while giving retirees and Veterans a meaningful way to share their expertise.

Let's work together to create a win-win for our kids in local communities around the world. As someone once said: "It takes a village." We all need to help. That includes local Model Railroad Clubs, NMRA Local Divisions and Regions, the TCA, The TTOS, LOTS, LCCA, national modeling associations, and local stores such as model railroad hobby shops, local Hobby Lobby, local Lowe's or Home Depot may even get involved.

If your kid is interested in a STEAM education and is not a part of the local Robotics Club and/or if you have skills these kids need to help them improve their STEAM knowledge and creativity and are not yet involved, please investigate your local program and see how your kid can benefit and how you can help. Your help can be a win-win for both you and the kids you work with. There is no greater honor a mentor can get than being asked for advice and help, and no greater gift the mentor can give than to provide the information asked for. The gift of education we provide our kids will last their lifetime, and no one can ever take it away.

You can contact me at [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com). I look forward to hearing from you about your participation in, or questions about, your local community's program.

Time for me to move to the next rabbit hole I am exploring: storytelling. I recently read that about 25% of model railroaders use storytelling to enhance their model railroad experience. This figure is expected to grow to somewhere around 75% in the next 10 years. So, I decided to investigate what storytelling in model railroading was all about.

## **My First Attempt At Storytelling**

I promised last article I would post my first story. OK, here we go with the story from my challenge to you from last month. Keep in mind this is my first attempt at being a model railroad storyteller and I know I have a lot to learn. But I enjoyed my first effort and plan on spending time developing a style of storytelling I am more comfortable with using for numerous scenes on my railroad empire.

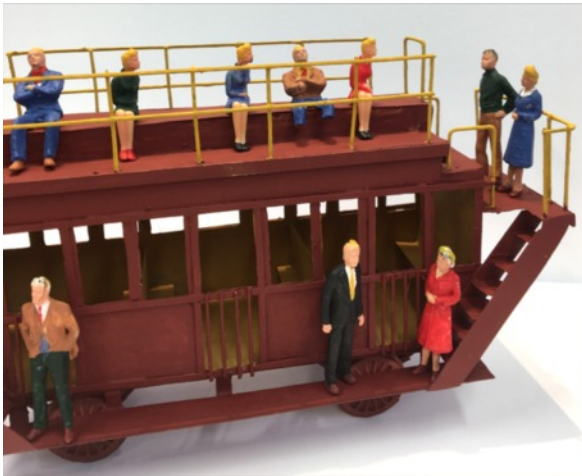
But for now this is my story based on the template information I outlined last month I provided Perplexity AI to develop my story. I would really appreciate having your comments and suggestions to improve my story.

My story: All Aboard for a Wedding, Whirl-arounds, and a guy wearing plaid pants on a trip to the JN&P RR Conservatory and Museum

At exactly one o'clock on Saturday, June 3rd, 1930, the JN&P Railroad Station is hopping — well, as much as a sleepy country station can hop.

The first afternoon trolley from Lucky Hurricane, Florida rattles in with a cheerful bell that sounds just a little too proud of itself. The doors swing open, and out leaps — as though launched by a spring — the day's

most enthusiastic tourist: a large gentleman whose plaid trousers nearly deserve their own passenger ticket. Behind him shuffle a mixed crew.



Workers in period costumes straight out of the 1800s, and visitors still fanning themselves in modern 1930s hats. Those waiting on the platform, bags in hand and ready to return to town, watch this little parade with vague amusement. No one seems in a rush — after all, this is the countryside where time walks slower than the horse that pulls the Conservatory's famous double-decker carriage.



### Attractions Too Charming To Rush Seeing

The JN&P RR Arboretum Conservatory and Museum has everything a visitor could want: hand-built statues, ridiculously picturesque trails, and benches strategically positioned for maximum people-watching (or pigeon-watching, depending on one's outlook). Families sway along the trails while their children spin themselves silly on the whirl-around — an invention designed either for joy or motion sickness, depending on who you ask.

Meanwhile, a piano player, in a wagon painted in patriotism as loud as his chords, bangs out tunes that make some wonder whether Chopin could've survived Florida heat. Local legend insists this player knows only three songs, but he plays them with the confidence of a man convinced the tourists won't notice.

And speaking of tourists, quite a few look sunburned already, proving once again that hats may be fashionable, but shade is priceless.



### The Day Includes a Marriage

Adding spice to the afternoon is today's big event: a wedding in the Conservatory. The chapel — so small it makes a broom closet look ambitious — just couldn't handle this crowd. So now the Conservatory shines as the setting, its brass and glass catching the sun like it's auditioning for magazine covers.

The soon-to-be-newlyweds have already had a quiet ceremony with the pastor in the chapel (lucky them — it has better acoustics and far fewer nosey cousins). Now, they'll repeat their vows for everyone else in the glass palace while relatives and townsfolk do their best to look polite and not faint in the heat.



### The JN&P RR Knows How to Make a Dollar!

They're offering special wedding trolley fares and even honeymoon packages — a deal so good the groom might be tempted to elope using it.

### A Place Built to Last (and Laugh)

The whole operation is a point of pride for Lucky Hurricane. It's more than a tourist attraction; it's an escape from the daily hustle and bustle of town life. On Saturdays, people stroll instead of scurry, children laugh instead of sulk, and even the pigeons seem more relaxed.

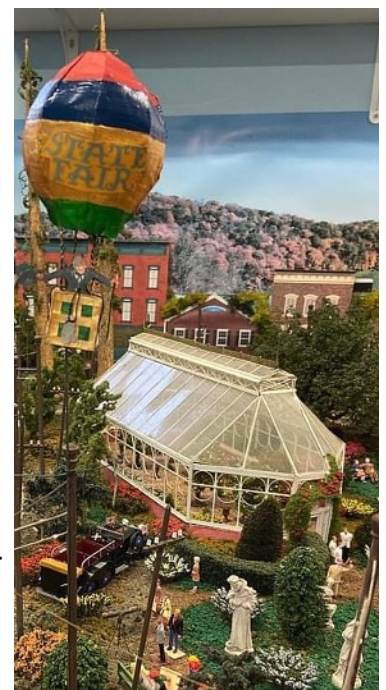
Every part of the grounds was crafted by local artisans, from granite statues that grimace in noble silence to the Conservatory itself, a brass-and-glass wonder that, according to one visitor, is "like standing inside a greenhouse built by someone with too much free time." The church, too, was shipped piece by piece from Europe —

though someone swears one plank still smells faintly of the ocean.

For ten years, the JN&P Conservatory has been a hit. Tourists love it, locals love it, and even that gentleman in plaid pants seems destined to love it — once he stops running around like a child who's had too much lemonade.

Well, well, hear comes the Mayor of Lucky Hurricane Florida to make sure all his citizens see him checking out the area. He is using hot air for transportation, so it must be election year again. Hello Mayor. Keep supporting New Tracks and the JN&P RR and you have my vote.

I hope you enjoyed my first effort at storytelling. I would love to see what your story looks like. Does your first story effort with AI make you want to become a storytelling model railroader? It sure did for me! I look forward to seeing everyone's stories. My email is [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).



Time to get back to railroad modeling. Here is my fictitious story about a trip on the East Broad Top (EBT) Railroad a long time ago. I hope you enjoy it. I also hope it can stimulate some modeling opportunities for you. It did for me.

## A 1920s Journey On The East Broad Top Railroad

Now let's take an imaginary Bucket List Train Journey on the EBT in the 1920s. Is anyone modeling the EBT? If so I would love to hear from you and see photos. Also tell me what you think about my imaginary trip?

In Citrus County, Florida our first railroad was the Silver Springs, Ocala, and Gulf Railroad which arrived in 1887. Ken Marotte, a member of the Citrus County Historical Society wrote an excellent article in the March 3, 2024, issue of the Chronicle newspaper about this railroad and its various owners before it eventually ended up as part of CSX. It originally served the local phosphate industry and people in our county much like the EBT did. To me it is another example of how small local railroads which connected to major railroads helped build our country in the early part of the 20th century.

But before I start this article about the EBT, I need to thank Lee Rainey for his help, knowledge, and guidance to me in preparing this article. I used these publications written and graciously provided by Lee Rainey in this article: *The Mail Train; Branchline Operating Schedules; Two Million Miles on the EBT*; Neville Shaver; and *How To Read An EBT Timetable: The Twentieth Century*.

### It's the early 1920s and the EBT is at the height of its glory. Let's get on and take an imaginary trip!

The early 1920s amended the constitution to give women the right to vote, began Prohibition and criminal bootlegging, had Babe Ruth hitting almost every baseball thrown to him for a home run, Al Jolson was singing one of his great hits, "Swanee", and it's the time of F. Scott Fitzgerald, flappers, and the Jazz age. The Great Gatsby was about to become a mystical figure. There is no question it was a time of dramatic social and cultural changes in America, including how we traveled. Train travel exploded as the 20s roared along!



I never heard my mom, dad, or my grandparents even talk about the 1920s. I think my parents must have been too young and my grandparents too old for it to have had a significant influence on their lives. Therefore, I must rely on historical documents to try and piece together the life and feeling of a family during the 1920s. But one thing is certain. If the family lived in central Pennsylvania they definitely rode the EBT and probably also rode the Pennsylvania Railroad.

This era was the height of passenger traffic on US railroads, with the average citizen traveling 447 miles a year by rail. Today, 2024 the vast majority of us never board a train. This growth was also felt by Short Line Railroads like the East Broad Top (EBT) Railroad which served the coal mining area of central Pennsylvania.

In 1920 the Pennsylvania Railroad carried 20% of all rail passengers in the United States, and for many in the heart of Pennsylvania, exhibited the elegance and refinement of rail travel. That same year approximately 250,000 passengers, its peak number, traveled on the EBT.

This accounted for about \$50,000 of the line's \$700,000 total revenues. Mail and express service matched the passenger revenue, with the vast majority of the EBT revenue coming from its coal hauling business between Robertsdale and Mount Union, Pennsylvania.

From its start, the EBT's management sought to grow freight tonnage, ridership and increase the narrow gauge trackage to over 60 miles including 33 miles of mainline. By 1920, it had become a highly successful Short Line Railroad.

The EBT provided people throughout its central Pennsylvania service area railroad access not only to Mount Union for medical care and shopping, but to the world, via connections at Mount Union with the Main Line of the Middle Division of the Pennsylvania Railroad (now Norfolk Southern). The management sought to make sure not just the coal industry, but the people living in the area, got the best, and most up to date, train service possible.

The fast speeds of the daily mail trains, the fairly new coaches with heat, and electric lights, and the beautiful interiors of the separate day coaches, aka, the Ladies coaches used by the Gentry, combined with the personalities, and friendliness of the train crews and various types of customers made for memorable trips.

For example, the Conductor, Orlando (Fish) Moore, is a legend. Orlando collected the tickets and always selected the most beautiful lady in the coach to sit beside for the trip. Rumor has it the ladies kept a record of how many times "Fish" had sat with them and used to compare their number to the numbers of other ladies in the community. The lady with the highest number was deemed the most beautiful. This in itself probably encouraged ridership by the ladies.

But above all, the unique travel experience getting to and from Mount Union and Robertsdale, combined with the dedication exhibited by the management and employees to provide the best travel experience possible for their customers, kept ridership high and customers happy. While a bumpy ride or a coach that needed attention were negatives complained about in the press, the EBT was the hometown economic and social backbone for the area and therefore the local citizens and political leaders supported it. But then Henry Ford and his everyone's Ford automobile came along and things changed.

The EBT ran to and from Mount Union and Robertsdale in the mountains. Robertsdale was a typical coal mining company owned and controlled town founded in 1873. The train station was in the center of town a few blocks across from the hotel, which was mainly used by visiting businessmen.

On its way to or from Robertsdale, the train passed through flag stop rural areas of Cooks's, Rocky Ridge, Cole's, Saltillo, Three Springs, Pogue, Orbisonia, Shirley, and Aughwick ending or starting in Mount Union, where it supplied the local industry with coal as well as transferring some to standard gauge rail cars on the mainline of the Pennsylvania Railroad for transshipment out of the area. Orbisonia was usually the busiest station stop as it was the location of the EBT Headquarters and maintenance facilities.

For passengers who had business or shopping to do in Mount Union, the EBT provided a day trip from Robertsdale and the surrounding rural areas arriving in the morning and returning passengers to their origin point late in the afternoon. Each full fair passenger could bring 150 pounds of luggage with them at no cost. Additional luggage was charged a fee per pound.

Mount Union was a thriving Pennsylvania Railroad yard location, and for many in the area, was the major "Big City" destination that could connect them to see lifestyles they could otherwise only dream about. It was a place to go to vote, serve on a jury, pay taxes, see a doctor or attorney, sell their extra farm produce, purchase products unavailable in the countryside where you lived, have a nice lunch, or just window shop to see what new fashion or technology was available.

It was also a place passengers could connect with a Pennsylvania Railroad train going in or coming from, either an eastern or western direction throughout the country. It must have been great fun to see the passengers

who disembarked from the Pennsylvania trains and make up stories about who they were, where they came from, and where they were going.

Naturally, an overnight stay at one of the the Mount Union local hotels was a major event for most people who rode the EBT, and naturally it provided memories that could last for years and for sure help liven up a person's local story telling.

So what was the EBT? For over 150 years it was a major economic engine and asset for the entire area it served. It fueled the steel industry until technology eliminated much of its need. It was a fairytale as well as a necessary method of transportation to get people from the countryside to Mount Union and back. It was a necessity to allow local farmers to sell their excess crops, and for people to obtain products not available in their areas. But then things changed for railroad passenger service in general and on the EBT.

I would have loved to have taken a trip on the EBT in 1920, met the fellow travelers, and absolved the sights, sounds, and smells, of that bygone era. What was it like to stay in the Robertsdale and Mount Union hotels? Wonder what the lady "Fish" was sitting next to looked like? How was she dressed? Wonder how many stops at Flag stop stations I would have had? What went on at each stop?

I will never know the answers to these questions as those times are long past and can only be recreated in our imagination and in our creative modeling. But a trip on the EBT would have definitely been on my bucket list of train trips to take. My only option today is to visit the EBT restoration and take one of the available tourist trips. If you go, say hello if you see a wide-eyed guy taking a lot of photos of everything that isn't moving and taking a lot of notes. That will probably be me.

To create my imaginary EBT trip, and to encourage you to imagine train trips that you are not able to actually take, is why I wrote this article. I believe imagining such trips may give us new modeling ideas for our model railroads. It does for me, and I hope for you.

Thanks to everyone for reading about my imaginary trip. I would love to hear about your real or imagined bucket list train trips. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com)

All this storytelling and going back in historic times got me thinking of my youth and the many fond memories I can recall along my way to sleeping in a big boy bed and adolescence. I decided to incorporate some of them in my model railroad scenery. It gave me some new subjects to model. What do you think?

## **Including Childhood Memories In Your Model Railroad**

You have heard me say many times I love to scratch build models of almost anything in brass. Well, here is another, some may say a little weird, example. But we all have our childhood memories. For me creating scenes that remind me of some of my best ones is something I really enjoy. My problem, in creating scenes however, sometimes requires me to go way outside my comfort level and learn new modeling skills. The playground scene shows an example of one of these situations.

Normally if I can remember events from my childhood, I can research those types of events in the correct time period on the Internet, and use that information to create a scene that allows me to model one of my past childhood memories. I really enjoy the research aspects of my model railroading hobby because it means I can build a wide variety of different scenes which can become a part of my hobby. I have not found any other hobby that allows me this kind of modeling variety. I can, in a small way, relieve a past memory every time I look at one of the scenes. For example: My grade school playground in the mid 1940s.

## **My School Recess Playground Memories**

For me, the best part of grade school was recess. One of my earliest memories is the school playground. The grade school and the public playground was near my home so Mom could also take me to it at non school times. Like most moms, she would stay and watch me and my friends to make sure we did not hurt ourselves, too

much, while playing on the various equipment. I often wondered how all the moms knew to arrive with their kids about the same time? Actually, I think the moms prearranged their arrivals and enjoyed the free time to just sit, relax, and talk with each other as much as we did playing. As long as we were not fighting, they just let us play. I think they just used watching us play as an excuse to get together and relax for awhile. Some days all the moms even showed up with snacks and cold drinks. Picnic time at the playground!

I started this project by looking up 1940 era photos of playgrounds on the Internet and at once recognized some of the swings and climbing equipment I loved to use, as well as, photos of how we kids looked back then.



This is a photo of the 1/4" scale playground equipment I scratchbuilt in brass for my playground scene. I have plans to add several benches with seated moms. So far I just haven't found the right photos for the moms.

Does anyone reading this article recognize any of the playground equipment from their youth? What was your favorite? Does one of the cutout photo kids look familiar? If so, then I bet you are able to recall your own experiences and that will help my model railroad come alive and become personal for you.

That relationship to the scene is, of course, what I hope happens as that feeling may cause you to want to create a scene from your memory and model it. Or at least tell me about your experience so I can include it in one of my scenes. That way your memories will become a part of my model railroad. Naturally, I will need your photo as a kid to include in the scene, so other viewers know whose memory they are seeing.

Notice that currently the kids are just cut out card figures from Internet photos I found. I want to replace them with more realistic 3D figures, but so far have not found a way to do that unless I sculpt them. Sculpting is something I must learn with the help of a mentor.

I must admit I considered trying to 3D print my figures, but I don't know how to use a CAD program to design the kids for 3D printing. In fact I don't even know if there is a CAD program that can design people in various poses? Having zero experience with any CAD programs and frankly considered hardly able to use a computer beyond email, Word to write my articles, and Google to search the Internet for my research needs. Obviously I would need a mentor for any CAD effort.

After talking to several talented CAD users who could be my mentors and showing them my very basic computer skills, their consensus opinion, after smiles, snickers, and in one case laughing out loud, was I should forget CAD and sculpt the figures. However if there is an experienced CAD designer who is interested in helping me create my kids for the scene as shown in the card cutouts, please let me know. In the meantime, back to trial and error sculpting.

However, I still needed a mentor as I had no idea how to sculpt a figure. The mentor I asked to help me learn to sculpt them said: "my best advice is for you to pick a photo, cover it with sculpting material, and then learn to make your figures by trial and error." I had hoped for a more hands on teaching experience, but then he continued: "Call me when you have completed a figure you like and we will discuss your effort and determine if you are ready to move on to your next lesson". He did suggest a sculpting material for me to try. So off I went learning to sculpt.

My first 'sculptured kid' effort is shown on the climbing bars. Notice the two kids at the top. One is card, the other I sculpted using the card figure as a guide. I hope you can you tell the difference? (See the picture on the next page.)



I am reasonably pleased with the result as a first attempt, but know I will improve with more practice. My mentor agreed. His exact words as I remember were: “Keep trying.” Let’s face it, we both understood the only difference between an excellent modeler (sculptor) and a beginner is the number of attempts and failures the excellent modeler has made. Patience and practice lead to great modeling. So I just need to keep trying to sculpt figures to get better. As I write this, I am still on lesson 1, and still trying to improve figure one. At the rate I am going, I will need to reorder more sculpting material soon. I wonder if the newly opened Hobby Lobby sells it. Road trip!

The product I used to sculpt the figure is Aves APOXIE SCULPT Modeling Compound from [www.Avesstudio.com](http://www.Avesstudio.com). I really like the product and plan to try some of the others they produce, including the APOXIE Clay, paste, solvent, and FIXIT Sculpt and believe their products are going to be my solution to create the figures I need. I am learning to use the material and improve my skills to create the best figures I possibly can. I gave up on ‘perfection’ for all my modeling a long time ago, but I definitely want to at least produce recognizable, realistic, prototypical, models.

You would be surprised how many ideas for scenes I get from other people once they have seen some of my memory scenes. Frankly this sharing of ideas is another one of the advantages of my hobby. Once a person sees what I am modeling, their memories seem to kick in and they share them with me. So don’t be shy about telling me anything you think I should add or change to the playground scene?

A friend has already suggested I animate the playground scene. Great idea, but that will definitely require another mentor. Anyone out there know how to animate scenes and want to help me learn how to do it? How about not only animating the scene, but also developing a robotic program to operate the animation? Another mentor needed. Maybe I get some help from the local Robotics club members.

Next time I will discuss my model airplane flying memories and show you the scene I created. I would love to get a photo and some comments about your model airplane flying memories I can include in the finished scene.

Finally, some comments from a respected reader about my recent “Future of O Scale Train Shows” article.

Well it took me saying in the [January/February issue of this magazine](#) how disappointed I was I had not received any comments about my article “The Future of O Scale Train Shows” in the November/December issue of this magazine, for John Wubbel, a very respected O scale model railroader and CEO of Conductor Railway Hobbies, LLC, which oversees the modern All Nation Line, to provide me well thought out comments about my articles that I want to share with you. I appreciate John’s comments and again ask others to share their feelings about the “Future of O Scale Train Shows” and other subjects I write about.

“Hi Jim, I hope you are well in this new year. I try my best to read *The O Scale Resource* online cover to cover and I noted on Page 80 you said no comments were forthcoming regarding the future of O Scale Train Shows. Suffice it to say, it is entirely possible few people read your article and the reason being is the volume of content for the hobby for any scale in general is more than most of us have time to consume. So it could be that reason. It is not that noone cares about O Scale Train Shows, I think they do and for those of us dialed into the regular meet ups, we have those on our calendars.

But I think what matters is the need for the show promoters to do more outreach to people in the craft communities in the form of invitations, etc. As far as constructive ideas, perhaps new things can be tried regarding the internal structure of an ongoing show. For example, the K-Mart Blue Light special mini-clinics at a vendor table, they popup and are of short duration. Another example that was a good idea they did at the 2018 O Scale National, the vendors were located in a whole bunch of different medium size rooms and at the

door entrance way, were shoe boxes that you could register to win a door prize where vendors supplied some nice giveaways. How about a silent auction? I know when I was a kid and attended the NMRA National in St. Louis in 1970 they had a huge auction with an auctioneer that ran all night. I finally outlasted everyone and won that lantern I was after at 5am the next day. That was so much fun. But recall, I was early teens where as the older folks called it quits when it was their bedtime.

Now with regard to specific regional shows, I get invitations all the time, most recently to O Scale West. Well it is not practical for me to make that trip to the west coast. It is so close to the March Meet for us, thus I am wondering if there would be a way to remote into a show like that to visit vendors, clinics, or just social events. For a 2 or 3 day show that seems doable, but not for a short 4 hour show like Strasburg.

Anyway, I would also like to comment and advise that you go real easy on content generation with ChatGpt or Co-pilot. Yes it is easy to knock out content in just 30 seconds, but it is not always the best. Really the best copywriters use a professional subscription to Anthropic's Claude and the best way to utilize these AI tools is to write the copy yourself, do the research yourself and then run it through Claude, and do a re-analysis of the AI output before publishing. As it turns out, if one works with those AI interfaces in conversation, these engines are still very dumb. As one may note, your interaction is contributing to its training and more often then not you are sitting there giving it corrections or giving it a website that contains information that the LLM never data mined so it can't know about it or analyze it.

So the business plan from ChatGpt, probably not doing a info service in terms of someone getting into the manufacturing business as a cottage industry. It is significantly over generalized because the important factors not covered and not folded into the conclusion are missing. Even though you are talking Mom & Pop business here, labor, insurance expenses, product development costs to name a few are left out of the picture, plus there is so much public domain prints that one can not be competitive with to fund an ongoing firm. As it stands, the business plan, at least for me, reflects a different question and that is, does one want to be participating in the hobby of model railroading or does one want to be partaking in the 3D Printing hobby?

Realistically, who has time for both? I know in my case I can only work on my models after 5pm on week days, weekends or holiday/vacation time off. 3D printing robotics are great, but no one talks about the down time and maintenance, print failures and thus for a small in-home business, it is a pretty good bet there will be no ROI on that capital expenditure to get started. And I can tell you it will take 5 years before one can get ahead of the game no matter the type of business. And that can be very disappointing.

So that is why I am saying, less content may be better even in regards to the 3D printing preface to the business plan because most of those talking points are widely known already due to the proliferation in the literature and social media. I do think modelers appreciate the more focused articles on methods and techniques.

Best Regards and have a happy and productive new year. John Wubbel”

JK. Thanks John for your comments. I understand your positions and many of your ideas to improve “Future O Scale Train Shows” and hope O Scale show producers, vendors and attendees express their opinions.

I also appreciate your constructive criticism about another of my articles.

When I sent John a draft copy of this article, he sent me a second email which included the following:

“I was not aware you would publish or distribute my comments as I thought you were looking for private constructive comments. However, I am okay with you sharing my words, but do note my intention was not to be critical of your work. I do enjoy The New Tracks articles and all the work you put into it, I think it is great!”

Thanks again John, have a great 2026 and a great success with bringing back the All Nation Line to our hobby.

## **New Tracks Mentoring Inc: Reorganization and New Leadership for New Tracks**

As of January 1, 2026, all New Tracks Modeling educational activities are being incorporated into New Tracks Mentoring, Inc., 501(c)(3) non-profit corporation. These activities include all of our educational programs including the New Tracks Live Weekly meetup, our Facebook group, Website, New Tracks Mentor (NTM) Award Program, publications, and any of our New Track's promotions.

The officers and Board of Directors are:

- President: Phil Edholm NTM
- Vice President: Vacant
- Secretary: Hank Primas
- Treasurer: Bob Davidson NTM
- Members of BOD: Jim Kellow MMR, NTM; and Jeff Jordan NTM

At its last meeting, the Board also appointed Hank Primas the Chairman of the New Tracks Scholarship Committee. Members of the committee include Jamie Bothwell NTM, Steve Sherrill NTM, and Mitch (Doc) Lovelace NTM.

### **Purpose of New Tracks Mentoring Inc.**

Many of us have been involved with model railroading for what seems like our entire lives. The memories of our first layout, the excitement of operations, the pride that came along with finishing a craftsman kit... these are the things that drew us to, and kept us involved, in model railroading.

But we're facing a real challenge today. Fewer young people are getting involved in model building and railroading. There seem to be many reasons, including cost, time constraints, and social, educational, and family obligations. It is imperative for our hobby's future that we do everything to encourage young people to become involved in model railroading.

There are also many people coming back into our model railroad hobby that need to refresh and update their modeling skills and relearn in some areas how to build railroad models using new technology.

Encouraging more railroad modeling, providing educational programs and mentoring to help educate and pass along the old and new modeling skills and building confidence to modelers who need the help. These are the areas where New Tracks Mentoring Inc., has a role to play.

Our programs are designed to get more people building more railroad models, mentoring to help them build the best models possible and award academic scholarships to young model railroaders who are HS graduates entering, or are currently in, a STEAM (Science, Technology, Engineering, Arts, Math) undergraduate college, university, or technical school programs.

New Tracks Mentoring Inc., is a volunteer group of railroad modelers who are passionate about ensuring that our craft continues to the next generation. We see our programs, especially our scholarship, as a bridge between today's modelers and the next generation.

These are the young men and women who will be responsible for continuing our craft, and who will be developing the latest technologies that will heighten the modeling experience for all of us. Before our hobby "ages out" we are committed to taking whatever steps we can to ensure that railroad modeling remains a vibrant, rewarding pastime for years to come.

## Time to Start Getting Student Applications For Our 2026 Scholarships

Now it's time to start working on getting students to apply for our Scholarship program for 2026. For the 2026/27 academic year, NTMI will be awarding at least three, \$2,500 scholarships to deserving students. Please visit our website: [newtracksmodeling.com/scholarship](http://newtracksmodeling.com/scholarship) for details about making an application for, our 2026 Scholarship program (applications will be accepted between January 1, and June 1, 2026).

If you know a student model railroader in or entering college, let them know about the scholarship. Clubs and organizations can help us get the word out about the scholarship by distributing flyers or displaying one of our banners at train shows, conferences, meetings or at your local school or college. Contact Hank Primas for more information on helping us find applicants for the NTMI Scholarships. His email is: [Henry.Primas@newtracksmodeling.com](mailto:Henry.Primas@newtracksmodeling.com) Thanks to everyone in the model railroad community for all your help.

## Why Donate to New Tracks 2026 Scholarship Fund?

We hope all members of the model railroad community consider donating to the New Tracks educational activities including our Scholarship fund because it stands for a direct investment in the future leaders of the hobby.

Our unique scholarship program is specifically designed for model railroaders who are pursuing a college degree or technical school credential in a STEAM (Science, Technology, Engineering, Arts, and Mathematics) related field. By supporting these young individuals, the community is not only helping them achieve their educational goals, but also nurturing the next generation of creative and technically skilled modelers.

The scholarship committee reviews applications based on academic achievement, involvement in modeling activities, and an essay detailing how their hobby has impacted the applicant's lives. Donating to this fund is a tangible way to give back to our hobby, ensuring that the passion, knowledge, and innovative spirit of model railroading continue to thrive for years to come.

Every donation, of any amount is extremely welcomed and needed. We believe our scholarships helps to show students that their model railroad hobby is a worthwhile endeavor which will help them in their STEAM pursuits.

Businesses can provide support to the scholarship in several ways: First, by making a cash contribution of any amount. Sponsorship categories are available at the \$250.00, \$500.00, \$1,000.00 and \$2,000.00 levels. We also encourage manufacturers and distributors to include information about the scholarship in their product packaging. This can make a big impact by reaching out to the total modeling community.

You may sign up thru Zeffy to make either a one time or a periodic donation. For more information or if you have any questions, please email us at [NTMMS@newtracksmodeling.com](mailto:NTMMS@newtracksmodeling.com). Please join us with a donation thru Zeffy : <https://share.google/9GQhavF0PbWqxnSUq>.

Or you can send your donation checks to our Treasurer Bob Davidson at:

New Tracks Modeling Mentoring Scholarship  
c/o Bob Davidson  
6273 Gulf Stream Path  
Cicero, NY 13039

## Closing comments from New Tracks Founder: Jim Kellow MMR, NTM

I am extremely proud of all our current New Tracks volunteers, and activities. I believe, however, there may be other activities I should investigate. Even retired I am still around and modeling.

1. More New Tracks Newspaper articles

My monthly column in my local Chronicle newspaper is having readers approach me with ideas I have never previously through about. I am doing everything I can think of to promote railroad modeling, mentoring and our Scholarship Program. Here is a link to my December 19, 2025, and January 16, 2026 articles in the Citrus County Chronicle newspaper.

December 19, 2025, article: Discover the art of storytelling for your model scenes

Here is the link to the article: <https://share.icloud.com/photos/031wxg2jkYh9WfcEl07fHZoZQ>  
Continued on next page here: [https://share.icloud.com/photos/02fnx2xWBw0G4OxXQ\\_ZzVJbMA](https://share.icloud.com/photos/02fnx2xWBw0G4OxXQ_ZzVJbMA)

January 16, 2026, article: Mentoring Kids through model railroading: A rewarding hobby

Here is the link to the article: [https://share.icloud.com/photos/078WNdnY0QcDAf\\_gEgo4gfimg](https://share.icloud.com/photos/078WNdnY0QcDAf_gEgo4gfimg)  
Continued on next page here: <https://share.icloud.com/photos/0763AcSOuPOHu0OZ4avqhW9bg>

Give your local newspaper a call and see if they are interested in articles about our hobby. You may be pleasantly surprised. Good luck.

## 2. Your ideas

Please write to me about your ideas for any of our New Tracks projects. I love getting your comments, suggestions, modeling ideas or just having a modeling conversation. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com)

Well, it's time for me to do some modeling.

Time for me to return to my workbench and start working on something that I fell in love with and just must model. Happens all the time.

But before I start modeling, making/correcting mistakes, and learning, I want to ask you to please help us promote modeling and mentoring in our great hobby by helping support our various New Tracks Modeling programs by doing the following:

1. Volunteer to join our Team and help produce and develop our New Tracks Modeling Zoom and YouTube shows, Facebook Group, New Tracks Mentor (NTM) Award program, and our website. Email Phil Edholm NTM at [Phil.Edholm@newtracksmodeling.com](mailto:Phil.Edholm@newtracksmodeling.com).

2. Contribute to our Patreon account New Tracks Modeling to help pay our out-of-pocket cost to run our shows. Click here to donate on Patreon: <https://www.patreon.com/u80691578>

3. [Subscribe for free to our YouTube Channel](#), New Tracks Modeling, and ring the bell to get advance notices of our YouTube shows. Please watch the YouTube advertisements placed on our videos so we can earn a little revenue from YouTube to produce our shows. There are over 1,600 videos of our past shows available on our channel for you to view.

4. Subscribe for free to our website: [newtracksmodeling.com](http://newtracksmodeling.com) which provides login links to our Wednesday Zoom shows, and provides information about upcoming agendas for New Tracks Modeling.

5. Donate to our New Tracks Mentoring Inc., program. Details for Individual and Corporate donations are on our website: [newtracksmodeling.com](http://newtracksmodeling.com)

[To donate use our Zeffy account, just click here.](#)

6. Spread the word to High School Graduates and college students about applying between January 1-June 1, 2026, for our \$2,500.00, New Tracks Modeling Mentoring Scholarships to be awarded July 1, 2026.

Represent us at shows, conventions, and events with our banners and handouts.. Details and a Scholarship Application are on our website: [newtracksmodeling.com/scholarship](http://newtracksmodeling.com/scholarship).

7. Subscribe for free to *The O Scale Resource* online magazine so you don't miss any of my New Tracks articles and see some great modeling by various modelers who may become one of your mentors.

8. Let Kurt Thompson MMR NTM, know if you want to become a New Tracks Mentor (NTM) by being on one of our Zoom shows, or being profiled in one of my articles. Kurt can be reached at [Kurt.Thompson@newtracksmodeling.com](mailto:Kurt.Thompson@newtracksmodeling.com).

9. Write to me! I love getting your comments, suggestions, modeling ideas and just having a conversation with modelers. My email is: [jimkellow@newtracksmodeling.com](mailto:jimkellow@newtracksmodeling.com).

Thank you again for all your interest and for reading this far. Until next time with more New Tracks, I wish you happy, successful modeling with whatever you are building in 2026! Please don't forget to become a New Tracks Mentor (NTM) especially to young modelers.

# NEW TRACKS MODELING

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## Where Mentors Help Modelers Build



**NEW TRACKS LIVE – Streaming Every Wednesday from 7:00 PM to 9:00 PM Eastern on YouTube & ZOOM**

Subscribe for free on our website to receive program links: [www.newtracksmodeling.com](http://www.newtracksmodeling.com)

# NEW TRACKS MODELING

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## RESOURCE

*NEWS, REVIEWS, INFORMATION TO USE  
We're putting the "modeling" back  
in Model Railroading! 🚂*

MY BUILD offers modelers a platform to share projects, get advice, celebrate their achievements, and help others learn tips that can improve their modeling abilities. Monthly Modeling Challenges are designed to inspire your imagination and creativity, but all types of models are always welcome during these special events.

The MY BUILD is a friendly, non-judgmental space where modelers of all ages and experience levels can connect, teach, and grow. Even if you can not participate, please Join and follow our Facebook Group, New Tracks Modeling, to meet and connect with other modelers, and discuss the MY BUILD show content and ideas for the future.

To participate in one of our MY BUILD show segments, please send one or more photos of your modeling projects with captions to, Chris Coarse at [railrunner130@hotmail.com](mailto:railrunner130@hotmail.com). He will compile them into a slide show for the show. Participants are encouraged to talk about their work, share techniques, ask questions, or give feedback. You will find it's a fun and educational experience! We welcome your feedback and suggestions for MY BUILD.

### "My Build" Featuring Models From Our Viewers (12-17-25)

[You can see a video of the entire MY BUILD segment here.](#)

Each of the participants has an email address included, and welcomes your contact.

**This is a big one, all about various viewers layouts.**

Greg Cassidy NTM– [gcassidy2@verizon.net](mailto:gcassidy2@verizon.net) – HO scale





**Fred Cosgrove NTM - [ssshortie@wowway.com](mailto:ssshortie@wowway.com) - HO scale- Building his layout on a 14.5 x 9.5 ft sun porch in Florida. 3 ft isle. Modeling the Whirlpool factory in Clyde, OH. Wye at Belleview, OH. Monroeville with Grain towers. Sandusky East yard with car house, downtown Sandusky and a recycling area. Designed to be point-to-point or a loop.**

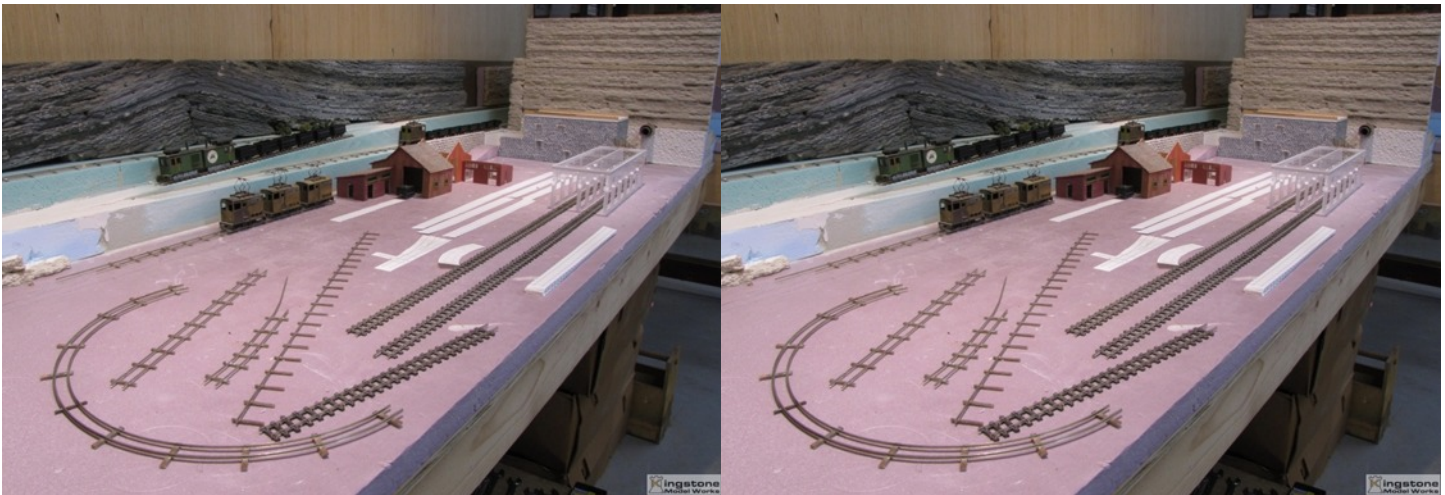


**These pictures only give a brief look at these wonderful layouts. Be sure to watch the video for a full description and many more pictures.**



Phil Edholm NTM - [pedholm@pkeconsulting.com](mailto:pedholm@pkeconsulting.com) - On30 Scale – Converting a game room to a train room using modules. Repurposed a mirror to add depth.





Bill Hawk NTM - [archhawk@comcast.net](mailto:archhawk@comcast.net) -O scale - took 6 weeks to build the small layout for a Christmas train at an assisted living facility. Built in his house and transported. Started with an old Marx train.

Frame pieces were cut in backyard work zone expansion.

Test placement for transformer in "out of the way corner" of the layout.

Saturday 11-08-2025

A frame was made from 1 x 3 lumber, both to protect the edges of the sub-base and the foam and to provide a slight depth to run wiring under the sub-base.

Easy testing of the small transformer proved a steep fall-off of power to the far end of the loop. Original clip-on w/ set was broken; one was found in supply bin and two additional were purchased for a total of three locations to be wired.

Civil engineering taking place for road bridge and land-form at tree base. Also testing a short 8" high backdrop.

Tuesday 11-11-2025

Word comes from final site location that table will be narrower than the layout! To help protect intended wiring additional 1 x 3's are added while still allowing for the wiring to be run underneath.

The transformer is lowered slightly from view and to more easily facilitate the power cord passing through the side wall. Track clips are nearing final location(s).

Sunday 11-09-2025

Greening the environment...

Friday 11-28-2025

Friday 11-28-2025

Friday 11-28-2025

Continuing work on the greening of the environment. The paving areas at the gas station and the firehouse are glued down and weighted. In certain areas the turf did not "stick" well enough to the base paint coating. These patches are back filled with glue, more sprinkled turf blend and all areas are further glued with 50/50 water/matte medium.

Parking area at the brewery is done as gravel with "fine" Woodland Scenics ballast.

Almost final shots...

Saturday 11-27-2025

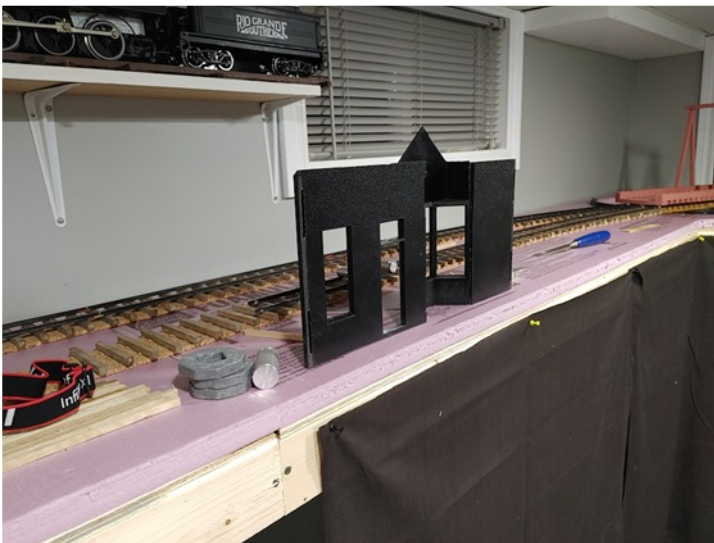
Saturday 11-27-2025

Saturday 11-29-2025

**SIGN UP**

**Did you miss an issue of The O Scale Resource? Forgot to look? Well, sign up here and never miss another exciting issue. We'll send you an Email when the latest issue is loaded to our site. Don't worry, we won't sell or post your Email address to anyone! Click Here to sign up today!**

Mike Lafod NTM - [steamfoamer163@gmail.com](mailto:steamfoamer163@gmail.com) - 120.3 scale. Turntable in progress. Reverse angle station to add depth. Will eventually expand into another room.



Jeff Jordan NTM - [jordan.jordan54@verizon.net](mailto:jordan.jordan54@verizon.net) - On30 – Update on his D&RGW layout. Ophir station is a Banta kit. Used a local store as a stand-in for one in Ophir. Looking to add depth to trees. Used a 1/48 Tamiya car kit. Abandoned mill is N scale. Revell Farmhouse is N scale. Les Davis figures to add life.





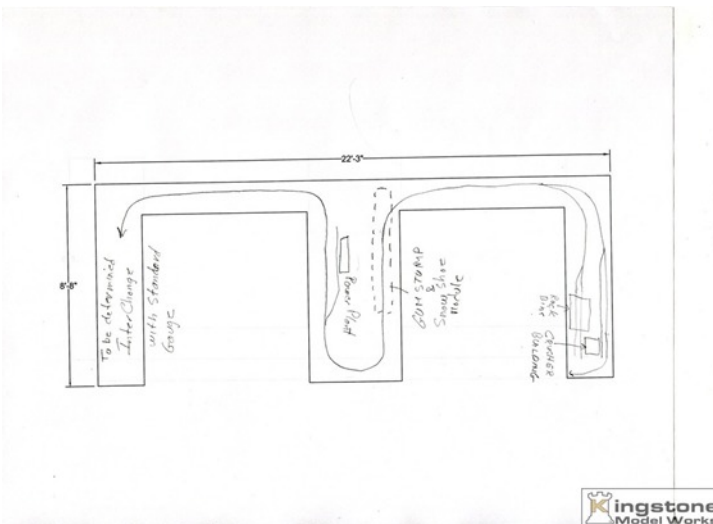


Bill Stimson NTM  
[mwstimson@q.com](mailto:mwstimson@q.com)

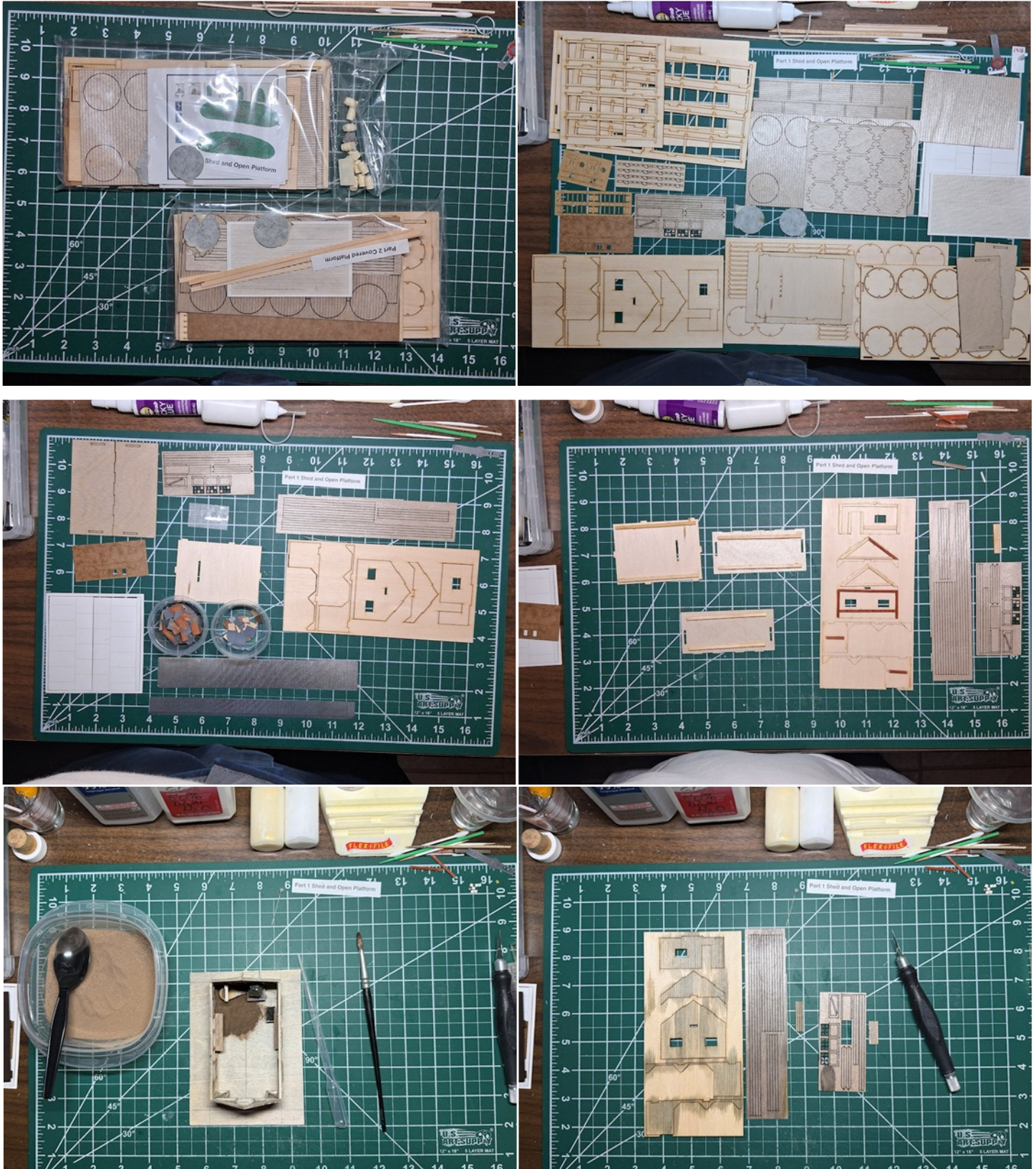
N Scale – Model  
 Tech Studios Feed  
 Store. Rail-Scale  
 shingles. Seasonal  
 miniatures 3D  
 printed by his son.



Bernd Fanghanel NTM – [protolancer@kingstonemodelworks.com](mailto:protolancer@kingstonemodelworks.com) - HOn3 quarry



Fr. Ron Walters NTM - [rwalters@friars.us](mailto:rwalters@friars.us) - HO scale – Preview of a future Watch Me Build – American Model Builders – G.R. Dill & Sons Salting Station. These photos are not backdrops, but they are what is on my workbench right now. I have only one photo of the continued work on the blacksmith shop. The rest are photos that I will use in a presentation in the future for the show of a salting station that Tom sent me.



# NEW TRACKS MODELING



## RESOURCE

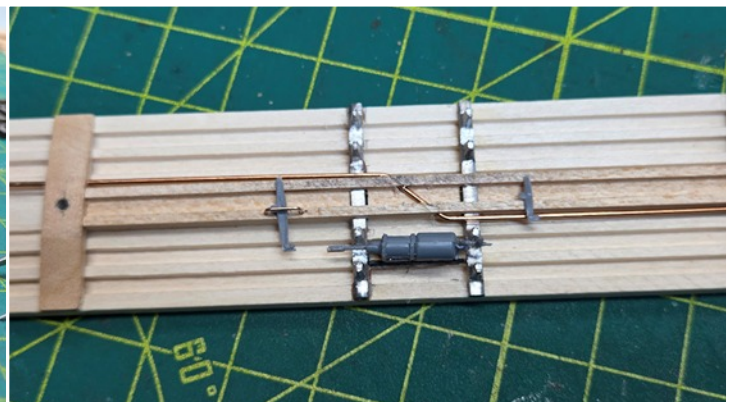
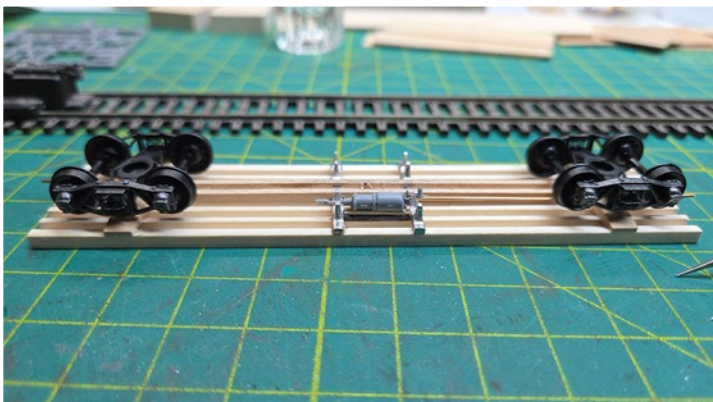
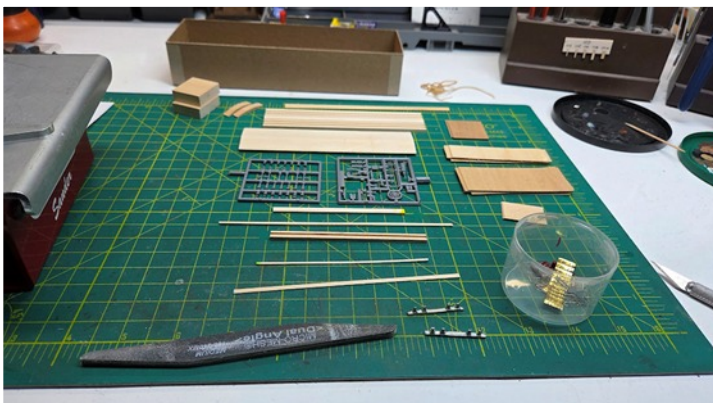
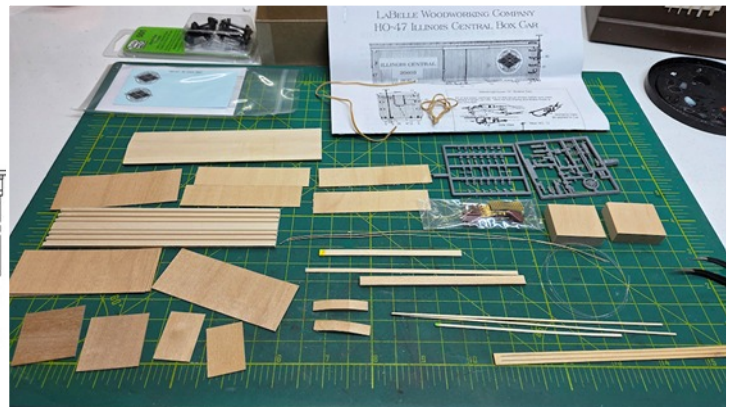
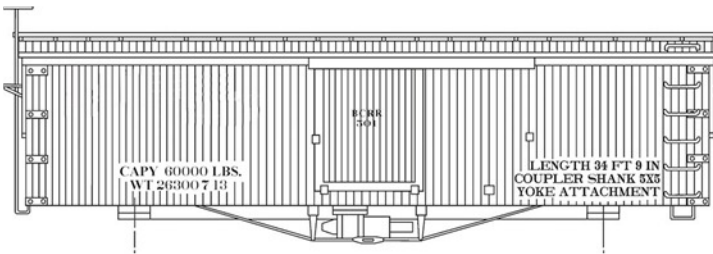
NEWS, REVIEWS, INFORMATION TO USE  
We're putting the "modeling" back  
in Model Railroading! 🚂

### "My Build" Featuring Models From Our Viewers (01-14-26)

[You can see a video of the entire MY BUILD segment here.](#)

Each of the participants has an email address included, and welcomes your contact.

Greg Cassidy NTM – HO – [gassidy2@verizon.net](mailto:gassidy2@verizon.net) - LaBelle HO kit



Chris Coarse NTM– HO – [conowingomodels@yahoo.com](mailto:conowingomodels@yahoo.com) Pilot model of a motor lodge in development. Based on a structure along US Rt.1 in Darlington, MD.



Phil Edholm NTM – O – [pedholm@pkeconsulting.com](mailto:pedholm@pkeconsulting.com) Work in progress of a Banta Warehouse kit. Intended to be a strawberry/fruit warehouse. Cartons are filament printed. Strawberries are beads.





Bernd Fanghanel NTM – HOn30 – protolancer@kingstonemodelworks.com Rotary dumper made with a CNC machine. Made out of ¼ aluminum.

## HON30 ROTARY DUMPER

PROCESS OF COMING UP WITH A DESIGN FOR AN HON30 ROTARY DUMPER.

Layout Example

## A FIND ON E-BAY

1:17 チャンプラー 組立説明書 (スケール 1/17mm 参照)  
企画・設計・製作/岐阜電機

の図はチャンプラーキットを右裏面よりご覧いただき、組み立て時に参考にしてください。このキットは、アランビー一家が普及する以前に中規模の鉄道模型でよく使われていたタイプの物で、現在はほとんど見られなくなっています。キットは、各鉄道模型店にある物を参考に、デザインをアレンジいたしました。スケール 1/17mm と 1/25mm を併用して製作できるように工夫してあります。

## SOME ASSEMBLY REQUIRED

BATTERIES NOT INCLUDED

Photograph of completed assembly

SHOWS THE SIZE DIFFERENCE BETWEEN A WALTHER'S ROTARY DUMPER AND TWO TOC LOCOMOTIVES. THE BLACK DUMP CARS SHOWS THE SIZE OF CARS TO BE USED.

Jeff Jordan NTM – On30 – jordanjordan54@verizon.net Work in progress of a body replacement for a Precision Craft Galloping Goose. Swapped out bus body for a 3DP Trains Pierce Arrow body. Headlights added, along with Soundtrax decoder and keep alive.



NEXT PAGE:

Dave Landels NTM - HO - [keddes@icloud.com](mailto:keddes@icloud.com) I've built these structures from references and photos I could find on the internet. Materials "Evergreen Styrene" the Silo's 60pcs are Vacuum tubing you find in homes, the actual Terminal has 245. The Blue tubes are used Pill bottles upside down with extra plastic pieces to look like tubes going into the Terminal.

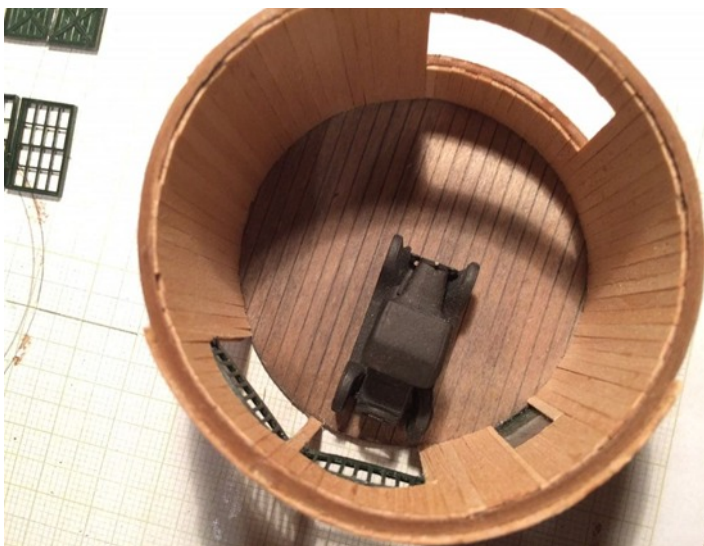
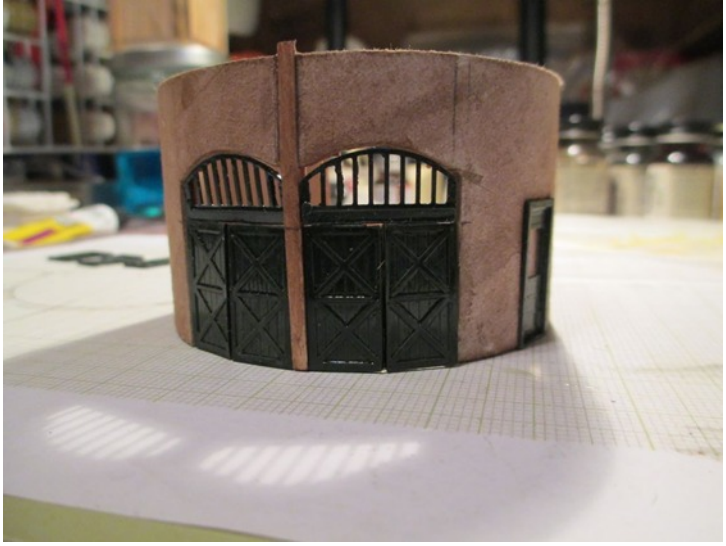


Bill Stimson NTM – N – [wstimson@q.com](mailto:wstimson@q.com) National Train Show purchase – Precision Lasercraft Dee’s Bees. Rock is a decal. Taskboard doors and windows.





Rich Wolfanger NTM – HO – Duff's Gas and Go - tribute to Dave Guffey. Scratchbuilt from a cardboard packing tape tube. Lighting added. Chooch rubble. Tichy doors. Sierra West kit- duplicated with permission. hydrocal walls. Round windows are by Scale Structures. Blacksmith shop in the aft building extension. Copper roof with real oxidized patina.



# Buy US A COFFEE



Know the old joke “How do you make a million dollars in the model railroad business? Start with two million.”? Well, same here. We are not going anywhere and will still continue to bring you the best in O scale information every other month but...

As with any ad supported endeavor, there are ups and downs. We have not raised our ad rates (and will not) since we started 11 years ago. We feel the rates are more than favorable for the thousands readers our magazine reaches. Of course, not everyone wants to advertise instead using Facebook and other social media. In a normal paper magazine you have the subscription fee that more than covers mailing and normal operations. Not having that is fine here, but our costs have also climbed.

Traveling to layouts, web services, storage, and hosting fees have all gone up. So we thought we would try “buy us a coffee” in hopes of helping with these extra expenses.

Therefore, if you are getting a good read and enjoying the information, please “buy us a coffee” and help us continue to bring you the best in O scale information. **Hey, even a buck an issue every few issues will help.**



So, if you like what you see and can go with out a Starbucks for a day, click here and give it try.

Thanks for your support!



## Here is how to contact us:

Phone: 815.263.2849

Email: [amy@oscaleresource.com](mailto:amy@oscaleresource.com)  
[daniel@oscaleresource.com](mailto:daniel@oscaleresource.com)

Mail: The Model Railroad Resource LLC  
407 East Chippewa St  
Dwight, IL 60420

[oscaleresource.com](http://oscaleresource.com)

# O SCALE SHOWS & MEETS

Have an upcoming O Scale event? We would like to help publicize it. Send us the information up to one year in advance, and we'll place it here along with a direct link to your Website and/or Email.

## **O Scale West, the only O Scale, S Scale, and Narrow Gauge Meet west of the Rocky Mountains, is a month away!**

### **New location on a New date**

**March 6-7, 2026 (Friday - Saturday)**

The dates are March 6-7 Friday-Saturday. It is located at the Portuguese IFES Hall at 432 Stierlin Rd., Mountain View, CA. Open hours are Friday 1-4PM and 6-9PM and Saturday 9-4PM. Family registration for both days is \$35, Saturday only is \$25. Register on-line here or click here to download and print the registration form.

The swap meet, favorite model contest, seven clinics, and four on-site operating layouts are available during the open hours. Several well done home and club layouts in the area are open for visiting, here's their schedule.

Save big on your O Scale and S Scale and Narrow Gauge needs at the amazing swap meet. Shop vendor tables with everything from brass models to ground foam. Learn at multi-scale content clinics, and visit amazing layouts in the San Francisco Bay Area. If you model in or are considering O Scale, S Scale, or Narrow Gauge, this is the meet for you. See the world of larger scales and narrow gauge modeling.

If you are from out of the area don't miss the opportunity to visit the California State Railroad Museum in Sacramento. Don't miss the NMRA's The Magic of Scale Model Railroading exhibit. You can get on trolley at the nearby correct rebuild of an SP depot that will take you to a short walk to board a Capitol Limited train (wi-fi, food) and walk from the depot to the museum from the Sacramento stop. There are several trains a day.

Questions? Please send email to [info@oscalewest.com](mailto:info@oscalewest.com)

## **Model Train Show**

**The N.Y. Society Of Model Engineers**

**The Oldest Model Railroad Society In America Celebrating 100 Years 1926 To 2026**

**March 7 & 8, March 14 & 15, March 21 & 22, 2026**

341 Hoboken Road, Carlstadt, NJ

Saturdays and Sundays 1:00PM to 5:00PM

Admission: Adults \$8.00 – Children Under 12 FREE

FEATURING: 5,000 Square Feet of Model Trains, Two Large Operating Model Train Layouts, Plus Model Exhibits, Railroad Memorabilia

See: [www.ModelEngineers.org](http://www.ModelEngineers.org) for details

Follow us on [facebook.com/modelengineers](https://www.facebook.com/modelengineers)

## **O Scale March Meet**

**March 20-23, 2025**

## **O Scale March Meet**

**March 19-22, 2026**

Westin Lombard Yorktown Center  
Lombard, IL

The March O Scale Meet is a 3 day gathering of vendors, customers, clinics, and fun held annually in March in the Chicagoland area. This is the Chicago O Scale train show you've heard of. See ad in this issue.

Website: <http://marchmeet.net/>

Email: [ChicagoMeet@yahoo.com](mailto:ChicagoMeet@yahoo.com)

## **Strasburg 2 Rail Train Show**

**April 11, 2026**

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

## **NYSME and METCA All Gauge Train Show**

**Saturday April 11, 2026**

**St. Joseph's School Auditorium**

**120 Hoboken Road, East Rutherford, NJ 07073**

In a joint effort of the New York Society of Model Engineers and the METCA Division of the Train Collectors Association, we will be co-sponsoring a Swap Meet in East Rutherford, NJ.

The meet will cover everything TRAINS, all types of Trains.

From the finest in Scale to finest in historic Tin Plate, pre-war Lionel, post-war Lionel, American-Flyer, all will be represented. This meet will offer everything: "G", High Rail, "O-2R", "HO", "N" Scales, both new and used. Along with scenery materials and buildings for your layout. There will be tools, books, building materials, paints, detail parts, repair parts... First time offering of private collection... White Elephant Table to benefit NYSME.

NYSME layouts will be open, 341 Hoboken Road, Carlstadt, NJ

Same street, opposite side. Hours: 9:00 am to 2:00 pm

Admission \$5.00

NOTE: You do not have to be a dealer. Hobbyist, take a table and thin your collection

Additional table information and Registration Forms at

[ModelEngineers.org](http://ModelEngineers.org)

Phone: 201-939-9212 e-mail: [contact@ModelEngineers.org](mailto:contact@ModelEngineers.org)

## **Harrisburg Narrow O Summer Meet**

**June 12-13, 2026**

Friday June 12, 11am to 6pm

Saturday June 13, 9am to 3pm

Sponsored by: [Narrow Gauge Modeling Company](#)

New Hope Church

584 Colonial Club Drive, Harrisburg, Pa. 17112

[See the Facebook page for more information.](#)

**Strasburg 2 Rail Train Show  
August 8, 2026**

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

**Strasburg 2 Rail Train Show  
October 10, 2026**

Strasburg Train Show: Two-rail swap meet at the Strasburg Fire Co, 203 W. Franklin St, Strasburg, Pennsylvania. 9 am-1 pm. Admission \$7, wives/children/military w. ID free, tables \$35 for first table, additional \$30 per. Great food, modular layout, clinics. Contact Richard Yoder EST evenings 484-256-4068 [Click here for info.](#)

**Indianapolis O Scale Show 2026  
September 15-19, 2026**

LaQuinta Inn Indianapolis South  
5120 Victory Dr,  
Indianapolis, IN 46203  
Fri. Sept. 18, 3:00 – 7:00 PM  
Sat Sept. 19, 9 :00AM – 3:00PM  
Website: [indyoscaleshow.com](http://indyoscaleshow.com)  
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## SHOWS & MEETS

**March O Scale Meet**  
**March 19-22, 2026**  
<https://marchmeet.net/WP/>  
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# Announcing New Products In O Scale Model Railroad Resource 3D Division - Resin Parts



**Fancy Standard  
Globe Options**



We offer two types of globes as well as choice of LED color. Cold white for modern lighting and Warm white for pre-sodium-vapor lamp / LED lighting, 1970's and earlier. Package include 5 poles, 5 LED's with color selection, 5 globes, standard or fancy and five 9V to 14V resistors for \$10.00 and ready to ship.



## Announcing Passenger Car Parts:

- Dining Tables
- Parlor Car Seats
- Two Seat Settee
- Dining Car Chair
- Modern / Streamline Coach Seats

<https://modelrailroadresource.com/WP>